



DESIGN RECOMMENDATION
MEETING - EAST DRB

SEPTEMBER 11, 2024

DPD PROJECT # 3041005-LU
416 15TH AVENUE EAST
SEATTLE WA, 98112

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Contact:
Melissa Wechsler
Runberg Architecture Group
1 Yesler Way
Seattle, WA 98104

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PROJECT DATA OVERVIEW

416 15TH AVENUE EAST



The project site is located on the eastern edge of the Capitol Hill Urban Center Village. The site itself occupies most of the west half of the block bound by 15th Ave E, E Republican Street, 16th Ave E and E Harrison Street. There is a fully paved, 2-way alley along the east property line that serves the existing businesses on the west half of the block and the residential structures on the east half of the block. The two-story Fire Station building south of the site has an active retail use at the ground floor that takes advantage of a pedestrian setback area along 15th Ave E, and green space setback off the alley to the east.

There are no existing street trees along the right-of-way frontages on 15th Ave E or E Republican Street, but there are two mature trees (one that is a tier 2 tree) close to the north property line that the project would like to preserve. Streetscape improvements proposed include wider sidewalks, new street trees, and a curb bulb at the intersection of E Republican St to provide a permanent space for the bus stop along the 15th Ave E frontage.

15th Ave E is midway between I-5 to the west and Lake Washington to the east. It is a lively commercial street at the crest of Capitol Hill featuring a variety of shops and restaurants between E Mercer Street to the north and E Denny Way to the south. The project site is located east side of 15th Ave E, at the corner of E Republican Street, in the heart of this neighborhood commercial district. Contributing to and enhancing the dynamic quality of 15th Ave E is a primary objective for this project. The scale of the property provides an opportunity to develop a variety of retail and restaurant spaces that will fit into the scale of the existing commercial context. Enhancing pedestrian life and access through the neighborhood is another priority that will contribute to the viability of the commercial spaces and help make this project a destination that draws neighborhood residents and visitors alike.

RESIDENTIAL UNIT COUNT

172 UNITS

*INCLUDES A MIX OF STUDIOS, 1 BEDROOMS,
2 BEDROOMS AND TOWNHOUSE LOFTS

COMMERCIAL AREA

9,458 SF

BELOW GRADE PARKING

**101 STRUCTURED PARKING STALLS IN
BELOW-GRADE GARAGE**

SITE CONTEXT & URBAN DESIGN ANALYSIS

9 BLOCK SITE CONTEXT



THEN & NOW: THE “MAIN STREET” CHARACTER OF 15TH AVE E.

1937

2023



The expansion of the Capitol Hill Trolley to 15th Avenue East in the early 1900's supported the development of economic and cultural vitality along the street corridor. A range of smaller scale commercial buildings were constructed which helped define this street as an avenue of businesses nestled within the larger residential surrounding neighborhood. Set apart as a commercial area, 15th Avenue took on the character of a “Main Street” where neighbors can gather, and enjoy the lively mix of locally-owned businesses.

Over 80 years later, 15th Avenue East continues to support a wide array of small-scale commercial retailers. These businesses contribute to its pedestrian-oriented, active street facade. The prioritization of outdoor seating and generous pedestrian space helps maintain 15th Avenue's historic character while supporting a diverse mix of uses. This historic pattern and character of 15th Ave. E. will inform the character of the commercial retail spaces programmed in the ground floor of the proposed design.



LEGAL DESCRIPTION

PARCEL A:

LOTS 1 AND 2, BLOCK 17, LAW'S ADDITION TO THE CITY OF SEATTLE, RECORDED IN VOLUME 1 OF PLATS, PAGE 51, RECORDS OF KING COUNTY, WASHINGTON.

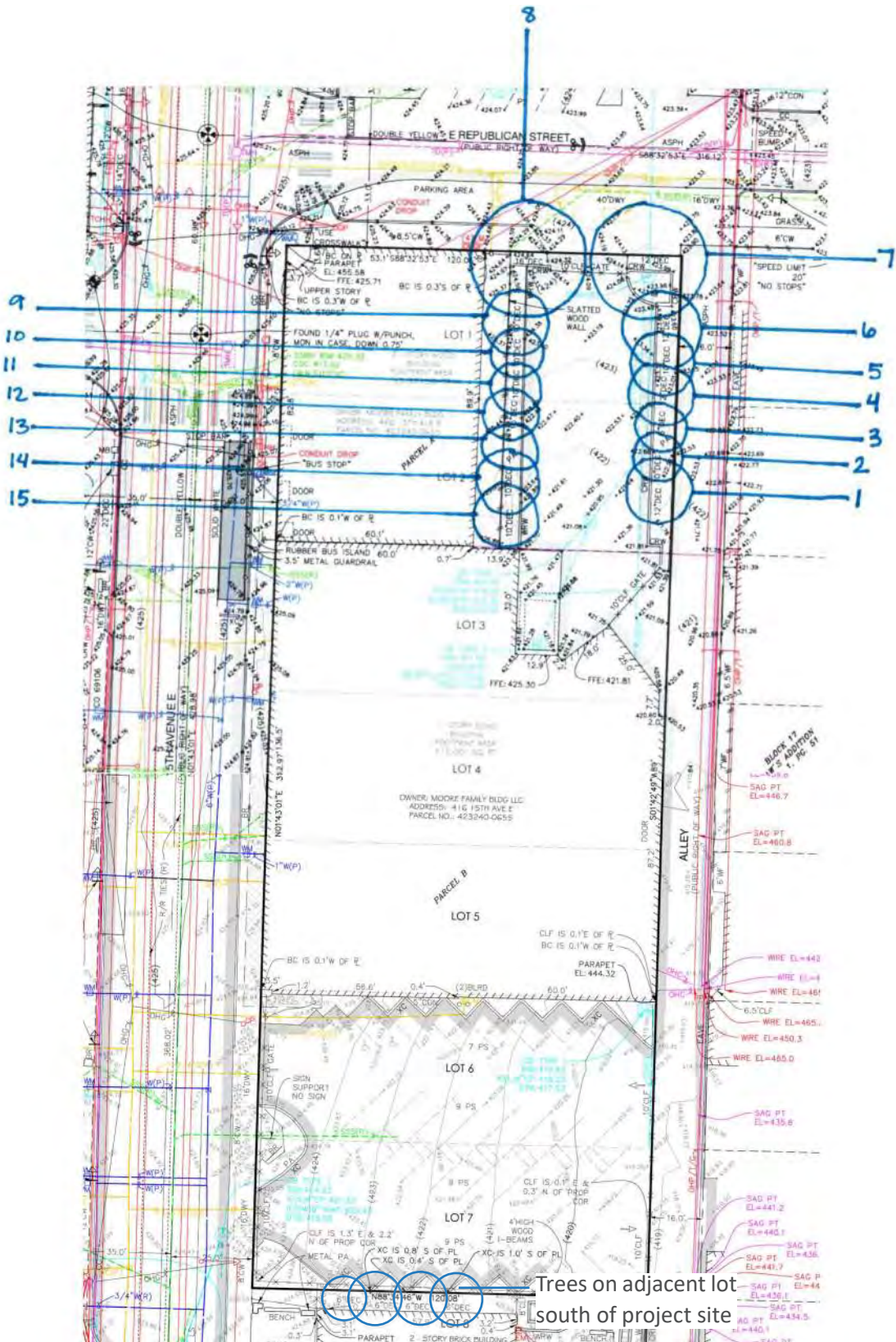
EXCEPT THE SOUTH 5 AND A HALF INCHES OF THE WEST 60.00 FEET OF SAID LOT 2

PARCEL B:

THE SOUTH 5 AND A HALF INCHES OF THE WEST 60.00 FEET OF LOT 2 AND ALL OF LOTS 3 THROUGH 7, BLOCK 17, LAW'S ADDITION TO THE CITY OF SEATTLE, RECORDED IN VOLUME 1 OF PLATS, PAGE 51, RECORDS OF KING COUNTY, WASHINGTON.

TREE CANOPY:

THERE ARE 15 TREES ON-SITE CURRENTLY. THE PROPOSED DESIGN PLANS TO SAVE TWO TREES (AS IDENTIFIED IN GREEN) AT THE NORTH PROPERTY LINE, WHICH INCLUDES A TIER 2 TREE. ADDITIONALLY THE ROW OF TREES ALONG THE SOUTHERN PROPERTY LINE WILL BE PRESERVED. THESE TREES ARE LOCATED ON THE ADJACENT SITE, WITH THEIR DRIP LINE OVERHANGING ONTO THE PROPERTY LINE. THE PROPOSED DESIGN WILL CREATE SPACE BY PULLING BACK FROM THE SOUTHERN PROPERTY LINE. THIS WILL ENSURE THAT THERE WILL BE NO ENCROACHMENT OF THE PROPOSED BUILDING ON THEIR ROOT ZONE.



SITE MAP WITH TREE LOCATIONS

Tree Inventory Table 416 15th Ave E

Tree ID	Scientific Name	Common Name	DB H (in)	Health Condition	Structural Condition	Tier	Proposed Action	TPZ (ft)	Dripline measurement (ft) as per survey	Notes
1	<i>Carpinus betulus</i>	European hornbeam	8	Good	Good	4	Remove			
2	<i>Carpinus betulus</i>	European hornbeam	6	Good	Good	4	Remove			
3	<i>Carpinus betulus</i>	European hornbeam	5	Good	Good		Remove			Not regulated
4	<i>Carpinus betulus</i>	European hornbeam	10	Good	Good	4	Remove			
5	<i>Carpinus betulus</i>	European hornbeam	6	Good	Good	4	Remove			
6	<i>Carpinus betulus</i>	European hornbeam	12	Good	Good	3	Remove			Replacement required
7	<i>Carpinus betulus</i>	European hornbeam	8	Good	Good	4	Retain	18' diameter	36' diameter	Protect as per SMC 25.11 See report
8	<i>Carpinus betulus</i>	European hornbeam	16	Good	Good	2	Retain	18' diameter	36' diameter	Protect as per SMC 25.11 See report
9	<i>Cercidiphyllum japonicum</i>	Katsura	12	Good	Good	3	Remove			Replacement required
10	<i>Cercidiphyllum japonicum</i>	Katsura	12	Good	Good	3	Remove			Replacement required
11	<i>Cercidiphyllum japonicum</i>	Katsura	12	Good	Good	3	Remove			Some tip die back Replacement required
12	<i>Cercidiphyllum japonicum</i>	Katsura	8	Good	Good	4	Remove			Some tip die back
13	<i>Cercidiphyllum japonicum</i>	Katsura	8	Good	Good	4	Remove			
14	<i>Cercidiphyllum japonicum</i>	Katsura	5	Good	Good		Remove			Not regulated
15	<i>Cercidiphyllum japonicum</i>	Katsura	12	Good	Good	3	Remove			Replacement required

There are 6 Hornbeam trees adjacent to the alley along the east property line in a narrow planting bed. Four are TIER 4, one is TIER 3 and one is below regulation size. There are 2 Hornbeam trees, one TIER 2 and one TIER 3, on the north property line along E. Republican also in a narrow planting bed. The TIER 2 tree (Tree #8) is recommended to be retained, and as both Tree #8 and #7 are in good health, the project proposes to retain both in our preferred design. There are 7 Katsura trees located between the building and the parking lot in a narrow and raised planting bed. Two of the Katsura are TIER 4, four are TIER 3, and one is below regulation size. All of the Katsura trees are proposed to be removed. **Confirming tree measurements were performed by Washington Tree Experts with SDCl’s Arborist on May 3, 2024. The findings of that effort were summarized in a Tree Land Use correction response. See file “3041005-LU_Tree_04.23.24_RESPONSE” for the full report.**

Arborist report prepared on March 7, 2024 by Jennifer Wells

Certified Arborist #PN6209A

ISA Qualified Tree Risk Assessor

SITE CONTEXT & URBAN DESIGN ANALYSIS

NEIGHBORHOOD DEVELOPMENT & USES



SITE CONTEXT & URBAN DESIGN ANALYSIS

NEIGHBORHOOD DEVELOPMENT & USES



(A) Kaiser Capitol Hill Campus



(B) Proposed 5 story Mixed-Use Building



(C) Capitol Hilltop Apartments (In Construction)



(D) The Gables Apartments



(E) The Ragley Apartments



(F) Anhalt Apartments



(G) Van Landingham Apartments



(H) Twin Gables Apartments



(I) Astor Court Apartments



(J) Station 7 Historic Firehouse



(K) Yardhouse Apartments



(L) Stream Fifteen Apartments

SITE CONTEXT & URBAN DESIGN ANALYSIS

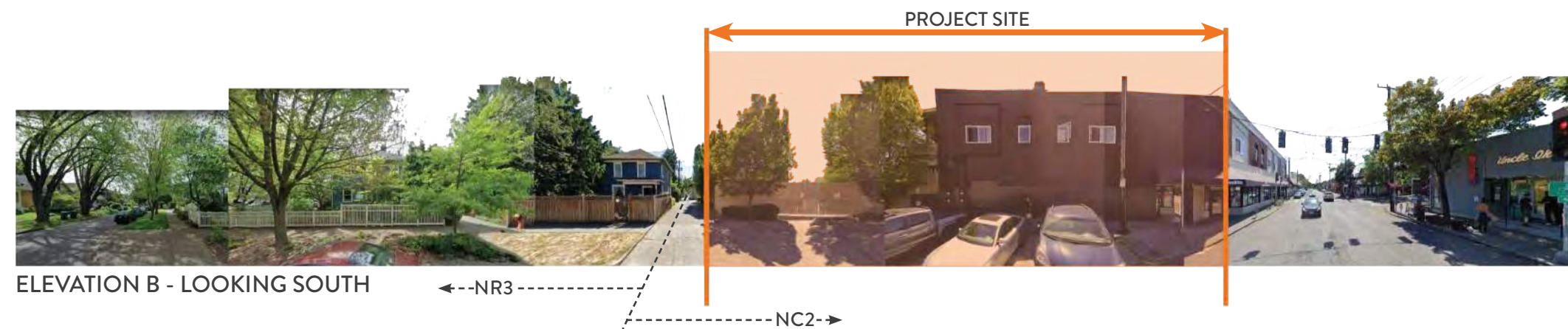
STREET ELEVATIONS - 15TH AVE E.



PEDESTRIAN ORIENTED, SMALL-SCALE RETAIL STOREFRONTS CONTRIBUTE TO WALKABLE "MAIN STREET" CHARACTER OF 15TH AVE E.

SITE CONTEXT & URBAN DESIGN ANALYSIS

STREET ELEVATIONS - E. REPUBLICAN ST



ZONE CHANGE ALONG ALLEY FROM
NEIGHBORHOOD RESIDENTIAL TO
HIGHER DENSITY MIXED USE

SITE CONTEXT & URBAN DESIGN ANALYSIS

STREET ELEVATIONS - ALLEY



ELEVATION A -
LOOKING WEST



ELEVATION B -
LOOKING EAST

NORTH - “FRONT
YARD”
(LANDSCAPED SETBACK FROM
STREET FOR RESIDENTIAL USE)

MIDDLE - “BACK YARD”
(SCREENED PRIVACY FENCES & LANDSCAP-
ING TO YARDS FOR PRIVATE RESIDENTIAL
USE)

SOUTH - “SERVICE”
(PARKING, TRASH AC-
CESS, SERVICE USES)



SITE CONTEXT & URBAN DESIGN ANALYSIS

BUILDING SETBACKS ALONG 15TH / OPEN SPACE

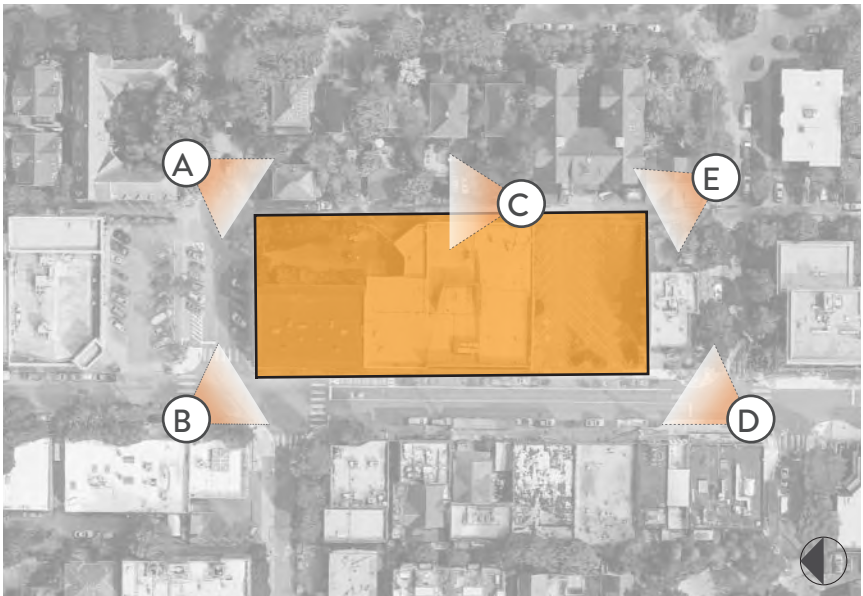


SITE CONTEXT & URBAN DESIGN ANALYSIS

SITE PHOTOS



(A) NE Corner



Key Plan



(E) SW Corner



(B) NW Corner



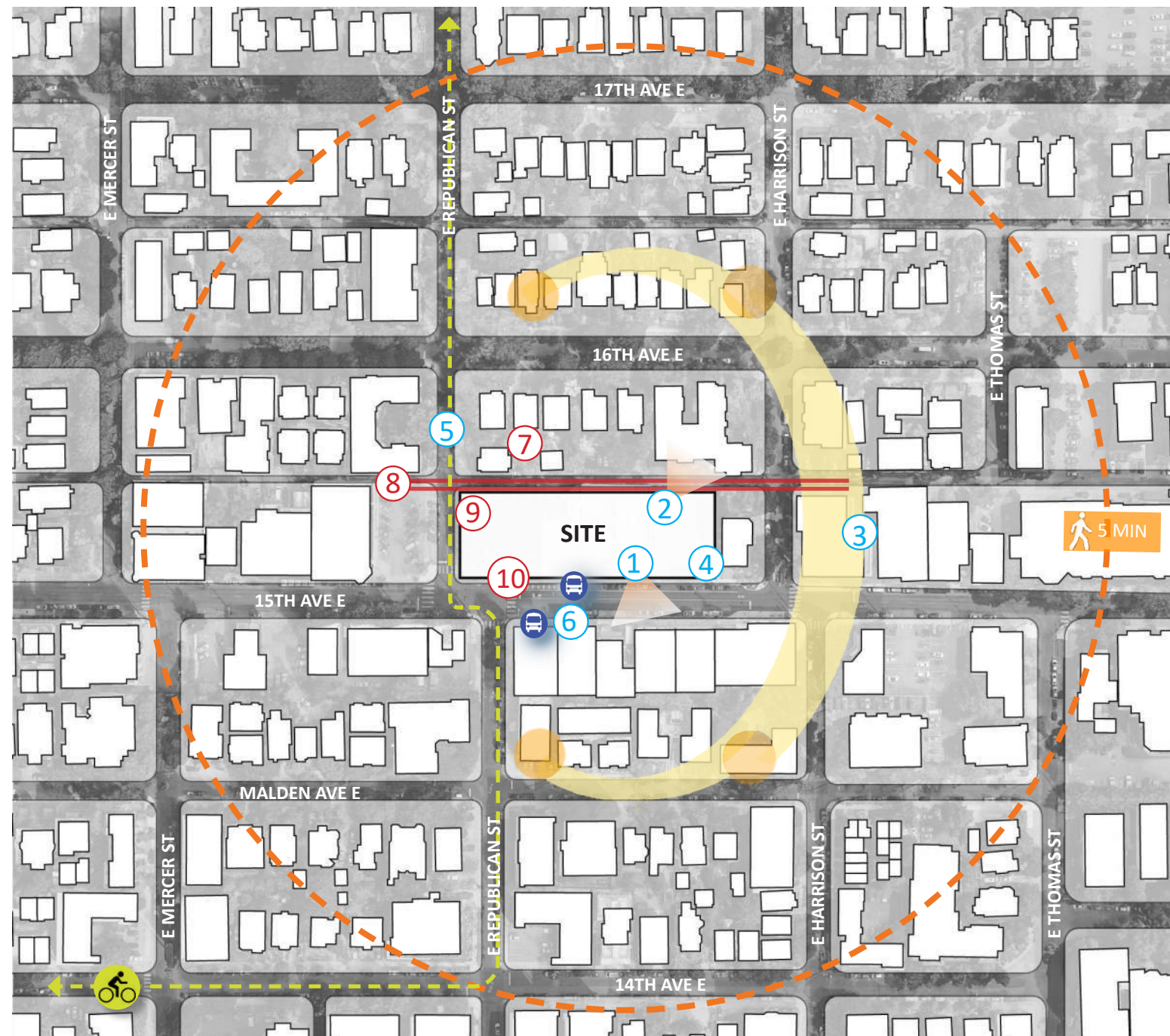
(C) Alley to the North



(D) SE Corner

SITE CONTEXT & URBAN DESIGN ANALYSIS

OPPORTUNITIES AND CONSTRAINTS



OPPORTUNITIES & CONSTRAINTS

OPPORTUNITIES

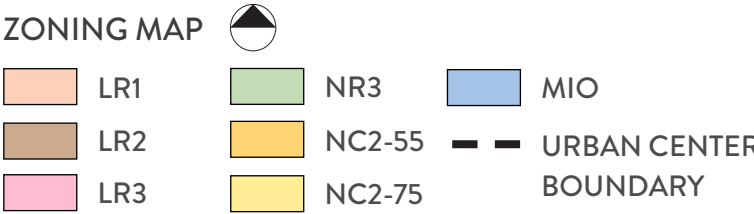
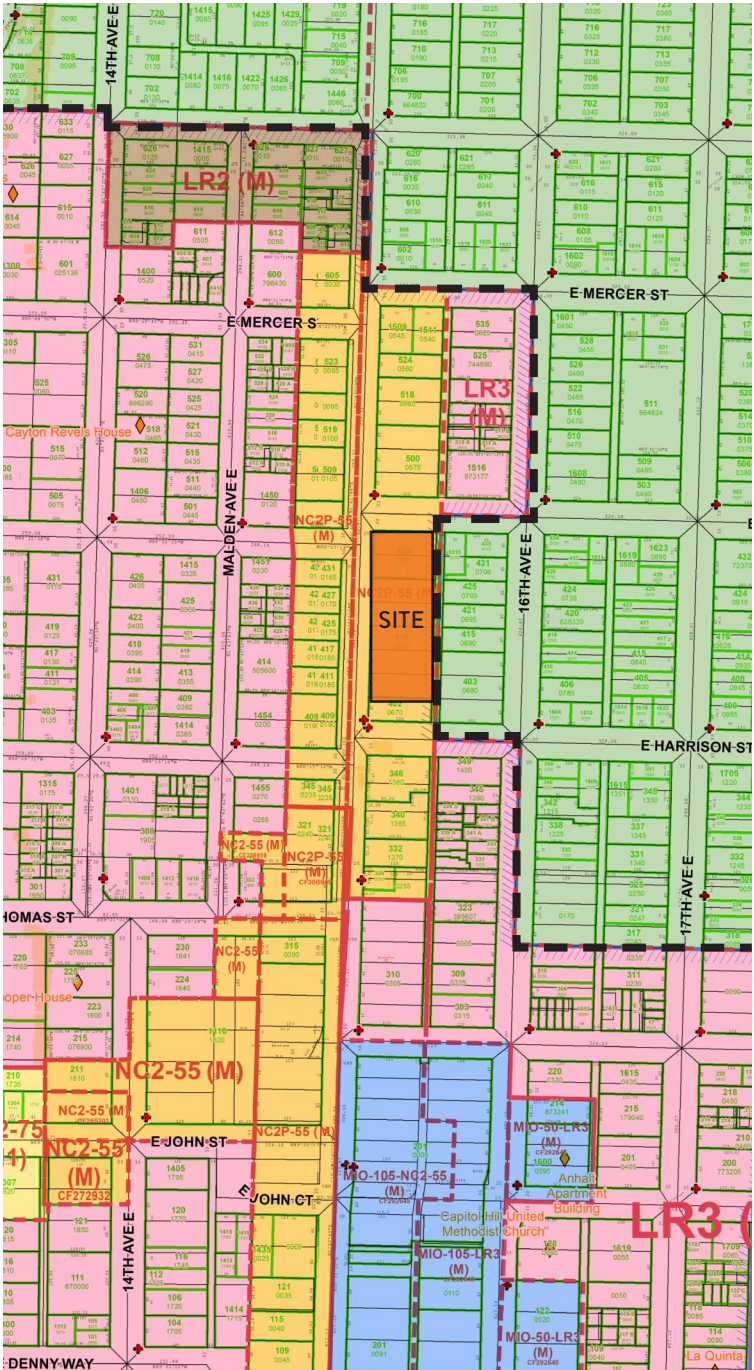
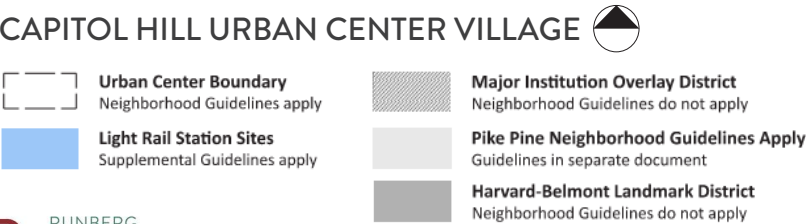
- ① Views to Downtown
- ② Views to Mount Rainer
- ③ Solar Orientation
- ④ Opportunity for highly visible corner to create connection to surroundings
- ⑤ Close to Bike Routes
- ⑥ Close to Transit Routes

CONSTRAINTS

- ⑦ Residential structures at North end of alley limit parking and trash access location
- ⑧ Zoning transition along eastern site edge from neighborhood commercial to neighborhood residential

SITE CONTEXT & URBAN DESIGN ANALYSIS

ZONING & URBAN VILLAGE MAPS

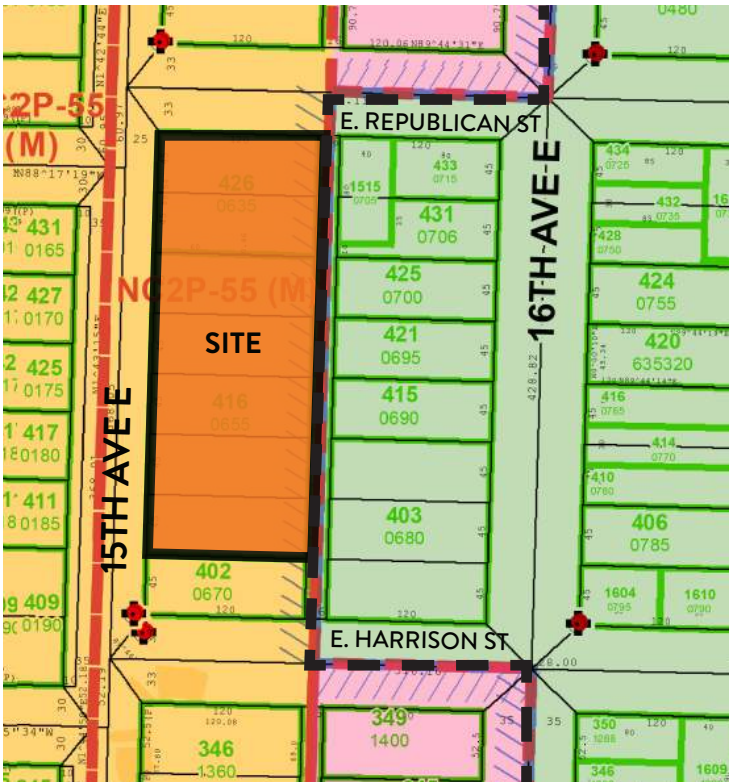


416 15TH AVE E.

ZONING SUMMARY

The project site is zoned Neighborhood Commercial 2 Pedestrian - 55' (NC2-P-55'M). The "Capitol Hill Urban Center Village" overlay applies to our site. 15th Avenue East to the west of the site is classified as "Principal Pedestrian" and "Urban Village Neighborhood". E Republican Street to the north is classified as "Urban Village Neighborhood Access".

The site is located along the eastern boundary of the Capitol Hill Urban Village. As such, the parcels adjacent to the eastern site boundary are zoned as Neighborhood Residential 3 (NR3), which supports lower density, single-family dwelling units. This zone change occurs along the adjacent alley to the east of the site, which splits the block in half.



SITE CONTEXT & URBAN DESIGN ANALYSIS


ZONING SUMMARY

APPLICABLE ZONING	SMC -SECTION	SUB-SECTION	REQUIREMENT
Permitted & Prohibited Uses	23.47A.004.A	Table A	ALL USES ARE PERMITTED OUTRIGHT, PROHIBITED, OR PERMITTED AS A CONDITIONAL USE ACCORDING TO TABLE A
	23.47A.005.C		IN A PEDESTRIAN-DESIGNATED ZONE, FACING A DESIGNATED PRINCIPAL PEDESTRIAN STREET: RESIDENTIAL USES MAY OCCUPY NO MORE THAN 20 PERCENT OF STREET FACING FAÇADE.
	23.47A.005.D		IN PEDESTRIAN-DESIGNATED ZONES, ALONG DESIGNATED PRINCIPAL PEDESTRIAN STREETS, 80 PERCENT OF THE STREET-LEVEL, STREET-FACING FAÇADE IS REQUIRED TO BE OF A USE FOUND IN 23.47A.005.D.1
Structure Height	23.47A.012.A		STRUCTURES MAY NOT EXCEED THE APPLICABLE HEIGHT LIMIT (55'), EXCEPT AS OTHERWISE PROVIDED IN SECTION 23.47A.012.
Floor Area Ratio	23.47A.013	Table A	MAXIMUM FAR FOR 55 FEET HEIGHT LIMIT IS 3.75
Setback Requirements	23.47A.014.B.1		FOR LOTS ABUTTING THE ALLEY FROM RESIDENTIAL ZONES, SETBACK IS REQUIRED WHERE A LOT ABUTS THE INTERSECTION OF A SIDE LOT LINE AND FRONT LOT LINE OF A LOT IN A RESIDENTIAL ZONE. THE REQUIRED SETBACK FORMS A TRIANGULAR AREA. TWO SIDES OF THE TRIANGLE EXTEND ALONG THE STREET LOT LINE AND SIDE LOT LINE 15 FEET FROM THE INTERSECTION OF THE RESIDENTIALLY ZONED LOT'S FRONT LOT LINE AND THE SIDE LOT LINE ABUTTING THE RESIDENTIALLY ZONED LOT.
	23.47A.014.B.3		AN UPPER-LEVEL SETBACK IS REQUIRED ALONG ANY REAR OR SIDE LOT LINE THAT IS ACROSS AN ALLEY FROM A LOT IN A NEIGHBORHOOD RESIDENTIAL ZONE. THE SETBACK SHALL BE 1) 15 FEET FOR PORTIONS OF STRUCTURES ABOVE 13 FEET IN HEIGHT TO A MAXIMUM OF 40 FEET AND 2) FOR EACH PORTION OF A STRUCTURE ABOVE 40 FEET IN HEIGHT, ADDITIONAL SETBACK AT THE RATE OF 3 FEET OF SETBACK FOR EVERY 10 FEET.
	23.47A.014.D		FOR STRUCTURES WITH A WIDTH OF MORE THAN 250 FEET, AT LEAST ONE PORTION OF THE STRUCTURE 30 FEET OR GREATER IN WIDTH MUST BE SET BACK A MINIMUM OF 15 FEET FROM THE FRONT PROPERTY LINE.
Street Level Standards	23.47A.008.A.2		BLANK SEGMENTS OF THE STREET-FACING FACADE BETWEEN 2 FEET AND 8 FEET ABOVE THE SIDEWALK MAY NOT EXCEED 20 FEET IN WIDTH. THE TOTAL OF ALL BLANK FACADE SEGMENTS MAY NOT EXCEED 40 PERCENT OF THE WIDTH OF THE FACADE OF THE STRUCTURE ALONG THE STREET.
	23.47A.008.B.2.A		SIXTY PERCENT OF THE STREET-FACING FACADE BETWEEN 2 FEET AND 8 FEET ABOVE THE SIDEWALK SHALL BE TRANSPARENT.
	23.47A.008.C.4		CONTINUOUS OVERHEAD WEATHER PROTECTION (I.E., CANOPIES, AWNINGS, MARQUEES, AND ARCADES) IS REQUIRED ALONG AT LEAST 60 PERCENT OF THE STREET FRONTAGE OF A STRUCTURE ON A PRINCIPAL PEDESTRIAN STREET
Amenity Area	23.47A.024.A		AMENITY AREAS ARE REQUIRED IN AN AMOUNT EQUAL TO 5 PERCENT OF THE TOTAL GROSS FLOOR AREA IN RESIDENTIAL USE.
Parking Access	23.47A.032.A		ACCESS TO PARKING SHALL BE FROM THE ALLEY IF THE LOT ABUTS AN ALLEY IMPROVED TO THE STANDARDS OF SUBSECTION 23.53.030.C
Parking Count	23.54.015	Table A &B	RESIDENTIAL & NON-RESIDENTIAL USES IN URBAN CENTERS HAVE NO MINIMUM REQUIREMENT.
Bicycle Parking	23.54.015	Table D	EATING & DRINKING ESTABLISHMENTS: 1 PER 5,000 SF (LONG TERM), 1 PER 1,000 SF (SHORT TERM) SALES & SERVICES, GENERAL: 1 PER 4,000 SF (LONG TERM), 1 PER 2,000 SF (SHORT TERM) MULTI-FAMILY STRUCTURES: 1 PER DWELLING UNIT FOR THE FIRST 50, .75 PER DWELLING
Solid Waste & Recyclable Storage	23.54.040	Table A	RESIDENTIAL WITH MORE THAN 100 UNITS: 575 SF + (4 SF *(DWELLING COUNT -100)
	23.54.040.B		IN MIXED USE DEVELOPMENTS, STORAGE SPACE FOR GARBAGE MAY BE SHARED BETWEEN RESIDENTIAL AND NON-RESIDENTIAL USES, BUT SEPARATE SPACES FOR RECYCLING SHALL BE PROVIDED.
Design Review Departures to Preserve Trees	23.41.012.B.10		FLOOR AREA RATIOS (FAR), EXCEPT THAT: DEPARTURES OF UP TO AN ADDITIONAL 0.5 FAR MAY BE GRANTED IF THE APPLICANT DEMONSTRATES THAT: 1) THE DEPARTURE IS NEEDED TO PROTECT A TREE THAT IS LOCATED ON THE LOT THAT IS EITHER AN EXCEPTIONAL TREE, AS DEFINED IN SECTION 25.11.020, OR A TREE GREATER THAN 2 FEET IN DIAMETER MEASURED 4.5 FEET ABOVE THE GROUND; AND 2) AVOIDING DEVELOPMENT IN THE TREE PROTECTION AREA WILL REDUCE THE TOTAL DEVELOPMENT CAPACITY OF THE SITE.
	23.41.012.B.11		STRUCTURE HEIGHT EXCEPT THAT DEPARTURES OF UP TO 10 FEET OF ADDITIONAL HEIGHT MAY BE GRANTED IF THE APPLICANT DEMONSTRATES THAT: 1)THE DEPARTURE IS NEEDED TO PROTECT A TREE THAT IS LOCATED ON THE LOT THAT IS EITHER AN EXCEPTIONAL TREE, AS DEFINED IN SECION 25.11.020, OR A TREE GREATER THAN 2 FEET IN DIAMETER MEASURED 4.5 FEET ABOVE THE GROUND; AND 2) AVOIDING DEVELOPMENT IN THE TREE PROTECTION AREA WILL REDUCE THE TOTAL DEVELOPMENT CAPACITY OF THE SITE.

DESIGN GUIDELINES

CITY OF SEATTLE AND CAPITOL HILL EDG RESPONSE

EDG 1 RECOMMENDATIONS	APPLICANT RESPONSE	EXHIBIT ON
<div>1</div> <div>MASSING</div>		
<p>1a. The Board unanimously supported the applicant’s preferred massing, the ‘S’ Scheme (Option C), that was also generally supported by public commenters. The massing option provides major modulations at the upper levels on each of the long west and east facades, breaking down the building scale along street-facing frontage on 15th Ave as well as along the zone transition to the Neighborhood Residential along the alley. [CS2-D Height, Bulk, and Scale, CS2-C Relationship to the Block]</p> <p>i. The Board supported the 10 foot setback along the south property line that provides space around the older Fire House structure on the adjacent site. [CS2-B Adjacent Sites, Streets, and Open Spaces]</p> <p>ii. The Board supported the 2 foot setback at the alley at the ground level that widens the paved vehicle access. On the alley facade, they supported the 8 foot setback at the building’s lower levels, as well as the additional 6 foot setback at the upper levels that steps the mass back at the zone transition [CS2-D-3. Zone Transitions]</p> <p>iii. The Board discussed the setback at the northeast corner along Republican St where a Tier 2 and Tier 4 tree are proposed to be retained. The Board commented on the idea that this setback provides a zone transition, as noted in the EDG packet, observing that this setback would only be evident along the northern street edge. [CS2-D-3. Zone Transitions]</p> <p>iv. The Board had concerns that adding a floor level (as proposed in the departure requests) negatively affected the overall development’s ability to create an adequate massing transition at 15th Ave E and at the corner of 15th Ave E and E Republican St. [CS2-D Height, Bulk, and Scale, CS2-1-c: 15th Avenue Corridor, DC2-A Massing]</p>	<p>iii. The design has maintained the south, east, and north property line setbacks as presented in our preferred Option C at EDG. To address CS2-D-3 Zone Transitions, the project sets back 10’-0” at grade along the alley and an additional 6’-0” above level 4. These setbacks add up to 16’ of separation between the NC2 and NR3 zones that meet along the alley centerline. Additionally, the preservation of two trees along the north property line creates a setback from ground to sky that pushes the street facing facade over 22’-0” back from the north property line, creating a setback that is nearly double the depth of the front yard setback of the adjacent single-family home just east of the alley. This setback creates a significant massing break along the north facade, breaking the building into two distinct masses along E Republican Street. The zone transition is further eased by preserving the existing trees (continuing the established tree canopy east of the alley to the west), and aligning the sidewalk and curb street improvements with the existing conditions east of the alley. The massing break created by the extensive setback at E Republican Street coupled with the alley setbacks at grade and upper stories creates an intentional massing setback at the NE corner of the project that opens up light, air and visibility for the NR3 properties east of the alley. [CS2-B Adjacent Sites, Streets, and Open Spaces, CS2-D-3 Zone Transitions, DC2-A Massing, DC2.B.2 Blank Walls]</p> <p>iv. To address the Board’s concerns about the additional floor level’s impact on the massing transitions along 15th Ave E and E Republican Street, the design has been modified since the EDG presentation to create significant setbacks at the NW corner and mid-block to pull the 6th story massing back from the street edges and ease the massing transition, particularly at the prominent corner at the intersection of E Republican Street and 15th Ave E. These added setbacks can be seen on the sixth-floor plan, upper roof plan, and the North and West elevation drawings. [CS2-D Height, Bulk, and Scale, CS2-1-c: 15th Avenue Corridor, DC2-A Massing]</p>	<div>Page: 24-39</div>

EDG 1 RECOMMENDATIONS		APPLICANT RESPONSE	OPTION C (PREFERRED MASSING AT EDG)
<div>1</div> <div>MASSING</div>			
<p>1b. The Board acknowledged that during the Master Use Permit (MUP) review, SDCI will determine whether the project can request the departures for extra height and Floor Area Ratio (FAR), related to retention of the Tier 2 tree. The Board gave guidance that could apply to various designs, depending on the SDCI determination and the applicant’s choices. <i>[CS2-D Height, Bulk, and Scale, CS2-D-3. Zone Transitions, CS2-1-c: 15th Avenue Corridor, DC2-A Massing]</i></p> <p>i. If the proposal is eligible for the design review departure for extra height and FAR, the Board requested the following information in the Recommendation packet:</p> <p>I. Studies investigating a partial 6th story, using the extra height where most effective for the architectural expression. For example, potentially using the additional height to differentiate masses at specific locations.</p> <p>II. Studies reducing the scale along the 15th Ave E frontage and at the corners where the height is most visible.</p> <p>III. Studies of the northwest corner, focusing on visual impacts of additional height at this corner and the potential for using the mass to emphasize wayfinding. The Board discussed the massing at the corner of 15th Ave and Republican St location and discussed how the mass could provide wayfinding. They noted any study of extra height needed to be assessed for visual impacts at this corner, especially as it is viewed along the 15th Ave corridor. Staff notes that eye-level perspective views along the 15th Ave corridor, as well as from Republican St, will be helpful at the Recommendation phase to assess visual impacts if massing along the street frontages.</p> <p>IV. Studies of other enhancements such as tree retention or additional landscaping at the ground level. As a suggestion, the Board noted that retaining more trees along the northeast portion of the alley would be an added neighborhood benefit.</p> <p>ii. If the proposal is not eligible for the design review departure for extra height and FAR, then the Board noted it was their preference to continue to preserve the trees at the north edge and shift the massing elsewhere on site, to emphasize the architectural expression, especially along 15th Ave E.</p> <p>iii. If the applicant modifies the design to remove the Tier 2 and 3 trees at the north property line, the Board requested that tree replacement occur somewhere else on the site.</p>		<p>1b. Noted. Currently the project is pursuing departures to height and FAR for SDCI’s review. We will provide studies as requested above in the DRB recommendation packet after receiving confirmation the departure requests are valid with our first round of MUP corrections. In addition to added setbacks at level 6, the preferred design provides significant new green space and trees along the alley to support the zone transition. If the departures are not supported by SDCI, the massing will change to recoup the area occupied by the existing trees.</p>	<div></div> <div><div><div>E REPUBLICAN ST</div><div>15TH AVE E</div><div>ALLEY</div><div>RES. ENTRY</div><div>GARAGE ENTRY</div></div><div><div>THE “S” SCHEME</div><ul style="list-style-type: none">• Preserves Tier 2 tree along E Republican St.• Larger plaza along 15th Ave E breaks up retail facade and provides additional corner retail units with adjacent open space.• North podium deck provides more daylight and spatial relief to neighboring homes.• Voluntary setback at south property line creates a pedestrian through block opportunity while giving the firehouse building and plantings more space.• Provides voluntary setback at the alley, increasing circulation width by 2’ and providing substantial setbacks from the neighborhood zone.• Activates alley with residential use.<div>CONS<ul style="list-style-type: none">• Requires departures for extra FAR and height to preserve Tier 2 tree.• Proposes canopy and parking departures</div><div><div>FAR = 4.03 (Reduced since EDG)</div><div>Residential Unit Count = 172</div><div>Total Parking Stalls = 101 stalls</div></div></div></div>

DESIGN GUIDELINES

CITY OF SEATTLE AND CAPITOL HILL EDG RESPONSE

EDG 1 RECOMMENDATIONS	APPLICANT RESPONSE	EXHIBIT ON
<div>2</div> <div>ARCHITECTURAL LAYOUT</div>		
<p>2a. The Board supported the setback along the south property line which effectively creates an additional ‘corner’ condition, creating activation of both building edges and providing extra visibility to the commercial unit. The Board recommended that access to the commercial unit at the corner should be provided on both the west and south facades to activate the open space. [CS2-1-c: 15th Avenue Corridor, CS2-C Relationship to the Block]</p> <p>i. The Board supported the location of access to bike parking along the south facade that could activate the open space. They specifically supported direct access to a bike storage room on the ground level that would be convenient for residents’ use. The Board discussed that the entry to the bike parking could also work if located off the Republican St. frontage as this is adjacent to the neighborhood greenway. [PL4-2 Planning Ahead for Bicyclists]</p>	<p>2a. The setback along the south property line remains as presented at EDG. This will activate the south edge, increase visibility for the commercial tenant, and eliminate extensive blank walls facing the historic Fire House building adjacent. It will also activate the space by providing pedestrian passage from the street to the alley, residential egress, and access to bike parking. The developed master plan for the block that has been developed with the neighboring tenants at the fire house will explore the programming along the through block connection. [CS2-1-c: 15th Avenue Corridor, CS2-C Relationship to the Block, PL4-2 Planning Ahead for Bicyclists]</p>	<div>Page: 52-55</div>
<p>2b. The Board supported the location of townhouses at the alley. Although they generally agreed that the double-height units reduced the feeling of being below ground, the Board did not support the below grade relationship as shown and encouraged the applicant to raise the level of the units to be on grade with the alley. The Board requested a study at the Recommendation phase of review showing how individual entrances to the townhomes along the alley could be provided to activate that frontage. [PL3-1-c. Ground-Floor Units, PL3-2 Residential Edges]</p> <p>i. The Board supported having an entry door on the alley for resident use but noted that visibility and wayfinding to the entry door should be enhanced as the design moves forward. [PL3-1-b. Residential Buildings]</p>	<p>2b. The project is striving to maintain the 13’-0” floor to floor requirement along 15th Ave E to ensure maximum viability for the commercial units along the pedestrian street frontage [PL3.4 Retail Edges], while also maintaining viable ceiling heights in the residential floors above. The door elevations along 15th are what determine the floor elevation of the Level 2 transfer slab, which in turn determines how low the first floor of the TH units need to be along the alley to have enough height to accommodate a mezzanine loft. This means the elevation of the ground floor level of the alley-related housing needs to be below adjacent alley grade; the only practical alternative would be to revise these units to flats as pushing the mezzanines up to the level 2 floor level would require complicated vertical interlocking of the Type IA and Type VA construction types, as well as excessive stair requirements that would take up a lot of space within the units. Flats along an alley condition are not conducive to keeping blinds open or a feeling of safety and security for the residents. Therefore, the project believes locating the ground floor elevation below adjacent alley grade and maintaining the two-story layout is a better solution than eliminating the loft conditions altogether. Since the time of the EDG the design has been refined further to work with grading requirements all around the site, and we were able to raise the ground floor elevation 6” higher, thereby reducing the offset in grade between the unit patios and the alley, but still maintaining a grading variation to provide a visual and physical transition between the street level and individual residences. [PL3-2.c Street Level Interaction] We studied the ability to provide exterior access to the units from the alley (so the residents can go straight into their units from the exterior) but for many of the units the number of stair risers required is prohibitive. At grade connection is feasible for the southernmost units, as they are close in elevation to the existing alley grade and access may be accommodated with a couple of risers. In those units we have revised the private patios to landscaped space to further increase trees and greenery along the alley. [CS14.a Plants and Habitat] We will illustrate this condition with diagrams in the DRB Recommendation packet for Board review. Finally, we agree with the Board’s suggestion that visibility and wayfinding to the common residential entry door along the alley is important, and we see it as an opportunity for another design feature to activate the alley frontage. Since the time of the EDG the design has evolved to incorporate storefront glazing, overhead weather protection, landscaping, signage and lighting. [PL3-1.b Residential Buildings]</p>	<div>Page: 56-61</div>

EDG 1 RECOMMENDATIONS	APPLICANT RESPONSE	EXHIBIT ON
2 ARCHITECTURAL LAYOUT		
2c. On the 15th Ave E facade, the Board encouraged the enhanced development of the commercial streetscape, including integration of the bus stop. The Board suggested integrating recessed entry doors at the commercial unit entries, as is typical of other commercial structures in the neighborhood. [CS2-1-c: 15th Avenue Corridor, PL3-4 Retail Edges]	2c. Agreed. Please see drawings 2+3/G1.9, A3.1, A3.3 to see how the commercial storefronts have developed since the time of the EDG presentation. All commercial spaces now feature recessed entries. The section of storefront adjacent to the bus stop has been set back an additional 3'-0" from the back of sidewalk to provide even more pedestrian space at this portion of the facade. A variety of storefront styles, sizes and operability are proposed to help break up the experience along the streetscape, similar to the existing conditions along the west side of 15th Ave E. Overhead weather protection has been provided as well. [CS2-1-c: 15th Avenue Corridor, PL3-4 Retail Edges]	Page: 40-51
3 ARCHITECTURE: SECONDARY ARCHITECTURAL FEATURES		
3a. The Board supported providing overhead weather protection as an amenity for the pedestrian realm. They specifically supported overhead weather protection at the entries of commercial units and suggested incorporating recessed entries with design of overhead weather protection to enhance wayfinding. They also noted that sufficient overhead water protection should be included near the bus stop. [PL2-3 Weather Protection, CS2-1-c: 15th Avenue Corridor, DC2-3 Secondary Architectural Features, PL3-1-a. Commercial Areas]	3a. See 2c response regarding recessed entries and the pedestrian conditions at the bus stop. Additionally, the design has incorporated the Board's feedback in the development of the overhead weather protection along 15th Ave E. The current design proposes 78.12% of the west facade will receive overhead weather protection as required by 23.47A.008.C.4.	Page: 40-51, 84
3b. The Board suggested integrating a detail of depth at windows to increase texture on the facades that can help to reduce perception of bulk. [DC2-3 Secondary Architectural Features]	3b. Since the time of the EDG the building design has developed distinct architectural languages for each of the primary building masses along the street facing facades. The design team refers to the building mass south of the entry court as the "south building" and the building masses north of the entry court as the "north building." These two "buildings" will receive complimentary but different architectural expressions to further break down the massing of the building along 15th Ave E. [CS2-D Height, Bulk, and Scale, CS2-D-3. Zone Transitions, CS2-1-c: 15th Avenue Corridor, DC2-A Massing] Within these architectural expressions there are different material applications that provide depth and visual interest both at ground level and at the residential facades above. See drawings DR2.1, DR 2.2, DR2.3 and A3.1, A3.2, A3.3 to see how brick is proposed to be applied as a continuous facade with punched openings on the "south building" upper stories, and as a frame element with more depth and shadow at the two corner volumes of the "north building." Bolt-on balconies at levels 3-6 and canopies or eyebrow projections at the top of the building create additional visual interest and reduce the perception of bulk. [DC2-3 Secondary Architectural Features]	Page: 24-39

DESIGN GUIDELINES

CITY OF SEATTLE AND CAPITOL HILL EDG RESPONSE

EDG 1 RECOMMENDATIONS	APPLICANT RESPONSE	EXHIBIT ON
<div>4 SITE</div>		
<p>4a. The Board supported the creation of public open space at the setback along the south property line, noting this would create a valuable open space resource for the neighborhood. [PL1-3 Walkways and Connections]</p> <p>i. The Board expressed concern with potential safety issues at the southeast corner of the building, with a pedestrian path at a blind corner next to the parking garage entry. The Board recommended that the applicant study visibility at this location to ensure that safety for pedestrians is considered in the design. [DC1-B-1. Access Location and Design]</p> <p>ii. The Board supported creating a relationship between the design of the pedestrian pass-through and the adjacent open spaces of the Fire House site. They specifically noted that reducing vertical screening and fencing could create more visual connections through these spaces and increase safety. [PL1-A-1. Enhancing Open Space, PL2-B Safety and Security]</p>	<p>i. The design team agrees with the Board’s concern about potential safety issues at the SW corner of the building. Since the time of the EDG the design has developed to extend the planter at the end of the throughblock path to protect pedestrians from cars as they enter the alley. See DR 2.2, DR2.3 and A1.2, A3.2, A3.3 to see how the design has developed since EDG. [DC1-B-1. Access Location and Design]</p> <p>ii. Since EDG the team continues to coordinate with the adjacent property Owner to consider opportunities to create connections between our through block connection and the backyard of the fire house. Current design locates a landing in the ramp at a location that could provide pedestrian access to the backyard space. If physical connection isn’t feasible, the project plans to provide visual connections between the spaces. [PL1-A-1. Enhancing Open Space, PL2-B Safety and Security] Our team has developed a landscape master plan for our half of the block, which includes phased improvements to the front courtyard and rear yard of the Fire Station building.</p>	<p>Page: 52-55, 62</p>
<p>4b. On the 15th Ave E facade, the Board encouraged the development of an enhanced streetscape design, including designing for safety, with amenities and overhead weather protection at the bus stop. The Board encouraged incorporating as many street trees as possible to enhance the pedestrian experience. [PL1-3 Walkways and Connections]</p>	<p>4b. See narrative responses to items 2c and 3a above. Additionally, the project is coordinating the maximum number of street trees feasible with SDOT via the SIP process. See drawings DR1.1 and Landscape plans.</p>	<p>Page: 40-51</p>
<p>4c. The Board supported retaining the Tier 2 tree along the north property line. The Board noted general support for retaining mature trees wherever possible, and suggested retaining more of the mature trees along the northeast property edge, if feasible. The Board also commented that if the Tier 2 tree is removed, they encouraged replacement tree planting on the site. [CS1-4-e. Tree Canopy, CS1-D-1. On-Site Features]</p>	<p>4c. See response to item 1b above. The team has evaluated the ability to preserve additional trees along the alley but given the impact on the below grade garage the inefficiency generated is prohibitive; the project maintains that pushing parking below grade to provide housing along the alley and avoid blank wall conditions is a significant benefit to the neighborhood and has a greater impact on the zone transition then maintaining the existing tree canopy. [CS2.A.2 Connection to Street, DC1.C.1 Below Grade Parking, DC1.1 Location and Design of Uses, DC2.B.2 Blank Walls] The project does agree with the suggestion of replacement planting and is proposing to add 32 trees along the alley, both at grade and on the podium decks facing east. This added tree canopy and landscaping will ease the zone transition. See drawings DR1.2, DR2.2, A1.2 and the landscape plans for more detail.</p>	<p>Page: 62-66, 86-92</p>
<p>4d. The Board requested that the applicant integrate street lighting along the 15th Ave E corridor with other aspects of the street facade and sidewalk furniture, to enhance the design of the overall redevelopment of the streetscape. [DC4-C-1. Functions]</p>	<p>4d. Agreed. See drawings DR1.1 and DR2.1 for current design development of the 15th Ave E street facade lighting and proposed sidewalk furniture, such as bike racks and the bus stop elements. A full lighting plan will be presented to the Board at the DRB Recommendation meeting. [DC4-C-1. Functions]</p>	<p>Page:68</p>

BUILDING MASSING

The Board liked the “S” massing scheme noting it provides major modulations at the upper levels on each of the long west and east facades, breaking down the building scale along street-facing frontage on 15th Ave as well as along the zone transition to the Neighborhood Residential along the alley. At EDG the Board recommended the following:

- To study the northwest corner, focusing on visual impacts of additional height at this corner and the potential for using the mass to emphasize wayfinding. Address the concerns that adding a floor level (as proposed in the departure requests) negatively affects the overall development’s ability to create an adequate massing transition at 15th Ave E and at the corner of 15th Ave E and E Republican St.
- Use the extra height where most effective for the architectural expression.
- Integrate a detail of depth at windows to increase texture on the facades that can help to reduce perception of bulk.

15TH AVE STREETSCAPE

The Board supported the proposed commercial activation along 15th Ave E and asked to see further development of this streetscape at the Recommendation Meeting. Specifically:

- Study the setback along the south property line which effectively creates an additional ‘corner’ condition, creating activation of both building edges and providing extra visibility to the commercial unit.
- Enhanced development of the commercial streetscape, including integration of the bus stop. Consider integrating recessed entry doors at the commercial unit entries, as is typical of other commercial structures in the neighborhood.
- Consider canopy coverage and weather protection as a pedestrian amenity. Incorporate recessed entries with design of overhead weather protection to enhance wayfinding.
- Incorporate as many street trees as possible to enhance the pedestrian experience.

NORTH FACADE & ZONE TRANSITION

The Board requested further study of the north facade along E Republican St., directing the Design Team to consider how the design responds to the Zone transition along that facade.

- Demonstrate how preserving the trees and setting the building back at the NE corner supports the zone transition.
- Consider new trees along the alley facade to replace lost canopy along that frontage.

ALLEY TOWNHOUSE FRONTAGE

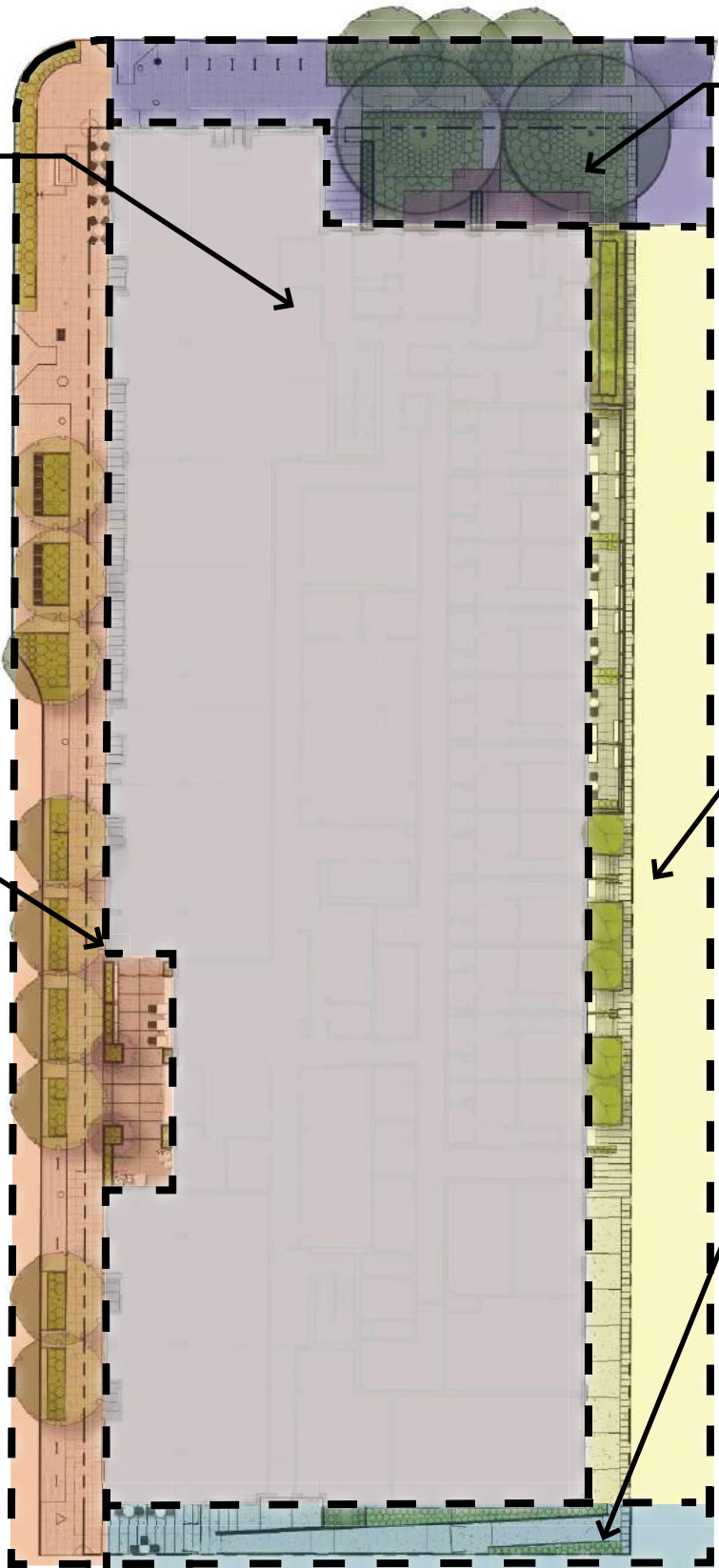
The Board supported the proposed residential use along the alley frontage. They requested the following studies for Recommendation Meeting review:

- A study for how individual entrances to the townhomes along the alley could be provided to activate that frontage.
- The Board supported having an entry door on the alley for resident use but noted that visibility and wayfinding to the entry door should be enhanced as the design moves forward.

THROUGH BLOCK DEVELOPMENT

The Board supported the creation of public open space at the setback along the south property line, noting this would create a valuable open space resource for the neighborhood. The Board requested the following for Recommendation review:

- Study visibility at the southeast corner of the building, to avoid a pedestrian path at a blind corner next to the parking garage entry.
- Create a relationship between the design of the pedestrian pass-through and the adjacent open spaces of the Fire House site. Examine reducing vertical screening and fencing to create more visual connections and increase safety.



BUILDING MASSING

MASSING DEVELOPMENT SINCE EDG



SW MASSING AT EDG



SW MASSING AT DRB



NE MASSING AT EDG



NE MASSING AT DRB

Since the time of the EDG the building has developed to break down the massing scale, reduce the impact of the additional height, and respond to the surrounding neighborhood context. The building massing development includes:

- ① Preserving the trees along East Republican Street to create a significant setback and ease the zone transition to the east. [CS2-D-3, CS1-4, DC2-A]
- ② Breaking down the mass to express as two buildings by changing facade material and color between the South and North massings on either side of the entry courtyard. [CS2-D, CS2-C]
- ③ Breaking the mass of the north building into three distinct volumes, with massing defined at either ‘corner’. [CS2-1-C, PL3-4, DC2-1]
- ④ Setting back the top floor at the corners of the north building to reduce height, bulk, and scale at the intersection with East Republican Street and along 15th Avenue East. [CS2-D, DC2-A, CS2-1-C]
- ⑤ The voluntary setback along the south property line creates a 15’ gap that gives space for the Fire House building to remain exposed on all four sides and creates pedestrian space at grade to help activate the pedestrian realm. [CS2-1-C, CS2-C, CS2-D, PL1-3-A]
- ⑥ The east facade steps back on the top two floors, and has significant setbacks from the property line at grade to help ease the zone transition along the alley. [DC2-A, CS2-D-3]
- ⑦ Individualized storefronts and overhead weather protection, along with recessed facades and entries, create variety at a human scale for an enhanced pedestrian experience along 15th Avenue East. [PL3-4, CS2-1-C]

BUILDING MASSING

MASSING DEVELOPMENT SINCE EDG



SW MASSING AT EDG



SW MASSING AT DRB



NW MASSING AT EDG



NW MASSING AT DRB

Since the time of the EDG the building has developed to break down the massing scale, reduce the impact of the additional height, and respond to the surrounding neighborhood context. The building massing development includes:

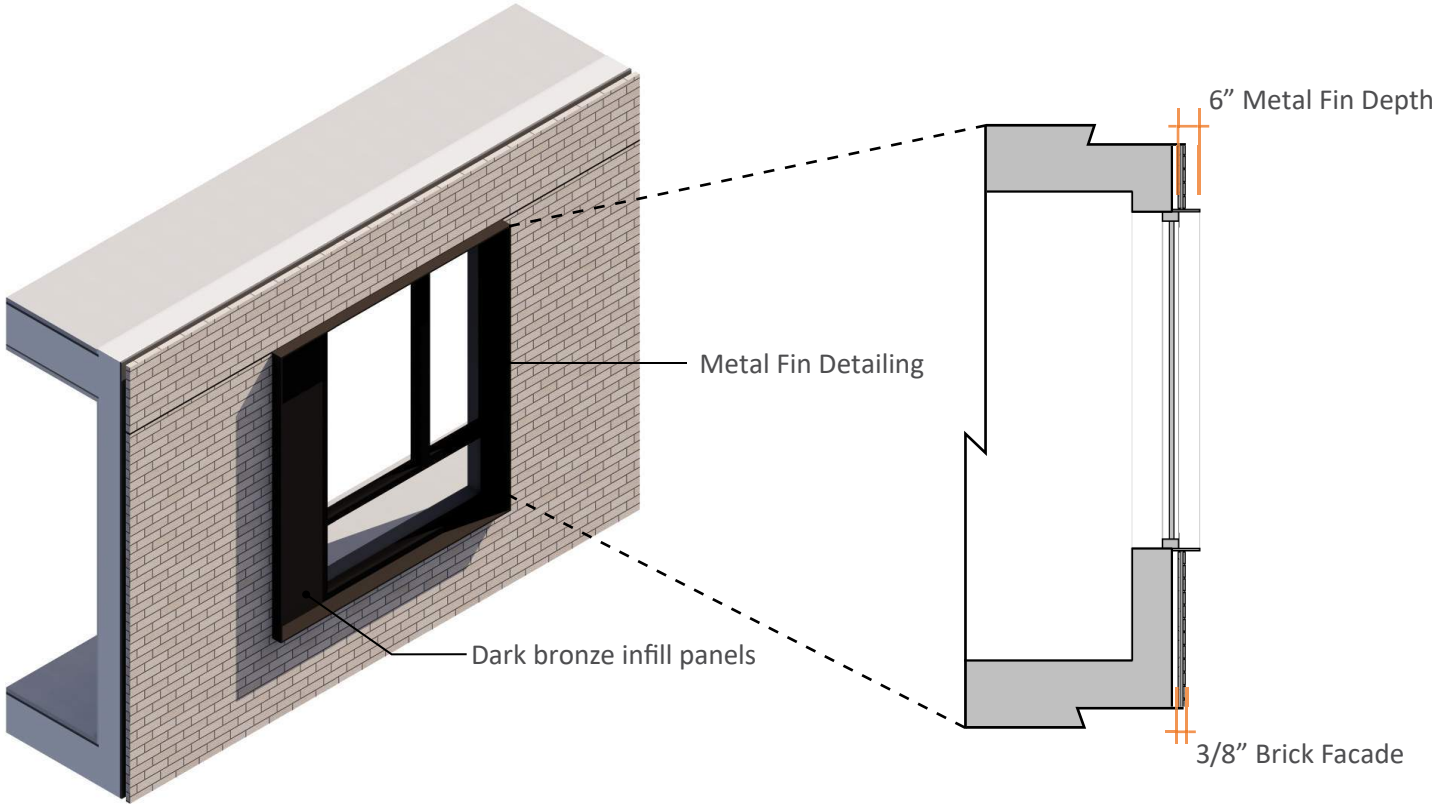
- ① Preserving the trees along East Republican Street to create a significant setback and ease the zone transition to the east. [CS2-D-3, CS1-4, DC2-A]
- ② Breaking down the mass to express as two buildings by changing facade material and color between the South and North massings on either side of the entry courtyard. [CS2-D, CS2-C]
- ③ Breaking the mass of the north building into three distinct volumes, with massing defined at either 'corner'. [CS2-1-C, PL3-4, DC2-1]
- ④ Setting back the top floor at the corners of the north building to reduce height, bulk, and scale at the intersection with East Republican Street and along 15th Avenue East. [CS2-D, DC2-A, CS2-1-C]
- ⑤ The voluntary setback along the south property line creates a 15' gap that gives space for the Fire House building to remain exposed on all four sides and creates pedestrian space at grade to help activate the pedestrian realm. [CS2-1-C, CS2-C, CS2-D, PL1-3-A]
- ⑥ The east facade steps back on the top two floors, and has significant setbacks from the property line at grade to help ease the zone transition along the alley. [DC2-A, CS2-D-3]
- ⑦ Individualized storefronts and overhead weather protection, along with recessed facades and entries, create variety at a human scale for an enhanced pedestrian experience along 15th Avenue East. [PL3-4, CS2-1-C]

BUILDING MASSING

MASSING DETAILS



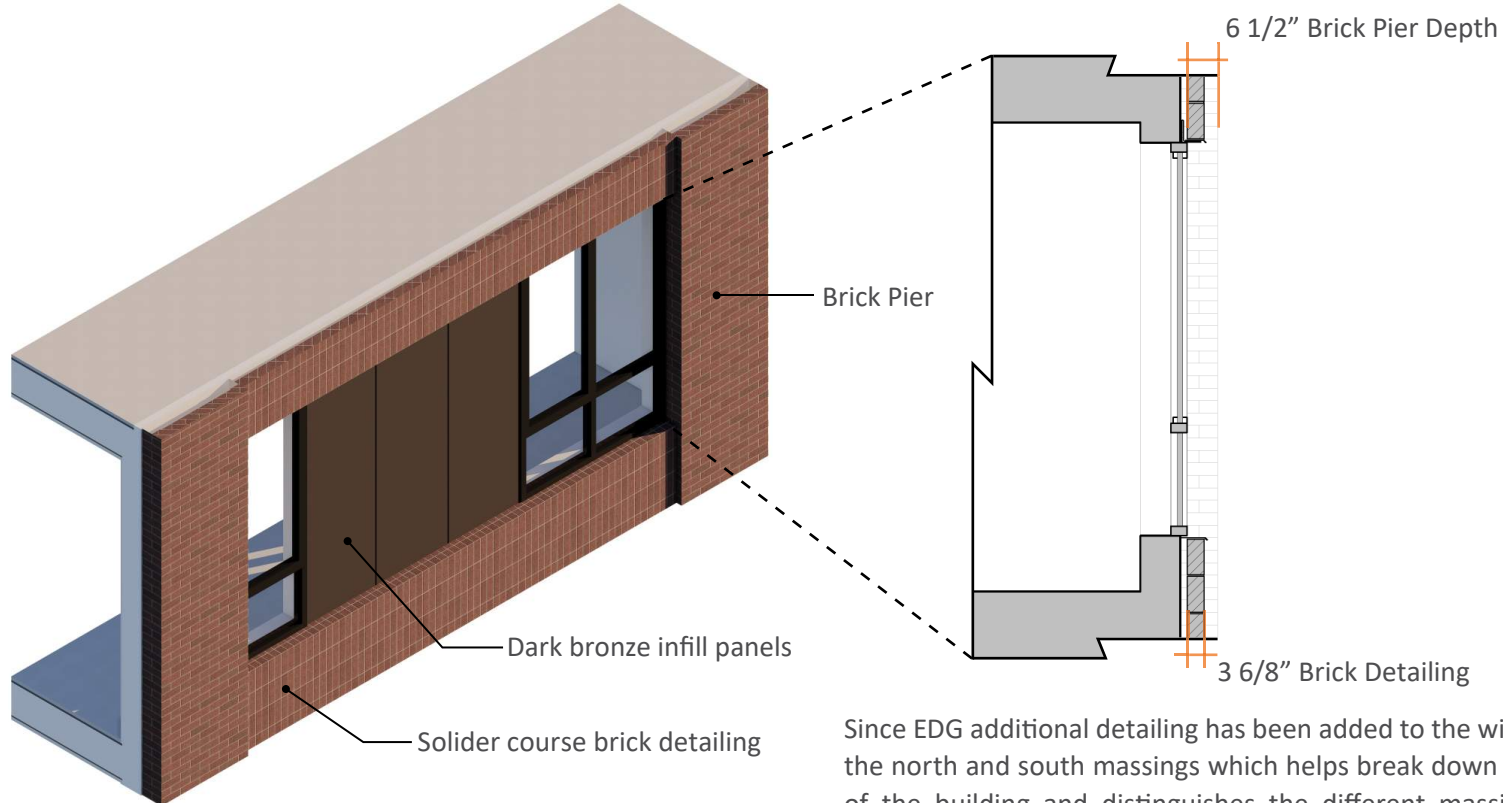
SOUTHWEST MASSING



① SOUTH MASSING WINDOW DETAIL



NORTHWEST MASSING



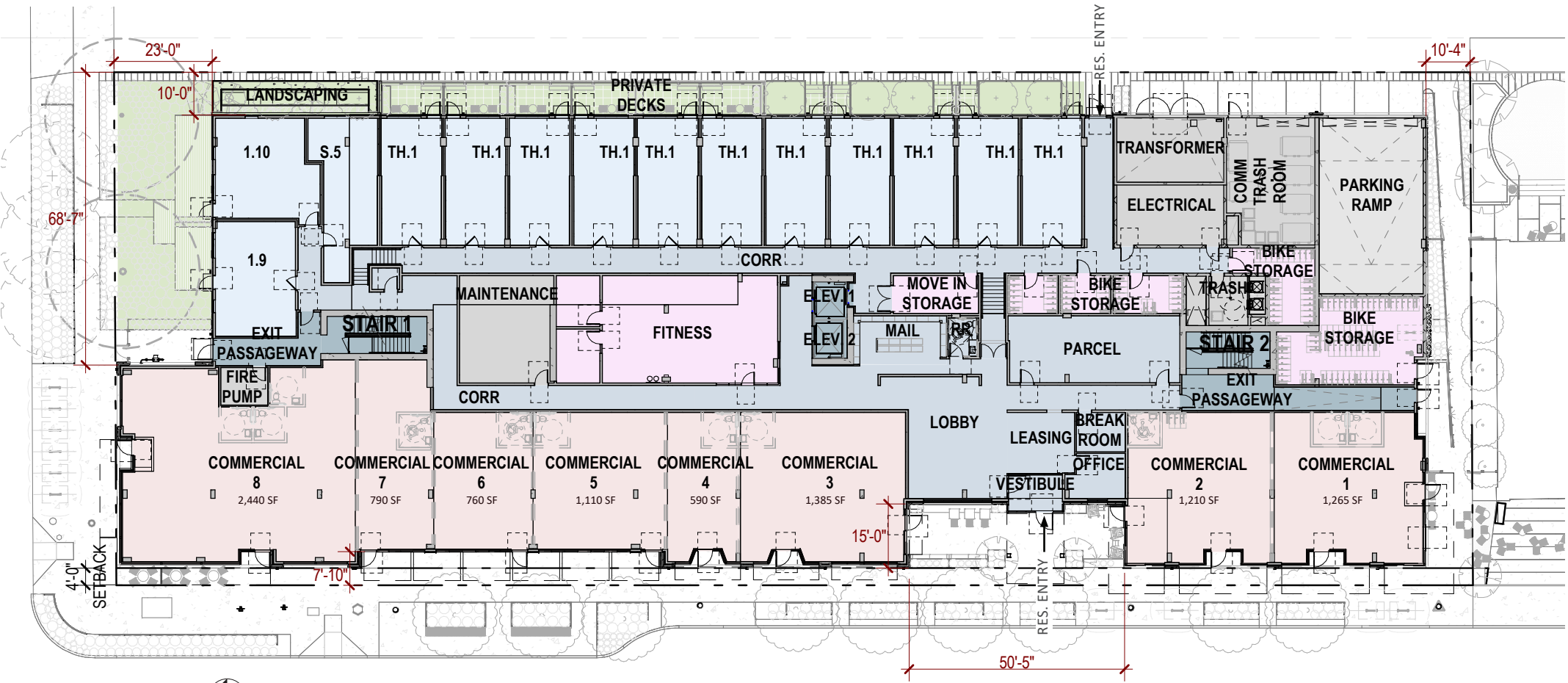
② NORTH MASSING WINDOW DETAIL

Since EDG additional detailing has been added to the windows in the north and south massings which helps break down the scale of the building and distinguishes the different massings. The window detailing helps add materiality, depth, and texture to the massing [DC4-1, DC2-B.1, DC2-C.1].

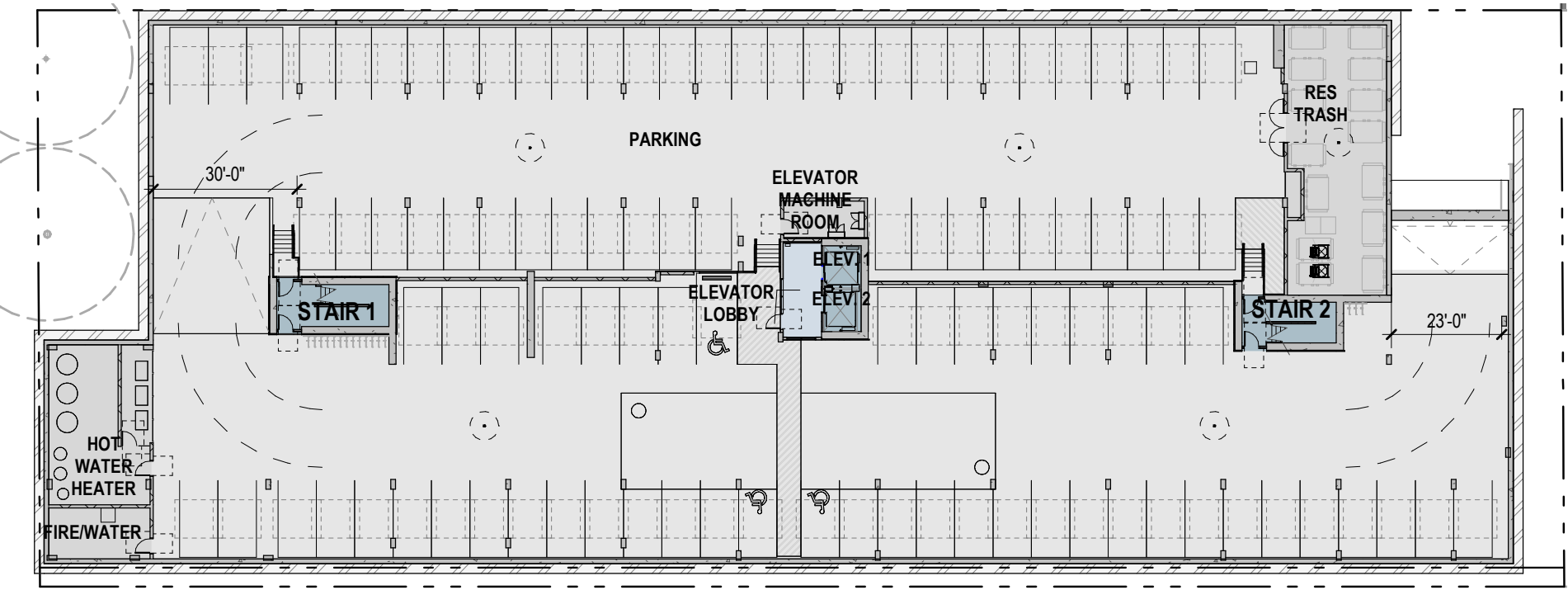


SITE PLAN

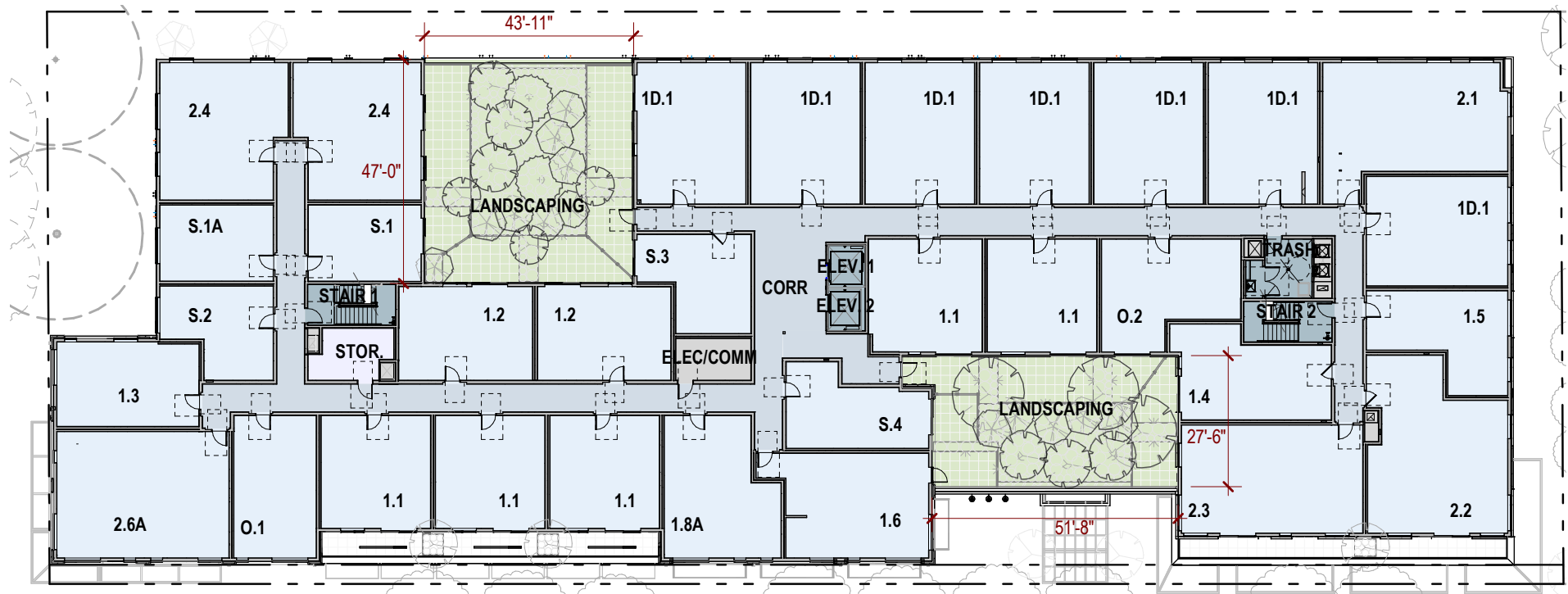
BUILDING MASSING
FLOOR PLANS



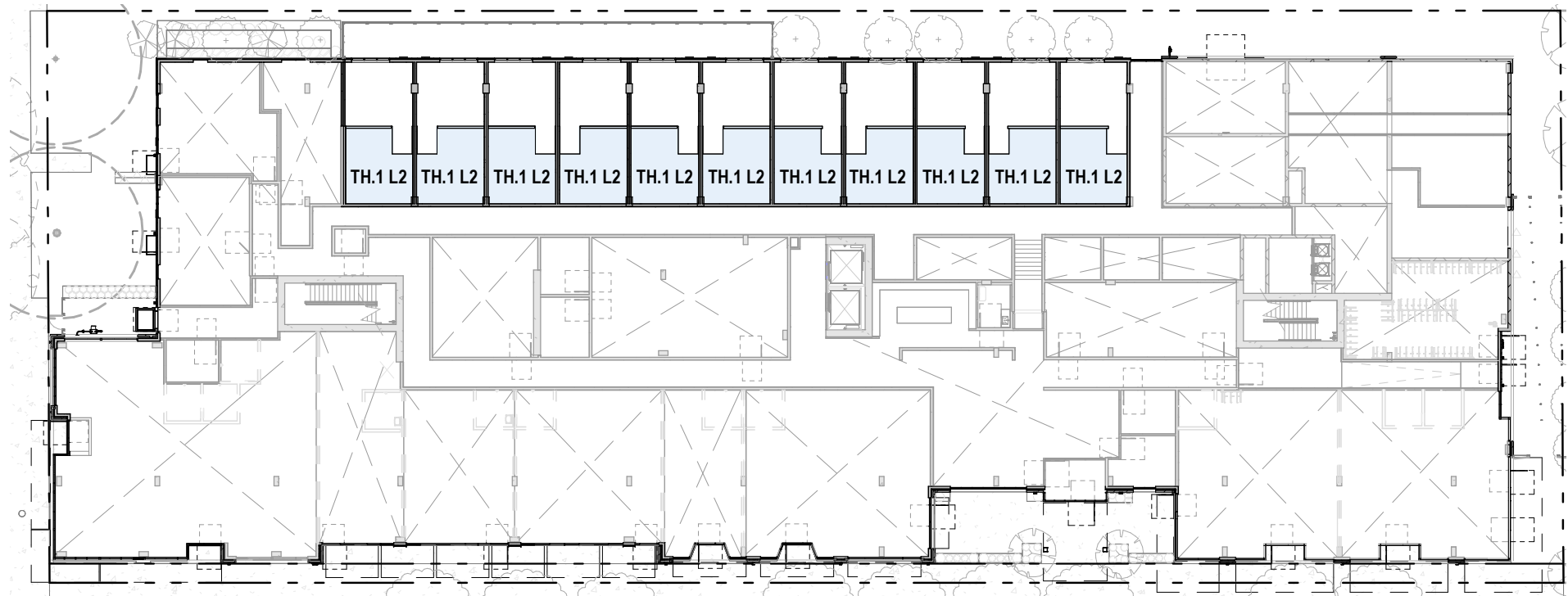
LEVEL 1



LEVEL P1

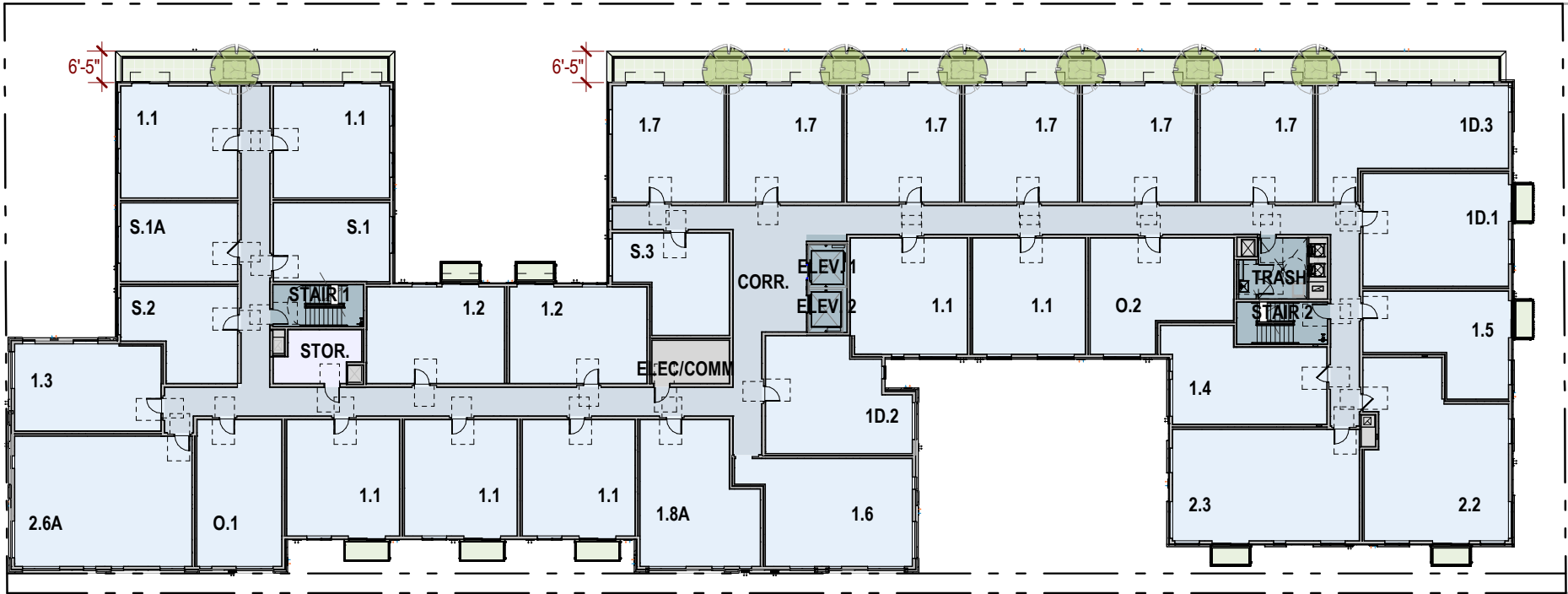


LEVEL 2



LEVEL 1.5

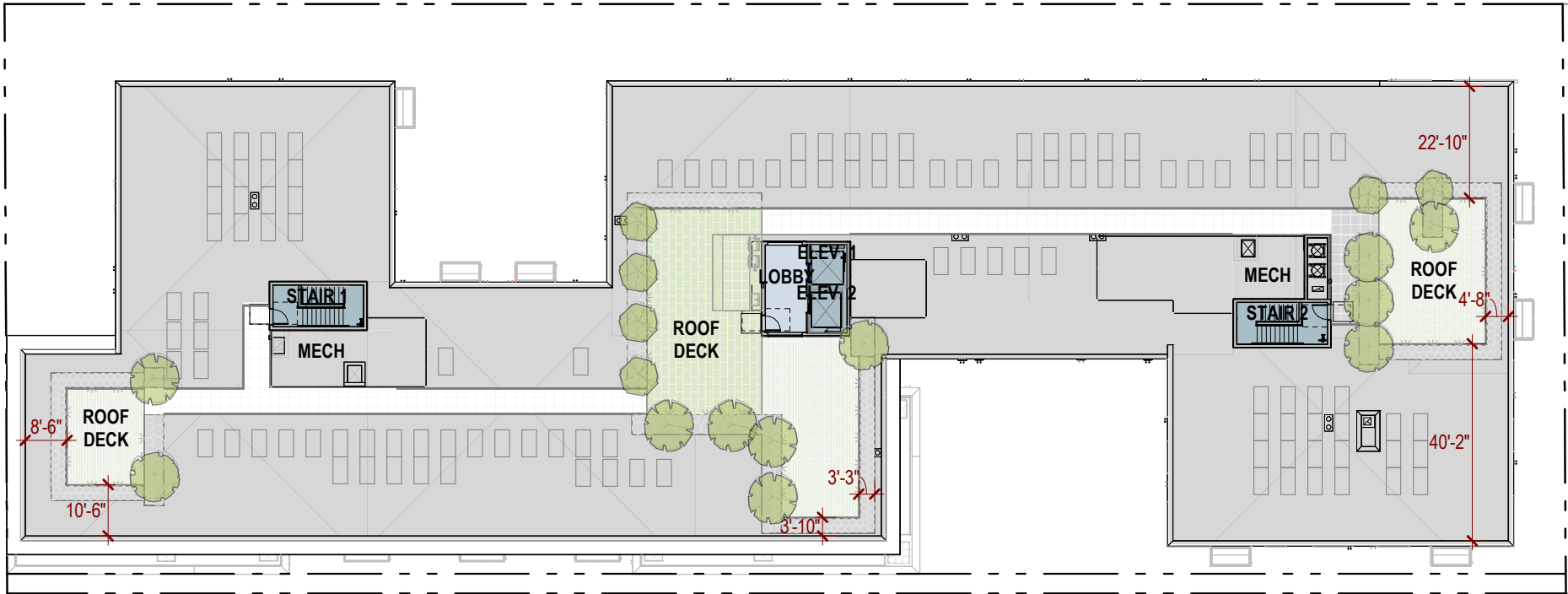
BUILDING MASSING
FLOOR PLANS



LEVEL 5



LEVELS 3-4



ROOF



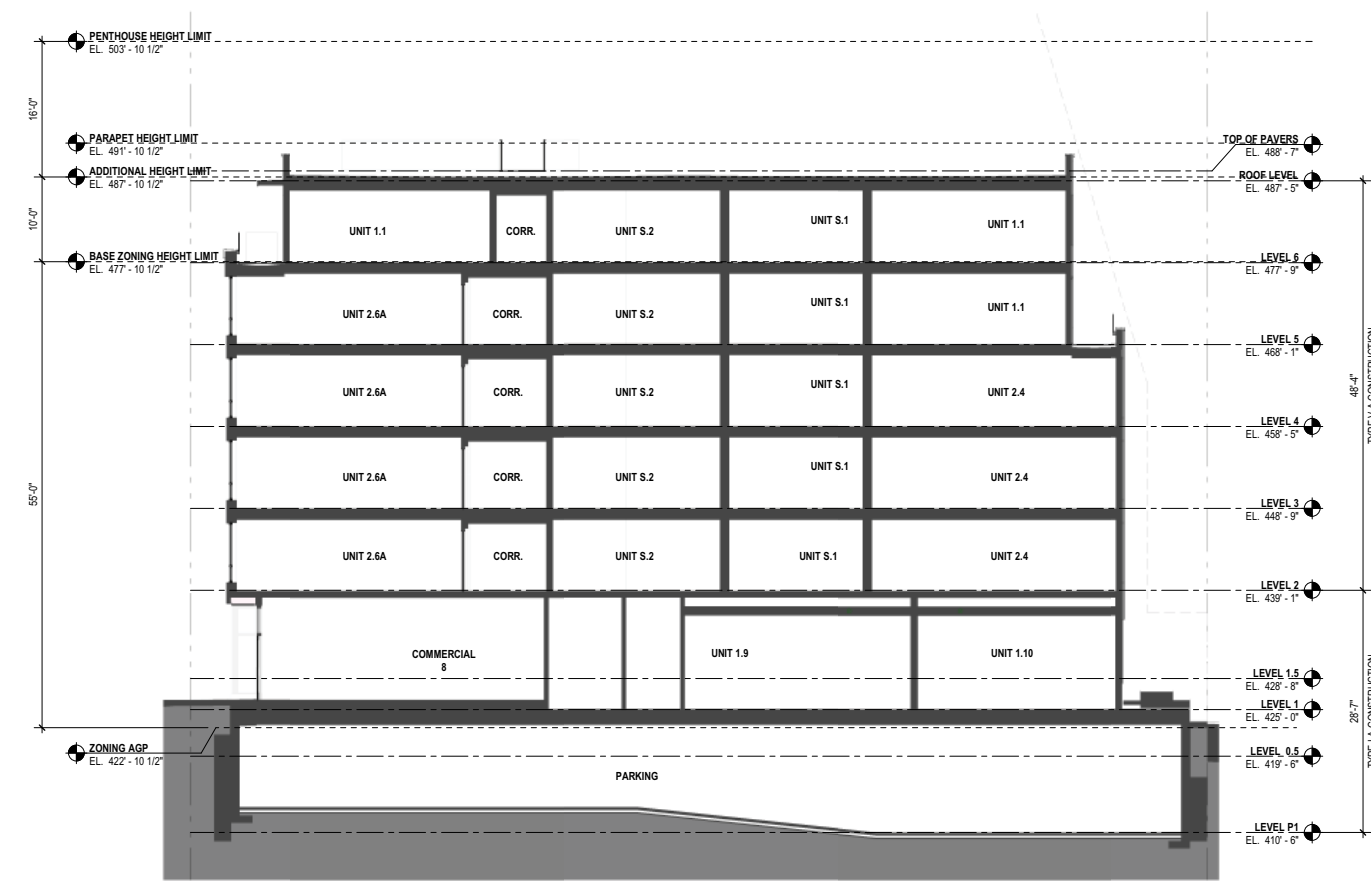
LEVEL 6

BUILDING MASSING

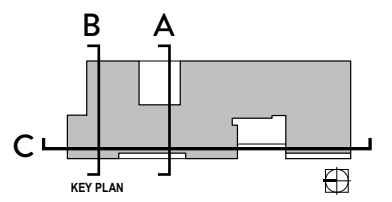
SECTIONS



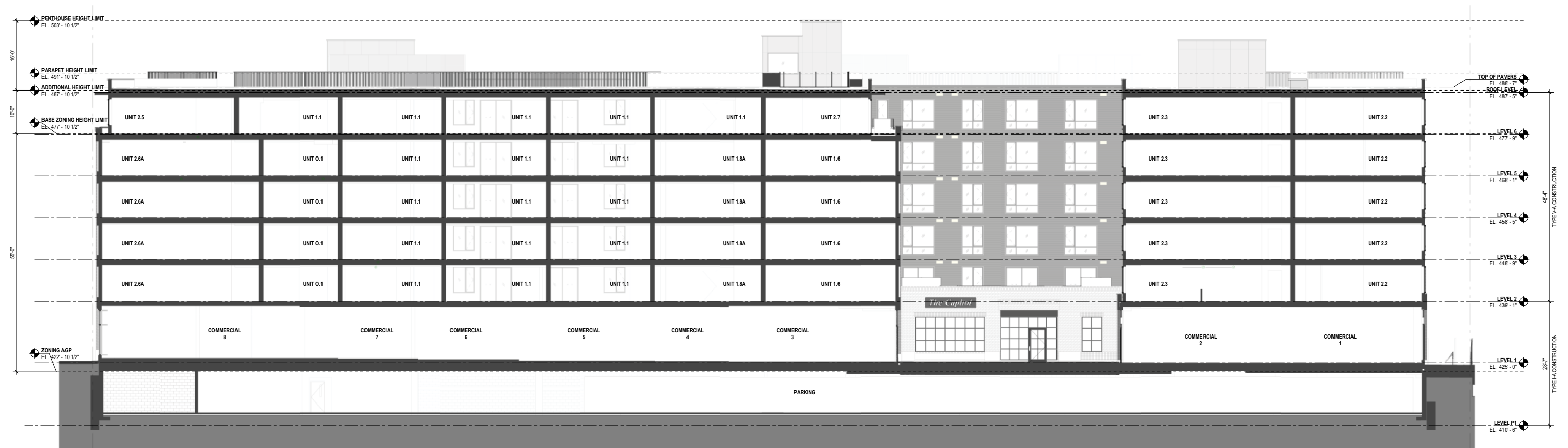
SECTION A



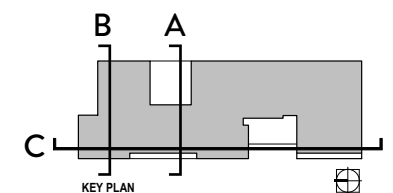
SECTION B



BUILDING MASSING



SECTION C



BUILDING MASSING
ELEVATION - WEST (15TH AVE E.)

DC2-A, CS2-D, DC3-C

The uppermost floor sets back along 15th Ave E to reduce the perceived mass at grade.

DC2-A, CS2-D, DC3-C

The north building massing is broken down into three distinct volumes which helps break down the scale. The massing is defined at either corner using different heights and materiality.

DC2-A, DC3-C, CS2-D, CS2-C

The building massing is broken down to express as two buildings by changing facade material and color between the south and north massings on either side of the entry courtyard and landscaped podium deck.

DC2-A, CS3-A, CS2-B, CS2-C

The south building massing has a unique material palette and is expressed as a 5 over 1 building massing, differentiating itself from the north building massing and creating a backdrop for the adjacent historic fire station.



WEST ELEVATION (15th Ave E.)

PL1-B, PL3-4, CS2-1-C, DC2-D

Individualized storefronts and overhead weather protection, along with recessed facades and entries, create a lot of variety at a human scale for an enhanced pedestrian experience along 15th Avenue East. Design of the commercial storefronts features an ensemble of elements including a variety of storefront design, brick piers and a variety of canopies that add to the human scale and pedestrian experience.

PL1-B, CS2-1-C, PL3-A, PL3-II, DC2-D

The wide entry courtyard along 15th Ave E provides additional pedestrian open space, that can be activated with retail spill out. A visually prominent residential entry is emphasized through signage, lighting, and a generous canopy extending to the sidewalk.

CS2-I, PL3-II, DC2-D, DC3-B

The large voluntary setback at the corner responds to the adjacent courtyard at the historic fire station. This provides opportunities for commercial activation and increases human activity at this new pedestrian through block path.

DC2-A, DC3-B, DC3-C

The upper 5 floors set back at the southern massing along 15th Ave E to reduce the perceived mass at grade. The windows and balconies add visual interest and avoid blank facade along the common property line.

CS2-I, PL3-II, DC2-D, DC3-B

The large voluntary setback at the corner responds to the adjacent courtyard at the historic fire station. This provides opportunities for retail activation and increases human activity at the new pedestrian through block path.



SOUTH ELEVATION (Throughblock Path)

DC2-A, CS2-D, DC3-C

The two upper floors set back along the alley to reduce the perceived mass at grade, and to help ease the zone transition.

PL3-III, DC3-A, DC3-B, DC3-I

The voluntary setback along the southern property line is carefully considered to tie into the neighborhood open space concepts with appropriate plantings, comfortable lighting and a pedestrian pathway to increase safety in the alley and improve pedestrian connections.

BUILDING MASSING

ELEVATION - EAST (ALLEY)

DC2-A, CS2-D, DC3-C

The two upper floors set back along the alley to reduce the perceived mass at grade.

DC2-A, DC3-C, CS2-D, CS2-C

The building massing is broken down by the podium deck with landscaping to express as two buildings.

CS2-D, PL3-2, DC2-A, CS1-4

The setback at the NW corner helps ease the zone transition along the alley, preserves existing tree canopy, and helps reduce the perceived mass at grade.



EAST ELEVATION (Alley)

CS2-B, CS2-1-C, CS2-C

The voluntary setback along the south property line provides additional pedestrian access, enhancing the neighborhood's pedestrian network and providing opportunity for increased human activity, and pedestrian passage from the street to the alley. This also sets back the south facade from the adjacent fire station, responding to the neighborhood context and avoiding a blank facade along the common property line.

PL3-1-C, PL3-2, CS2-D

The location of residential units at grade in the alley activates the frontage, providing residential use to help ease the zone transition.

BUILDING MASSING

ELEVATION - NORTH (E. REPUBLICAN ST.)

DC2-A, DC2-B, DC2-C

The upper floors of the building provide visual depth and interest with a layering of material colors and textures.

DC2-A, DC3-B, DC3-C

The uppermost floor sets back along 15th Ave E to reduce the perceived mass at grade.

CS2-D, CS2-D-3. DC2-A. CS2-1-C

The setback at the NW corner helps ease the zone transition along the alley, preserves existing tree canopy, responds to the adjacent neighborhood context by extending ground floor residential use along E. Republican, and helps reduce the perceived mass at grade.



NORTH ELEVATION (E. Republican St.)

BUILDING MASSING
PERSPECTIVE VIEWS



PROPOSED CONDITION



PROPOSED CONDITION



CURRENT CONDITION



CURRENT CONDITION



PROPOSED CONDITION



PROPOSED CONDITION



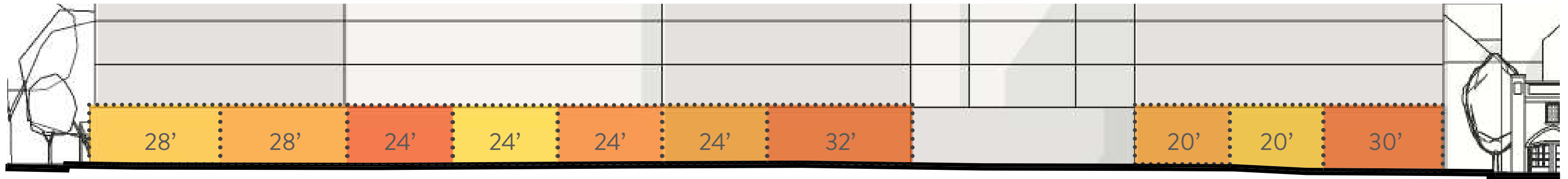
CURRENT CONDITION



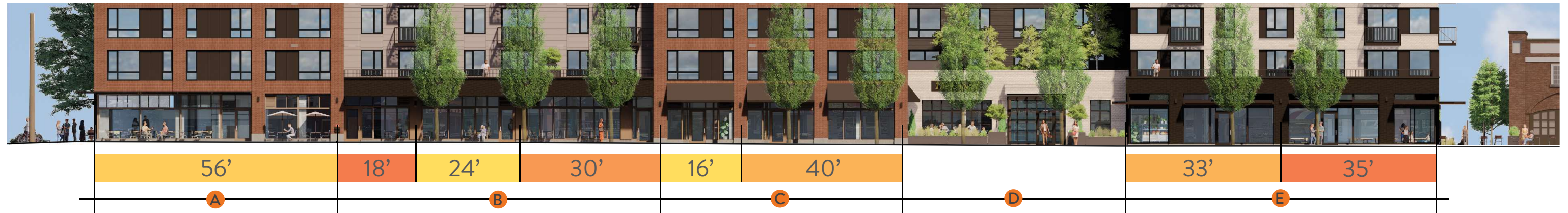
CURRENT CONDITION

15TH AVE STREETSCAPE

STREETSCAPE DEVELOPMENT



EDG - MASSING BREAKDOWN OF COMMERCIAL STOREFRONTS ALONG 15TH AVE E



DRB - MASSING BREAKDOWN OF COMMERCIAL STOREFRONTS ALONG 15TH AVE E

Since EDG the massing breakdown of the street frontage along 15th has been further developed to propose a variety of scale for future commercial use, accentuated by a range of materials, and varied overhead weather protection. The variety expressed along the 15th Ave streetscape can be broken down into 5 different approaches which respond to the surrounding context and overall building massing. These include:

- A** North Corner: Commercial use wraps the corner onto E. Republican St. An enlarged curb bulb supports retail spillout to activate the corner. This provides a strong anchor at the corner [CS2-C].
- B** Recessed Storefront: Small scale retail is tucked into an overhead setback adjacent to the proposed bus stop [PL3-A2, PL3-C]

- C** Brick Piers: The rhythm of the brick piers meet the sidewalk. This provides texture with their materiality at a human scale and a rhythm along the frontage, that defines the storefront and canopy scale [DC2-D].
- D** Entry Courtyard: The entry courtyard breaks down the overall massing along 15th, and provides an architectural focal point to signal the main entrance [CS2-D, CS2-C].
- E** South Corner: Commercial use anchors the south massing at the corner [CS2-C].



NORTHWEST PERSPECTIVE

416 15TH AVE. E.

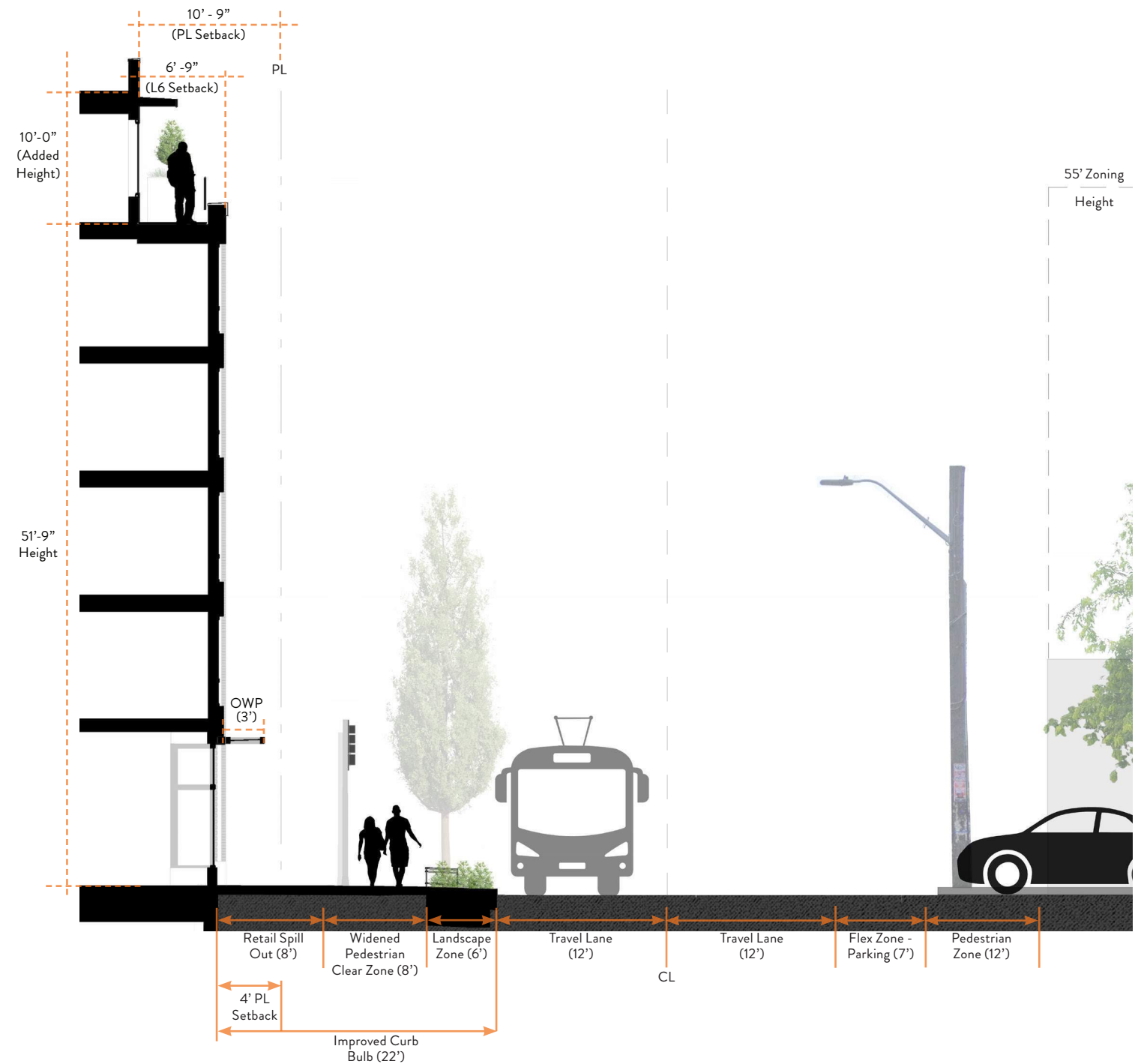
15TH AVE STREETSCAPE NORTH CORNER



15TH AVE PERSPECTIVE A - NORTH CORNER



KEY PLAN

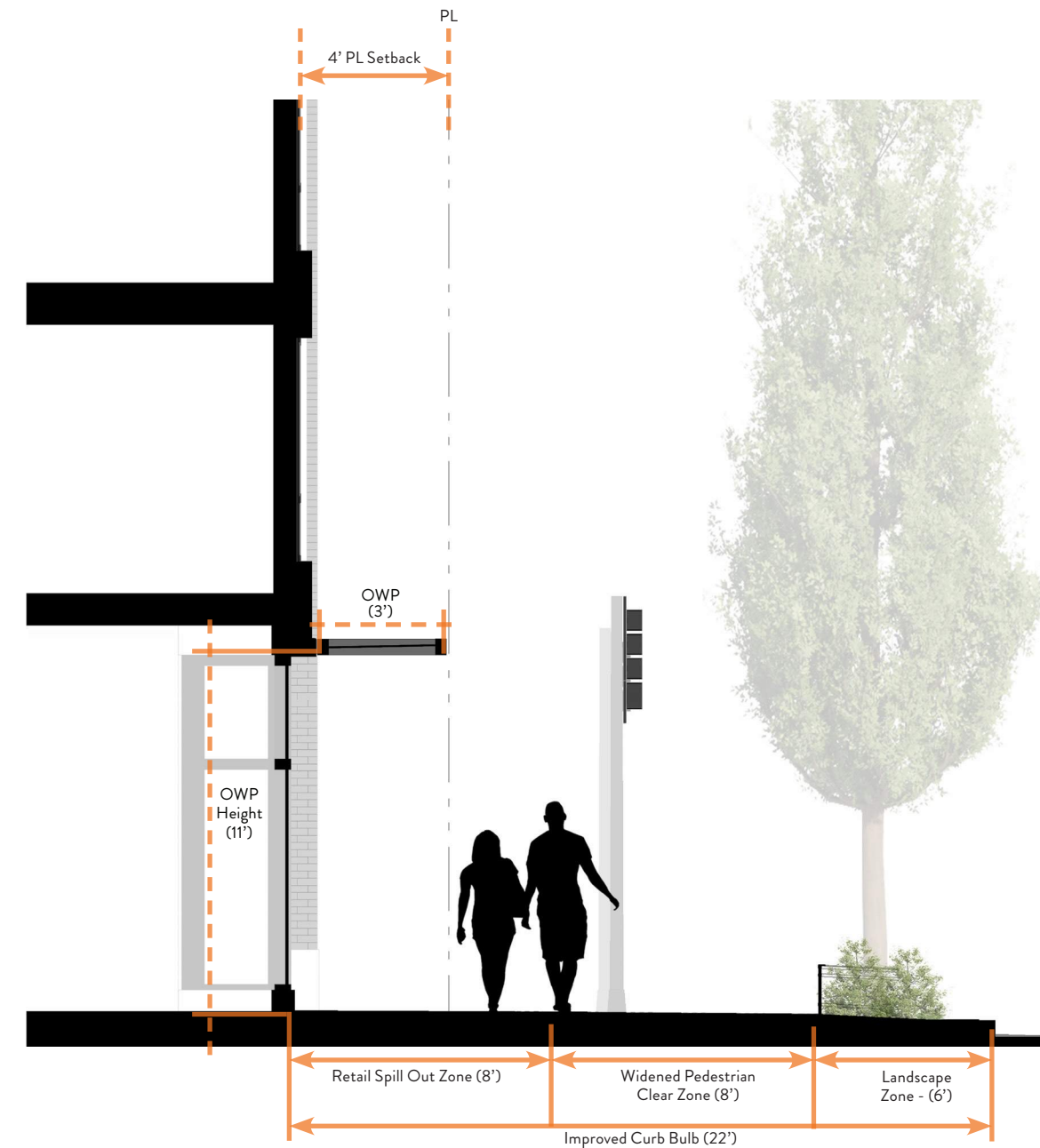


SECTION A - NORTHWEST CORNER

Architectural detailing and recessed canopies create a corner focal point. An improved curb bulb creates opportunities for retail spill out and corner activation, with increased landscaping along the street.



15TH AVE AXON A - NORTH CORNER

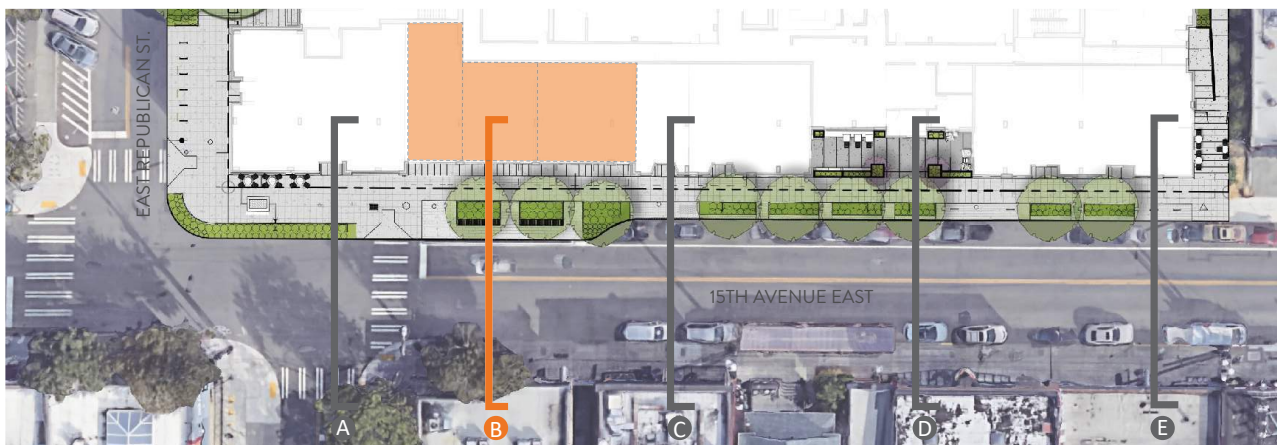


ENLARGED SECTION A - NORTH CORNER

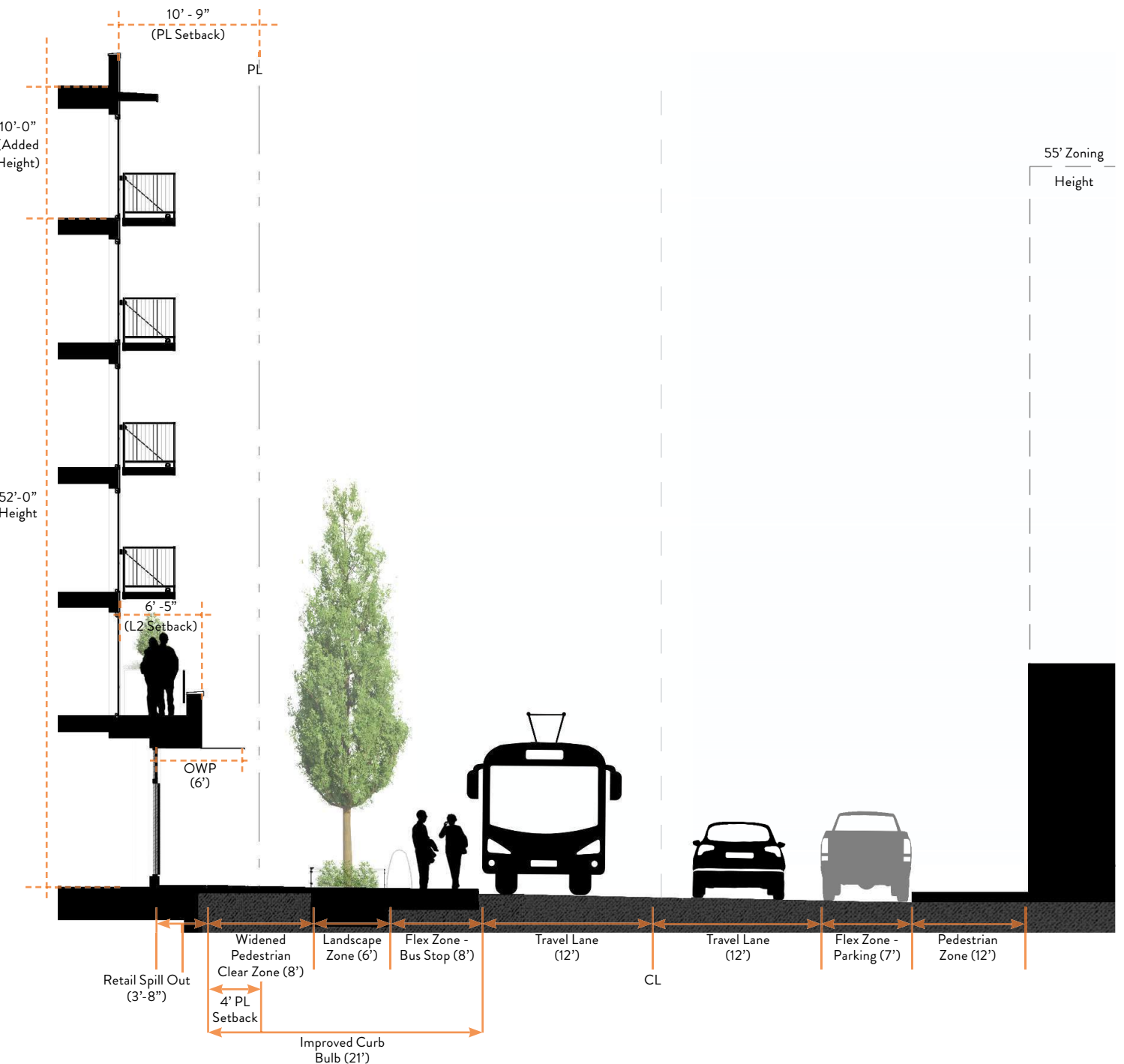
15TH AVE STREETSCAPE BUS STOP & RETAIL



15TH AVE PERSPECTIVE B - BUS STOP

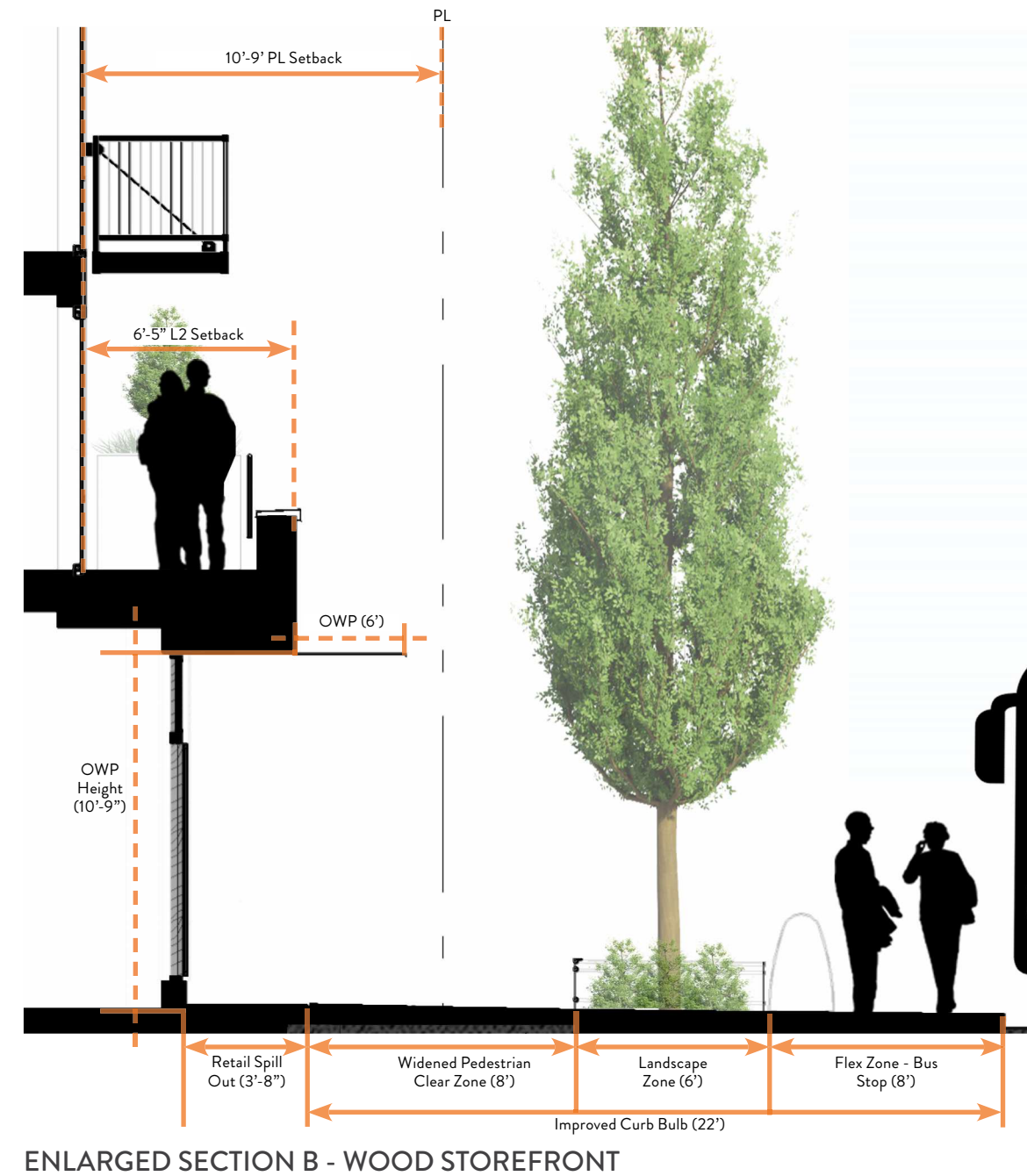
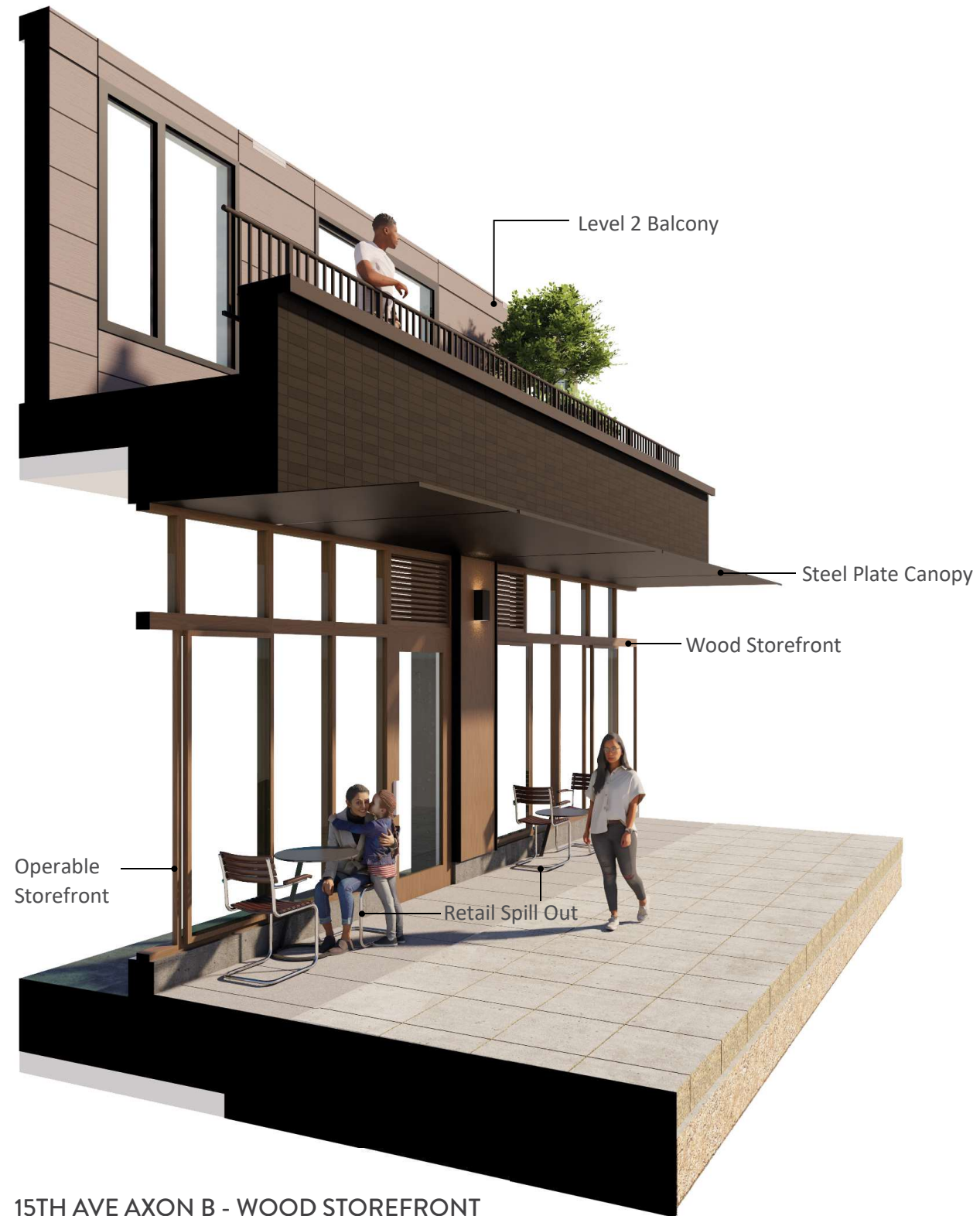


KEY PLAN



SECTION B - BUS STOP

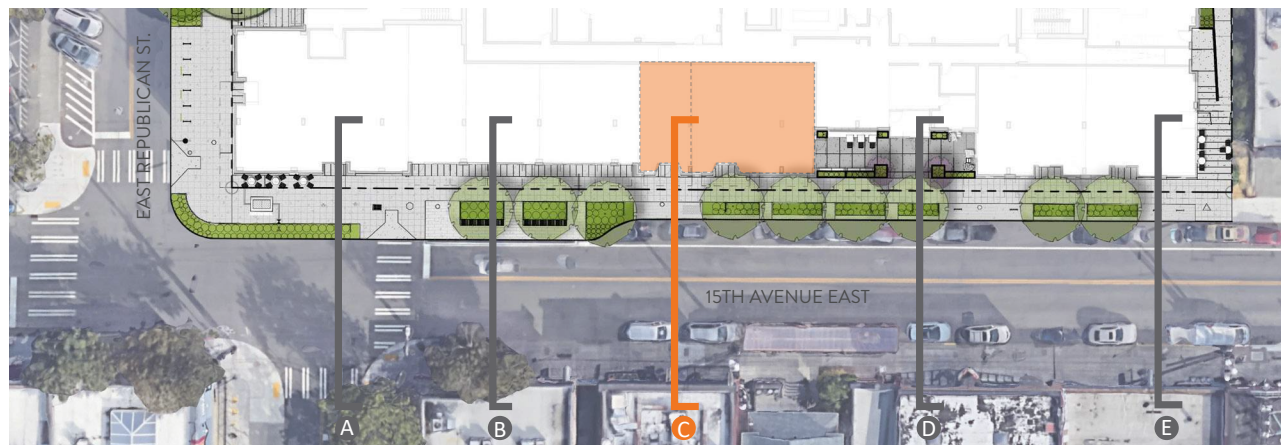
Recessed wood storefront with overhead protection responds to the adjacent bus stop. Creates spill out space, and opportunities for future personalization by small scale commercial tenants.



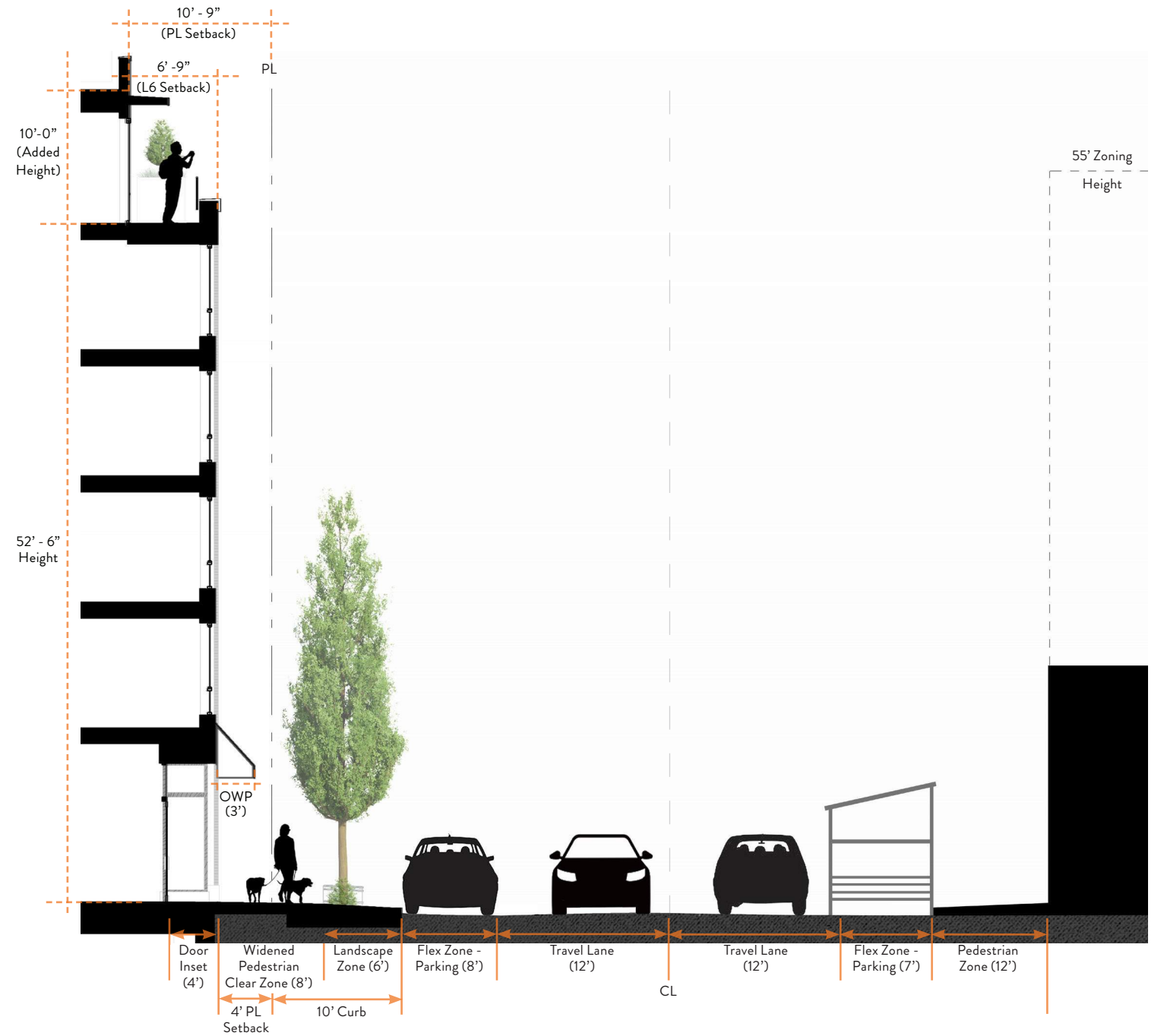
15TH AVE STREETSCAPE BRICK PIERS



15TH AVE PERSPECTIVE C - BRICK PIERS



KEY PLAN

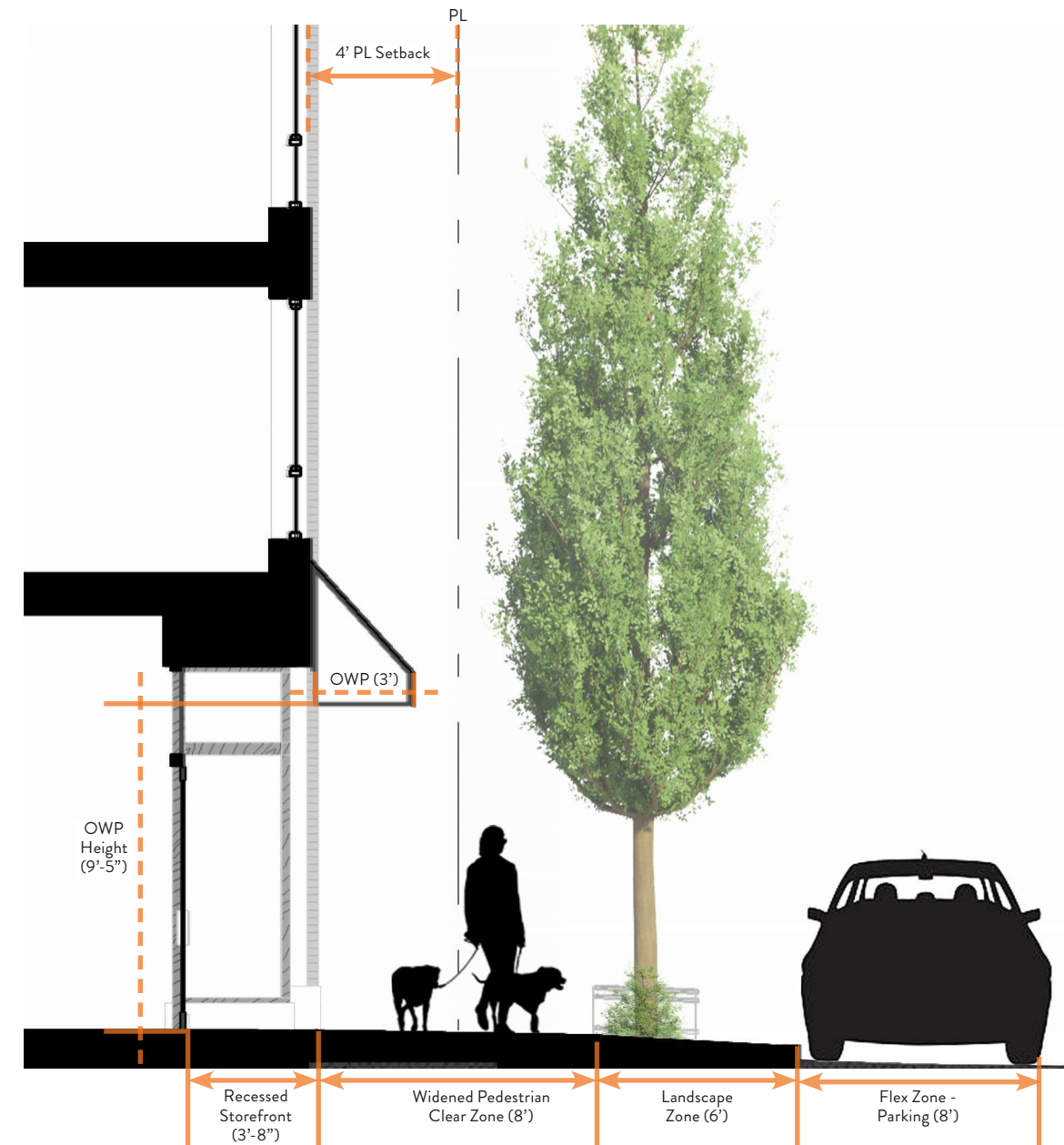


SECTION C - BRICK PIERS

Triangular cloth canopies are tucked into the brick piers at the recessed wood storefront retail entries. This adds variety along the facade and responds to the surrounding context in style and scale of canopy and storefront.



15TH AVE AXON C - BRICK PIER



ENLARGED SECTION C - BRICK PIER

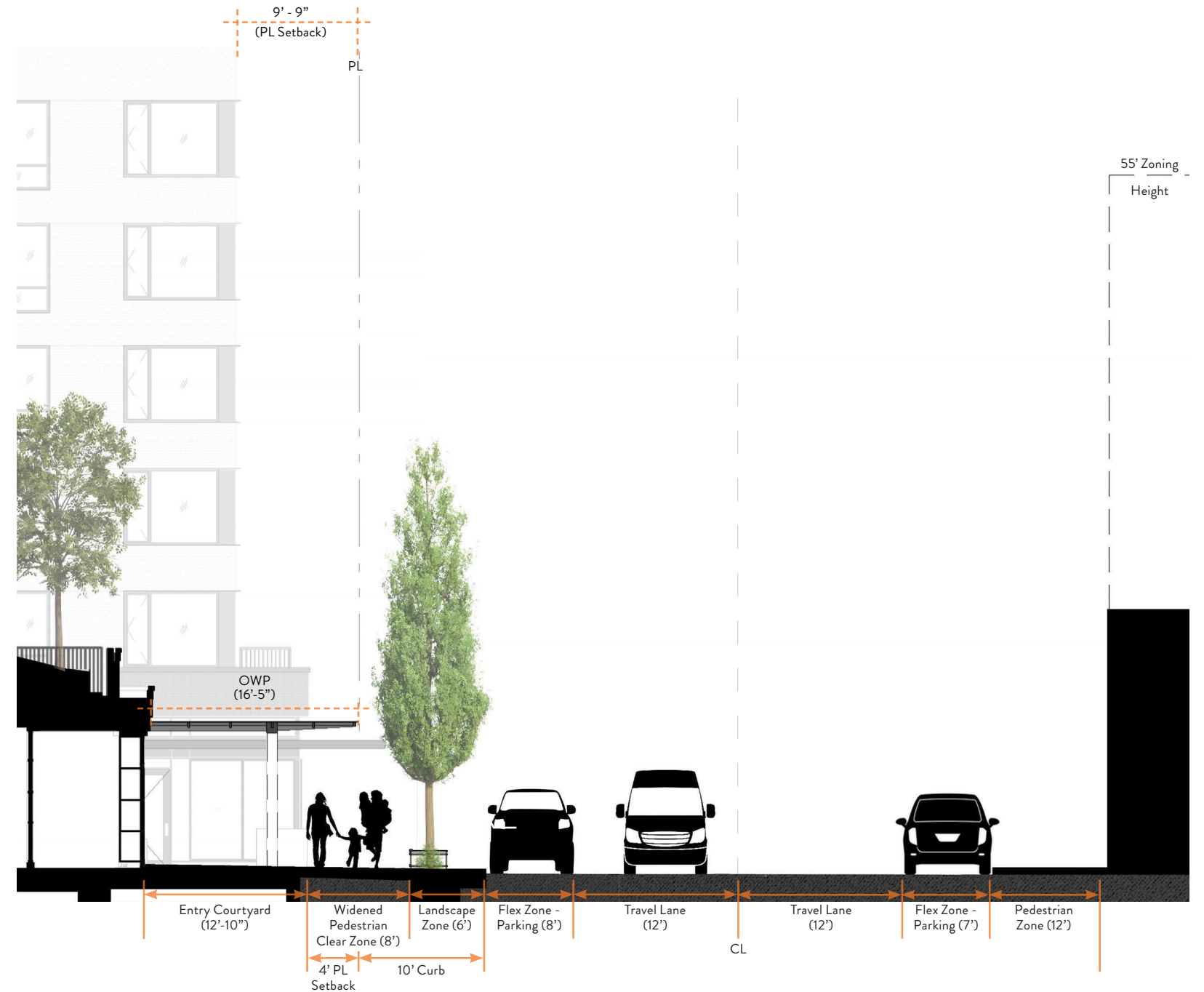
15TH AVE STREETSCAPE RESIDENTIAL ENTRY COURTYARD



15TH AVE PERSPECTIVE D - RESIDENTIAL ENTRY COURTYARD



KEY PLAN



SECTION D - RESIDENTIAL ENTRY COURTYARD

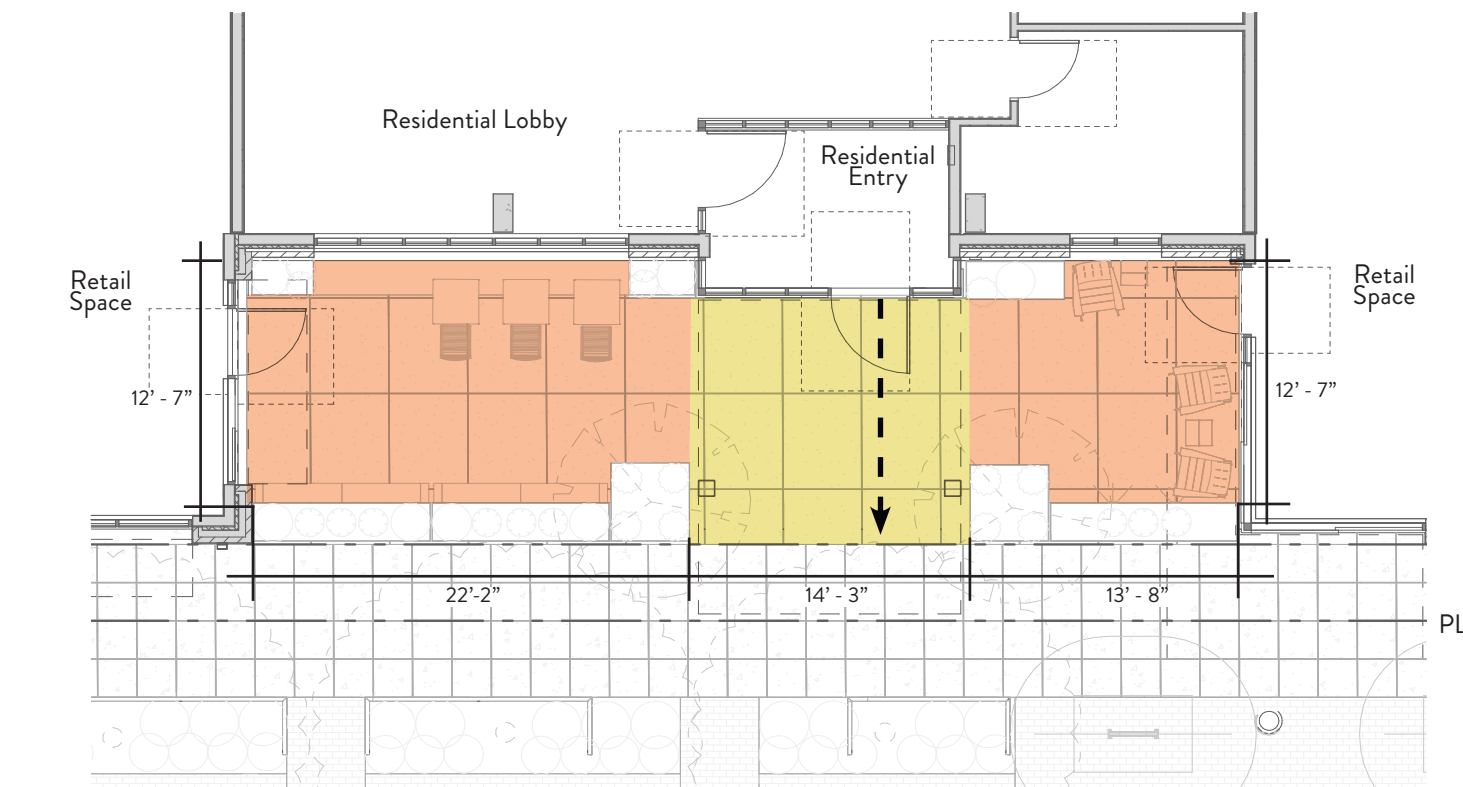
A recessed courtyard creates corner conditions for the adjacent commercial spaces to spill into, the focal point of the residential entry is emphasized by a large canopy, signage, and lighting.



COURTYARD PERSPECTIVE



NIGHT RENDERING



COURTYARD PLAN  RETAIL SPILL OUT RESIDENTIAL CIRCULATION

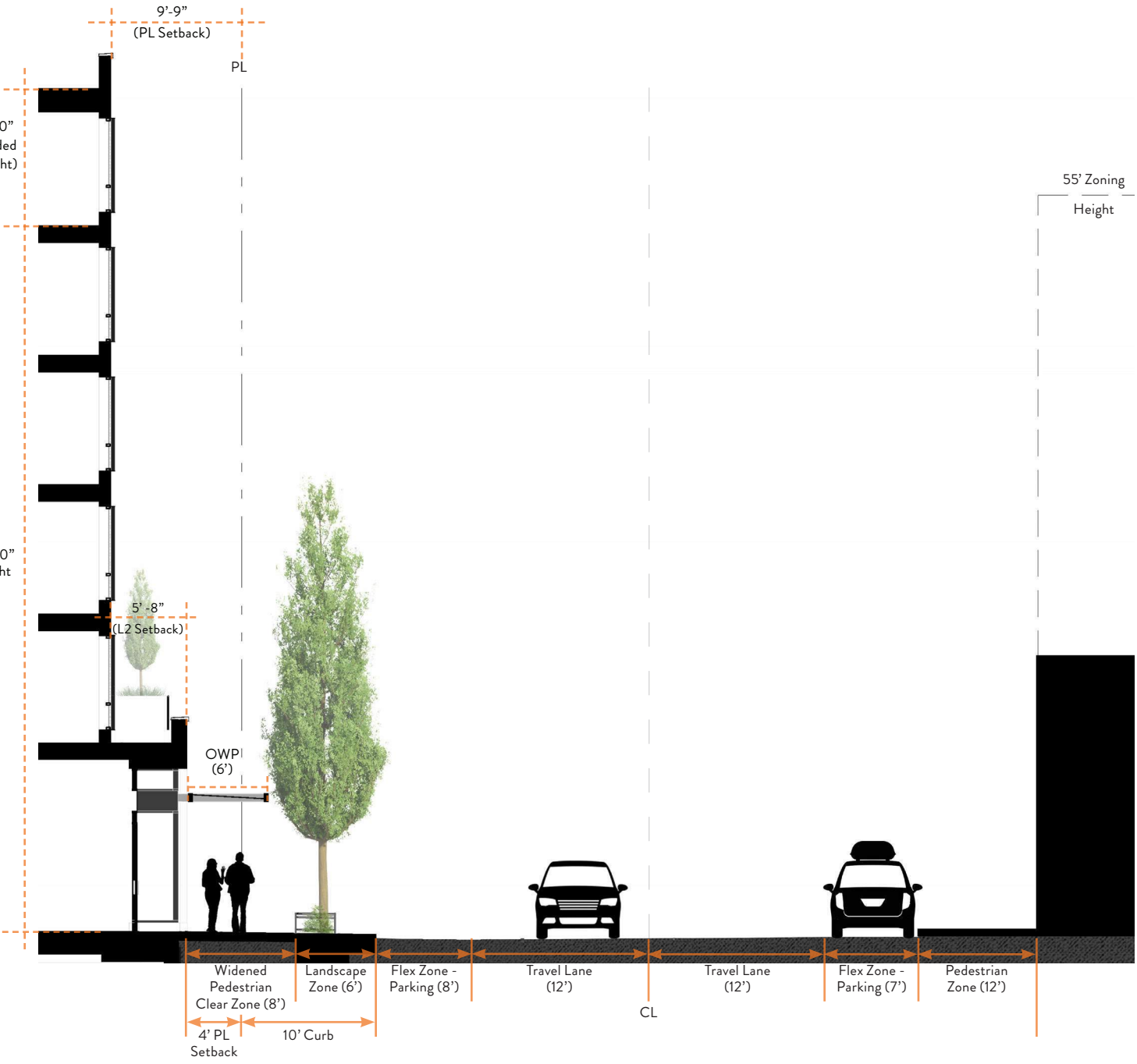
15TH AVE STREETSCAPE SOUTH CORNER



15TH AVE PERSPECTIVE D - SOUTH CORNER



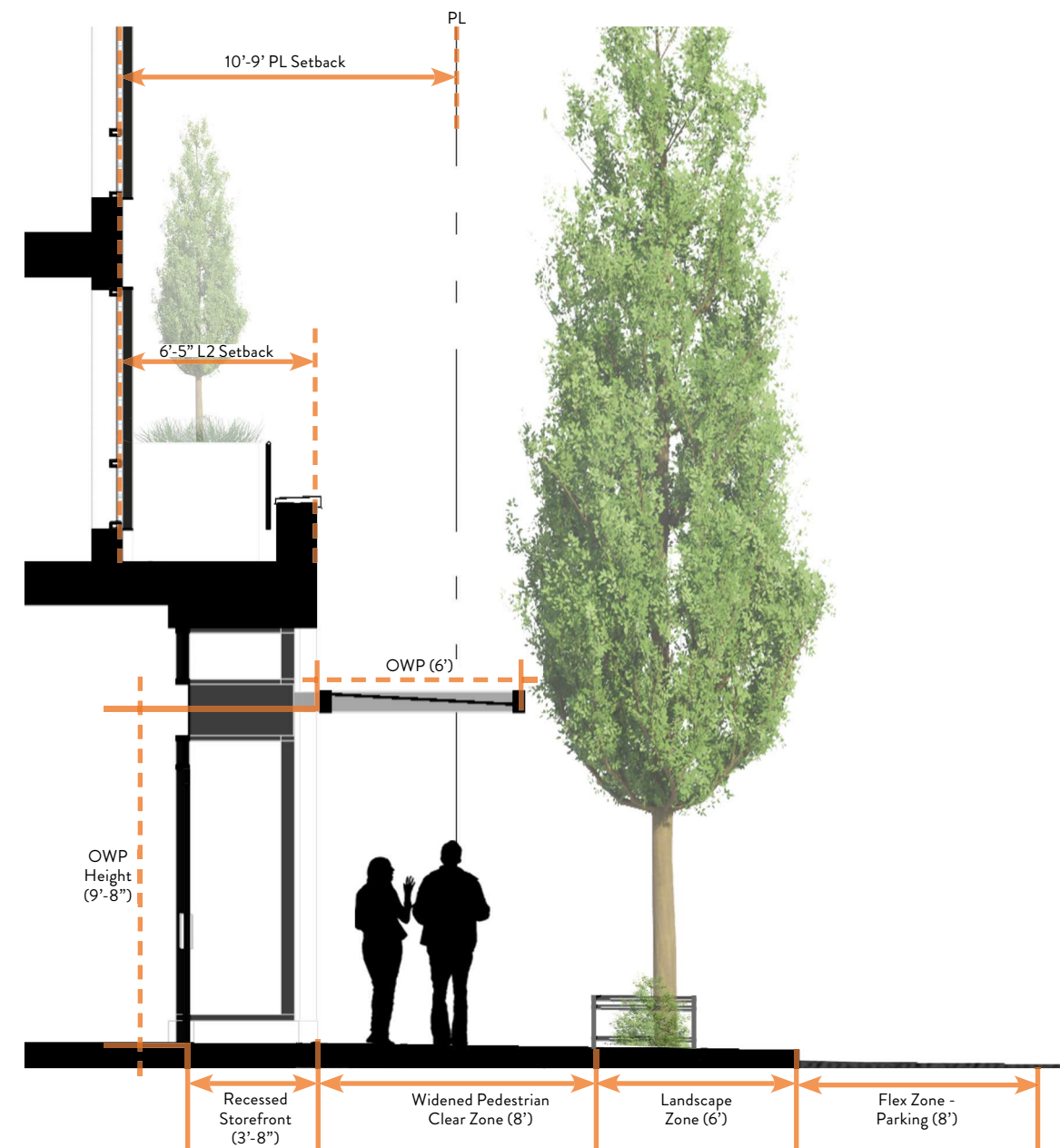
KEY PLAN



SECTION E - SOUTH CORNER
Canopies extend over the storefront and wrap the SW corner. The commercial corner responds to the adjacent plaza in front of the fire house, activating the through block path and prominent corner.



15TH AVE PERSPECTIVE - SW CORNER



ENLARGED SECTION E - SOUTH CORNER

THROUGHBLOCK DEVELOPMENT BLOCK MASTER PLAN



Since EDG there has been further collaboration with the neighbors at the adjacent Fire Station building to coordinate opportunities for public realm activation with the throughblock path. This has resulted in the development of a master plan for the block. The master plan proposes:

1. Three part phased development that will continue to enhance the space created by the southern setback and proposed throughblock connection.
2. Connecting the proposed retail spill out space at the southwest corner with the adjacent plaza to create a space for pop-ups, community activities, and pedestrian activation of the corner of 15th and East Harrison Street.
3. Coordinated landscape design including updated plantings along the throughblock path and locating the proposed ramp landing to meet the Fire Station property at their back yard. This will allow the pedestrian path to serve both adjacent properties, and will increase the use of the throughblock path.

PHASE 3 (Future): Fire Station plans to redesign their backyard space, connecting to the throughblock path.

PHASE 2 (Future): Fire Station connects plaza with retail spill out space at the SW corner which enhances the plaza for pedestrian activation. Updated landscaping along the Fire Station's north property line will enhance the proposed path to the alley.

PHASE 1: Throughblock path and landscaping completed along south property line, provides pedestrian access to the alley with landscaping and lighting and creates retail spill out space at the SW corner.

THROUGHBLOCK PLAN



SW CORNER PERSPECTIVE (GATE OPEN)



SW CORNER PERSPECTIVE (GATE CLOSED)



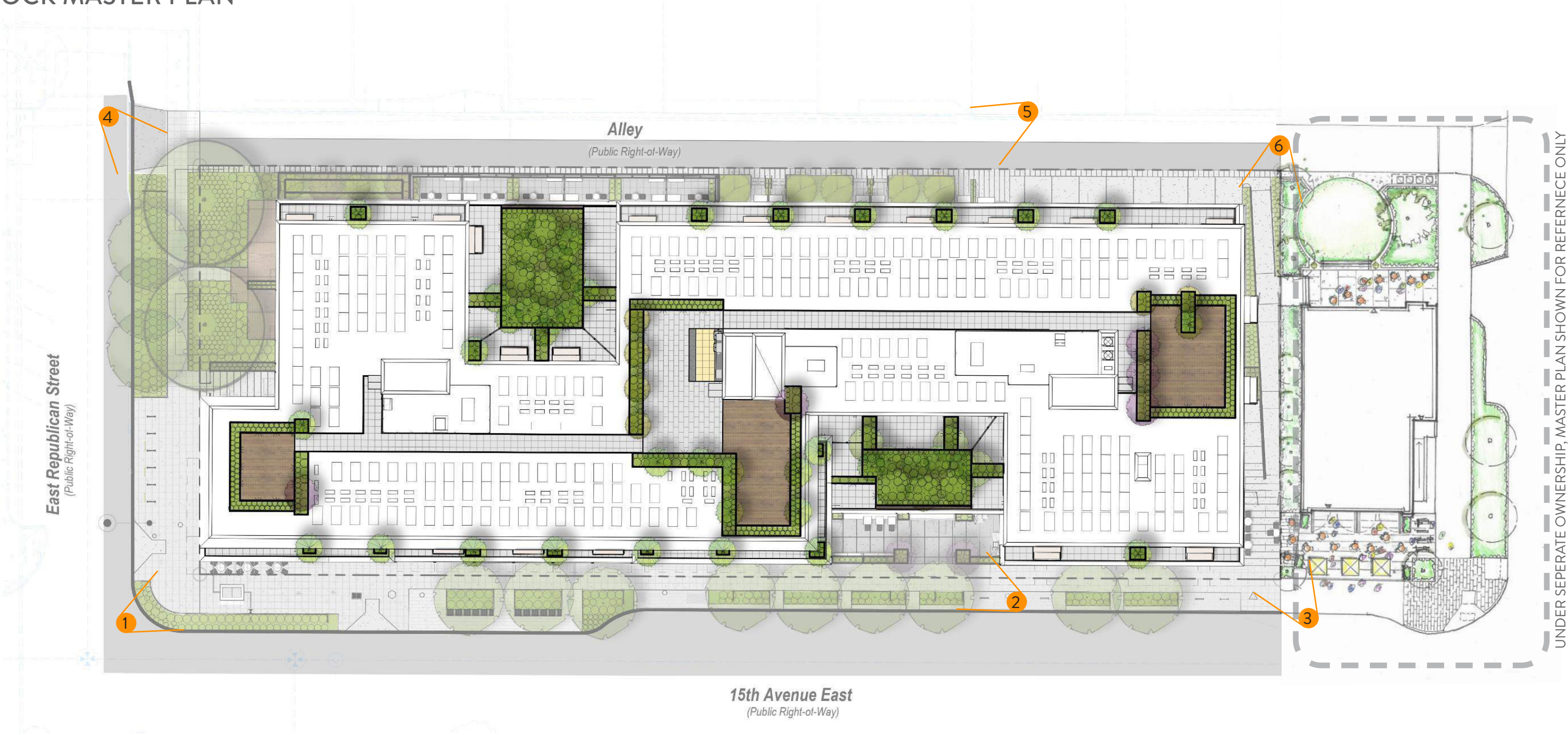
SE CORNER PERSPECTIVE (GATE OPEN)



SE CORNER PERSPECTIVE (GATE CLOSED)

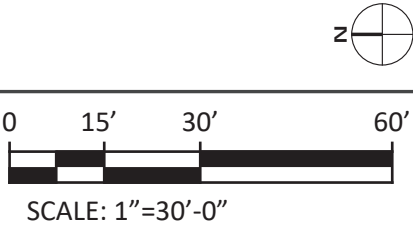
THROUGHBLOCK DEVELOPMENT

BLOCK MASTER PLAN



UNDER SEPERATE OWNERSHIP, MASTER PLAN SHOWN FOR REFERENCE ONLY

1 SITE MASTER PLAN



THROUGHBLOCK DEVELOPMENT
PUBLIC REALM AREAS - PERSPECTIVES



1 15TH AVE STREETSCAPE



2 15TH AVE ENTRANCE



3 PASS THROUGH (WEST LOOKING EAST)



4 CORNER OF EAST REPUBLICAN STREET AND ALLEY



5 ALLEY STREETSCAPE

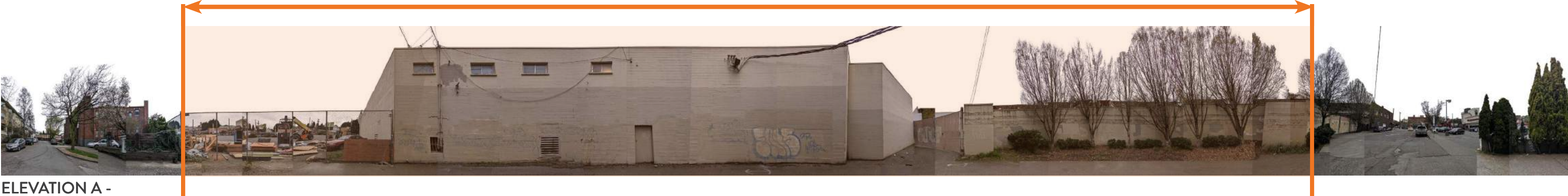


6 PASS THROUGH (EAST LOOKING WEST)

ALLEY FRONTAGE

ALLEY ELEVATIONS (EXISTING)

PROJECT SITE (NC2 ZONE)



ELEVATION A -
LOOKING WEST

OPPOSITE PROJECT SITE (NR3 ZONE)

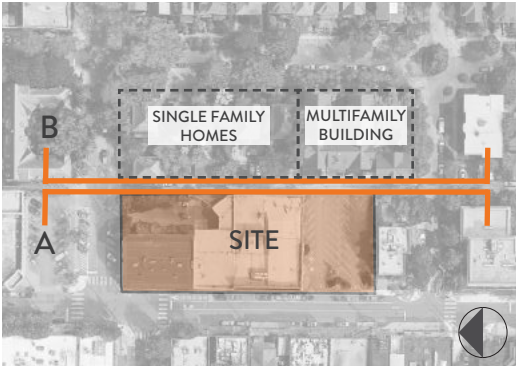


ELEVATION B -
LOOKING EAST

NORTH - "SIDE YARD"
(LANDSCAPED SETBACK FROM
STREET FOR RESIDENTIAL USE)

MIDDLE - "BACK YARD"
(SCREENED PRIVACY FENCES & LANDSCAPING
TO YARDS FOR PRIVATE RESIDENTIAL USE)

SOUTH - "SERVICE"
(PARKING, TRASH ACCESS,
SERVICE USES)



ALLEY FRONTAGE

ALLEY ELEVATIONS (PROPOSED)



ELEVATION A - PROPOSED

SOUTH - “SERVICE”
(PARKING, TRASH ACCESS,
SERVICE USES)

MIDDLE - “BACK YARD”
(SCREENED PRIVACY FENCES TO PRIVATE PATIOS &
LANDSCAPED YARDS WITH DIRECT ACCESS TO UNITS)

NORTH - “SIDE YARD”
(LANDSCAPED SETBACK FROM
ALLEY FOR RESIDENTIAL USE)

OPPOSITE PROJECT SITE (NR3 ZONE)

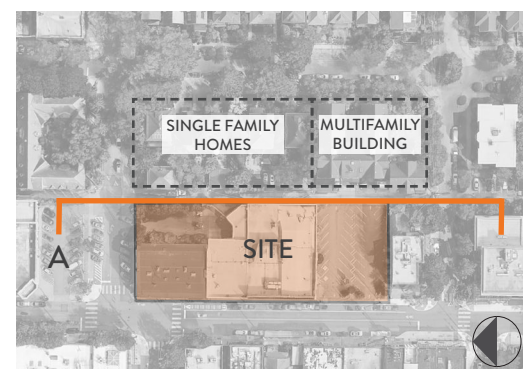


ELEVATION B - LOOKING EAST

NORTH - “SIDE YARD”
(LANDSCAPED SETBACK FROM
STREET FOR RESIDENTIAL USE)

MIDDLE - “BACK YARD”
(SCREENED PRIVACY FENCES & LANDSCAPING
TO YARDS FOR PRIVATE RESIDENTIAL USE)

SOUTH - “SERVICE”
(PARKING, TRASH ACCESS,
SERVICE USES)



Since the time of the EDG the east facade has developed to improve the zone transition, residential activation, and increase safety along the alley. To respond to the Board’s comments, the design team has:

1. Studied the existing conditions across the alley to inform the proposed approach to the east alley facade. We identified three distinct ways that the neighboring sites respond to the alley (north, middle, south), which helped further develop the design of the residential units, landscaping, residential and pedestrian access, and service use.

2. Explored several design options for the townhouse units to consider the feasibility of providing direct access from the alley, and considering how residential use along the alley can activate that frontage.

3. Developed the landscaping at the ground level to help ease the zone transition, and increase the greenery along the alley.

ALLEY FRONTAGE

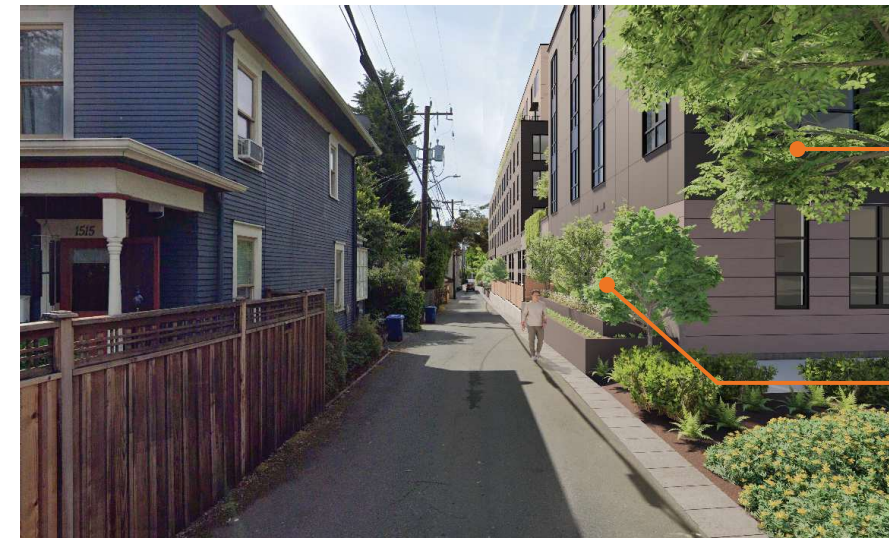
ALLEY COMPARISONS

CURRENT CONDITION

PROPOSED CONDITION

NORTH

- Landscaping and blank wall condition
- Side yard and residential use across alley



- Building setback forms front yard for units, allows preservation of existing trees
- Additional landscaping adds trees to northeast alley corner, eases the zone transition

MIDDLE

- Blank wall condition
- Fences and privacy landscaping across alley with access to backyard residential use



- Increased safety, eyes on the alley from loft units
- Occupiable private patios with landscaping
- Detailing of privacy fence breaks down scale along the alley to reduce blank wall condition
- Voluntary 2' setback widens alley, total 10' setback to building from property line

SOUTH

- Loading, trash, parking and other service uses across alley
- Surface parking lot (not in use)



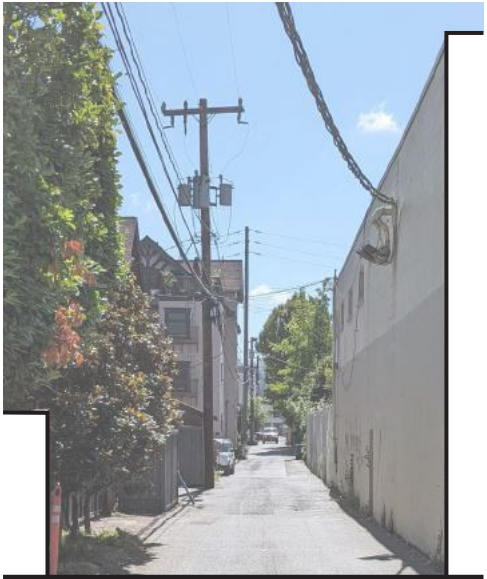
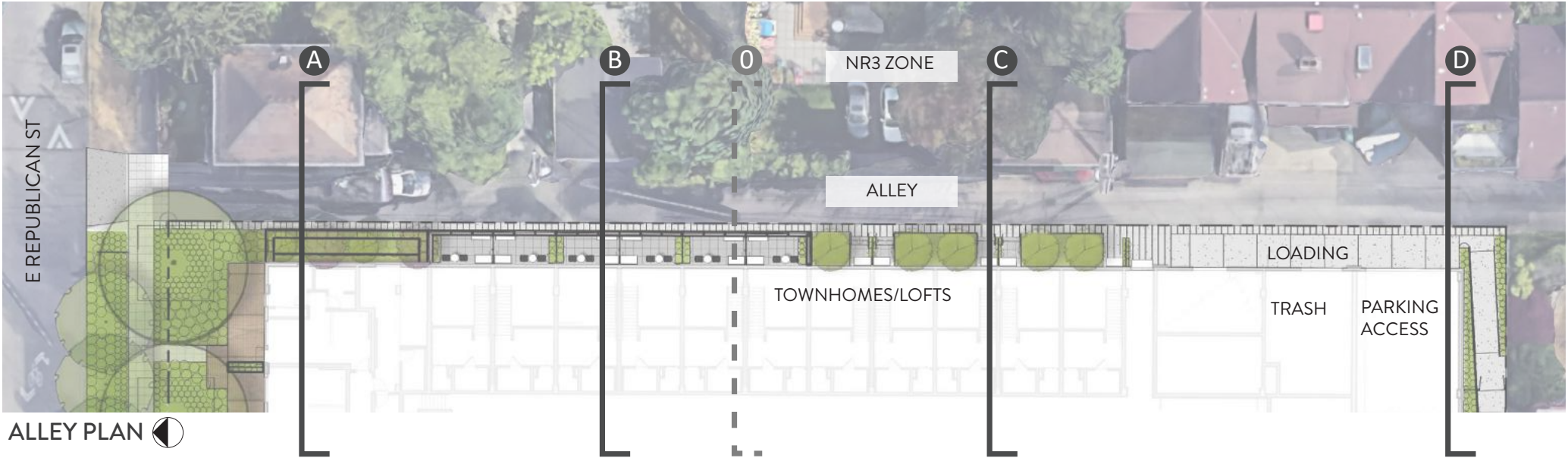
- Throughblock pedestrian activation and landscaping
- Additional wayfinding, landscaping, lighting and signage to mark residential entry
- Landscaping and direct access to units adjacent to residential entry to help activate alley frontage

ALLEY FRONTAGE

ALLEY SECTIONS

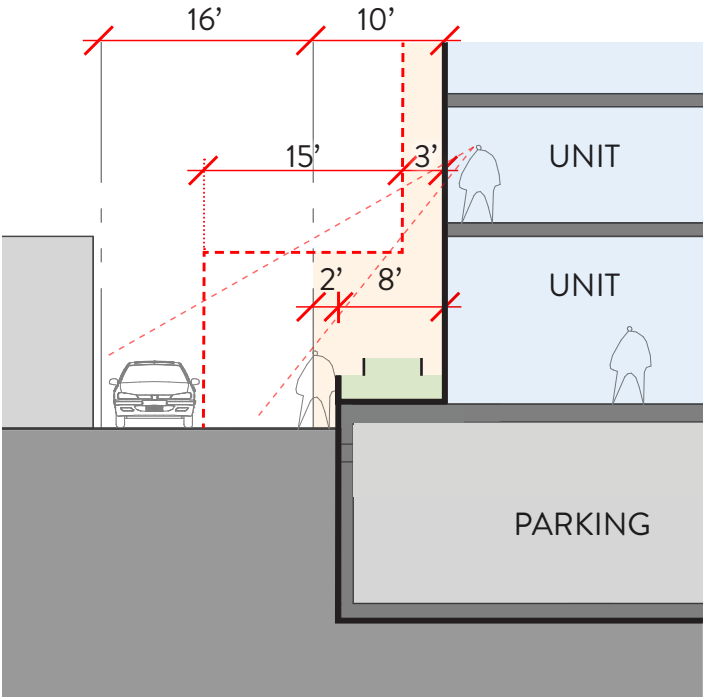
The design responds to the alley conditions to support safety, circulation, and activation by:

- Providing direct alley access for the three southern townhouse units
- Adding trees and landscaping to replace removed plantings and soften the alley facade
- Breaking down the scale of privacy fencing for residential patios

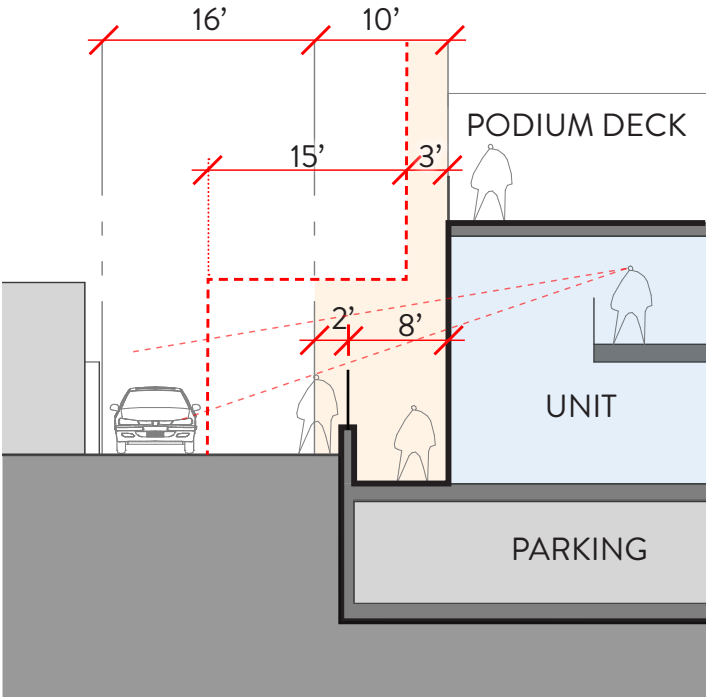


16'

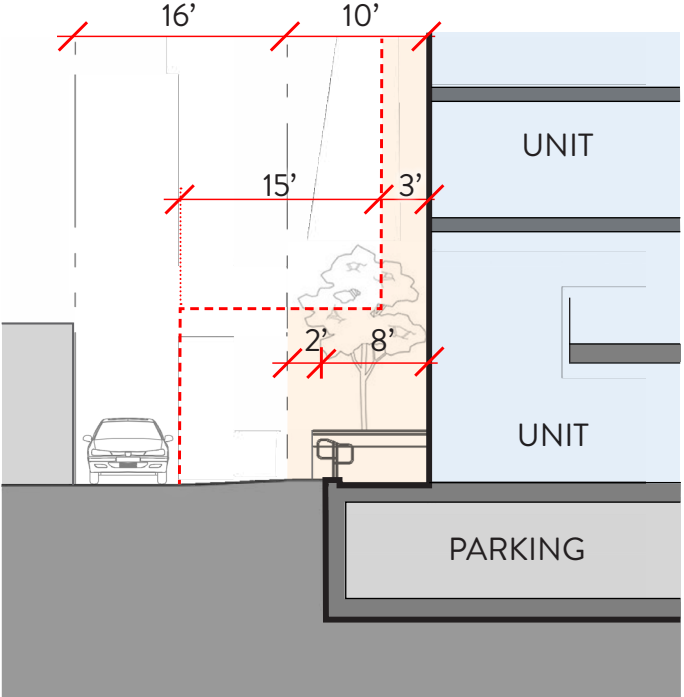
SECTION 0 - EXISTING



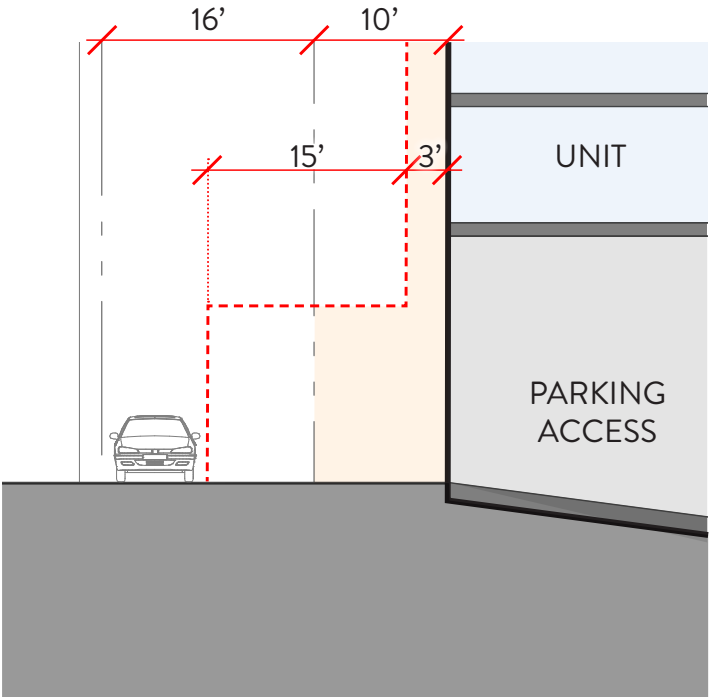
SECTION A - NORTH ALLEY (SIDE YARD)



SECTION B - MID ALLEY (BACK YARD)



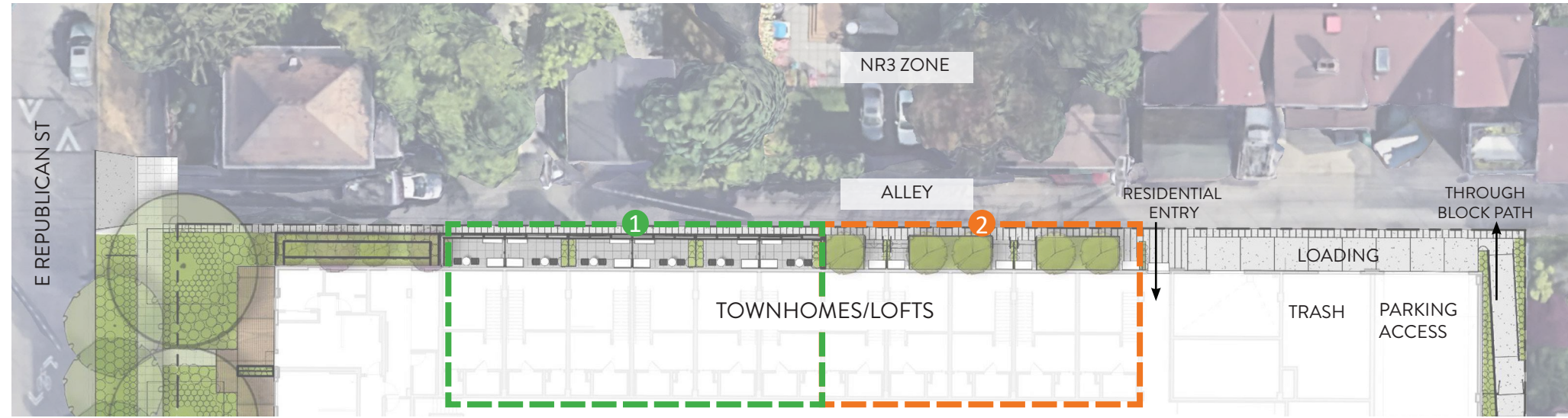
SECTION C - MID ALLEY (FRONT YARD)



SECTION D - SOUTH ALLEY (SERVICE)

ALLEY FRONTAGE

TOWNHOUSE DESIGN DEVELOPMENT



ALLEY PLAN

Since the time of the EDG the townhouse design has been developed to respond to the alley with a variety of units. To respond to the Board's comments, the design team has:

- Explored several design options for the townhouse units to provide direct access from the alley, and considered how residential use along the alley can activate that frontage.
- Refined the landscape design to ensure that the townhouse units had privacy and security, while enhancing the alley condition with landscaping and replacing tree canopy.
- Developed two alley conditions for the townhouse units (back yards and front yards) which respond to the surrounding context across the alley.



1. BACK YARDS (PRIVATE PATIOS)

For the units below the alley grade, a private patio space is provided. Each patio has a fence for safety and security. Since EDG, the proposed fencing has become more permeable, responding to the scale and materiality of the fencing across the alley and emphasize the residential scale of the facade.



2. FRONT YARDS

For the five southernmost units at the alley grade, the design eliminates the occupiable patio space, and provides landscaping at grade with direct access into the unit. This increases the proposed landscaping and softens the residential entry while activating the alley.

ALLEY FRONTAGE

SOUTH ALLEY DESIGN DEVELOPMENT



ALLEY PLAN

Since the time of the EDG the southern alley has been developed to respond to the alley with a variety of units. To respond to the Board's comments, the design team has:

- Developed the southern alley residential entry to improve safety and wayfinding by adding lighting, signage and a landscape buffer from the loading zone.
- Refined the landscape design to prioritize pedestrian safety while moving from the throughblock into the alley by extending the planter to prevent a blind corner into the parking garage.
- Developed the facade materiality along the loading and service area



THROUGHBLOCK PERSPECTIVE

Planter extends into alley to protect pedestrians from garage traffic and prevent a blind corner condition.

Signage and lighting provides wayfinding to the residential entry along the alley.



ALLEY ENTRY PERSPECTIVE

Signage, lighting, and the paving pattern designates the residential entry for wayfinding along the alley.

The residential entry and townhouse unit entry are separated by a planter that provides privacy while maintaining visual connection for safety and alley activation.

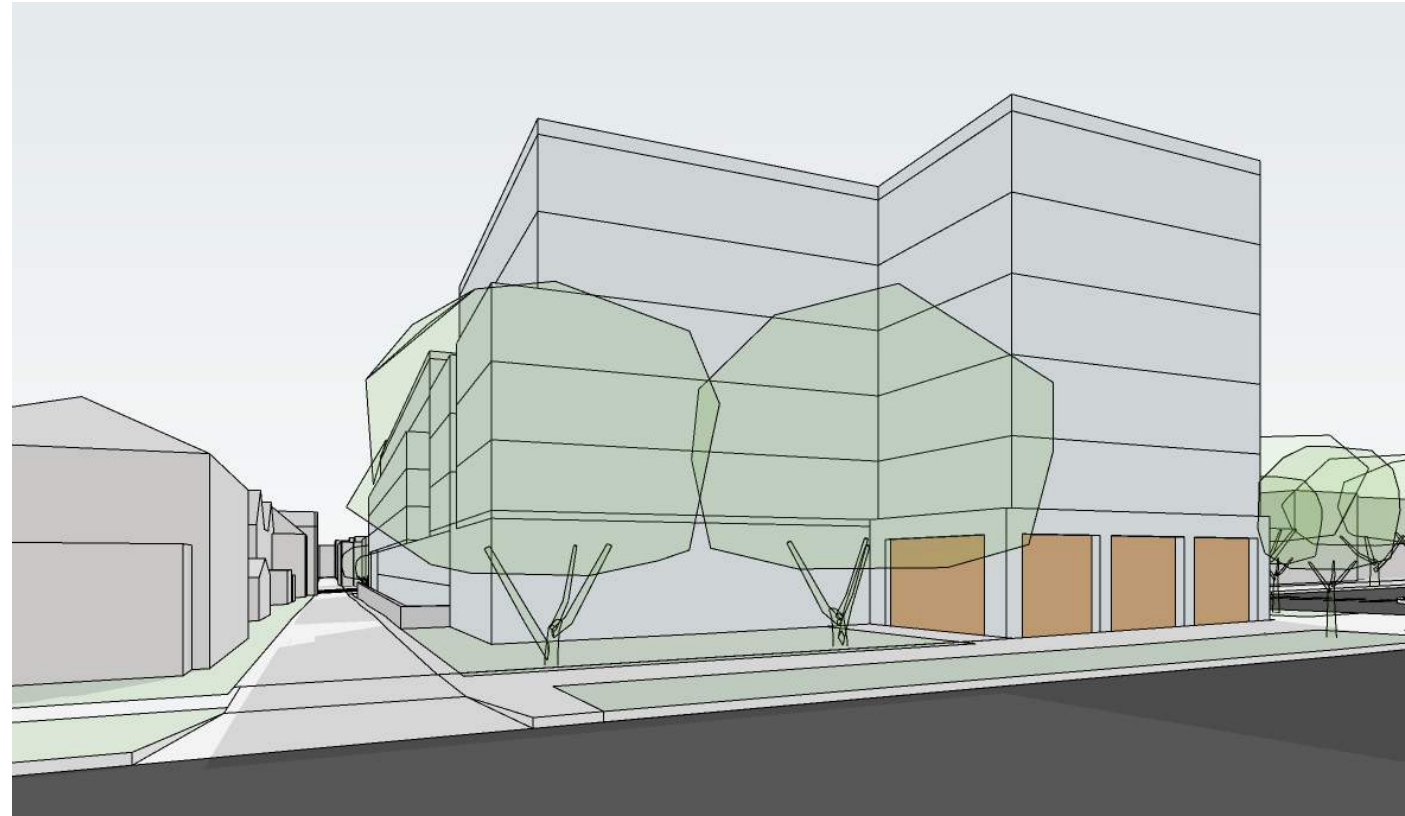
ZONE TRANSITION RESPONSE TO NEIGHBORHOOD CONTEXT



ZONE TRANSITION DIAGRAM LR3 NC2 NR3 TREE CANOPY FRONT YARD

RESIDENTIAL NEIGHBORHOOD CONTEXT
ALONG E REPUBLICAN STREET

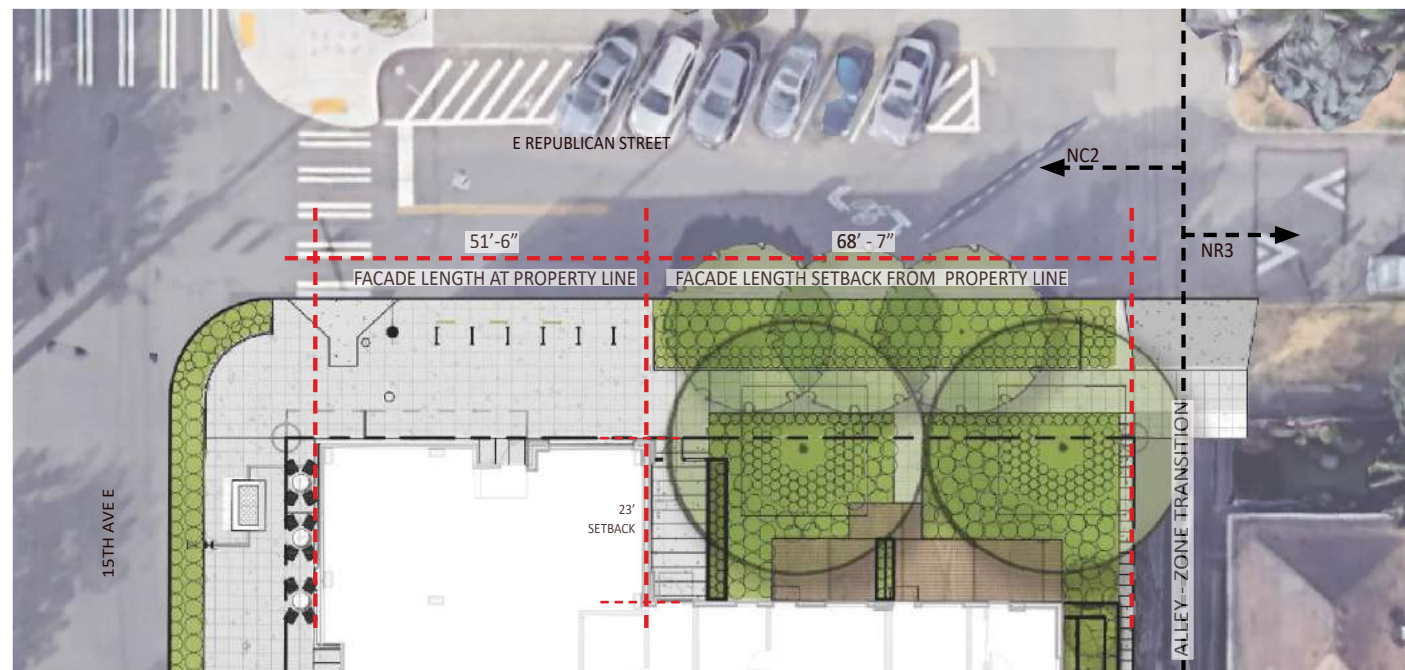




NE MASSING AT EDG



NE MASSING AT DRB



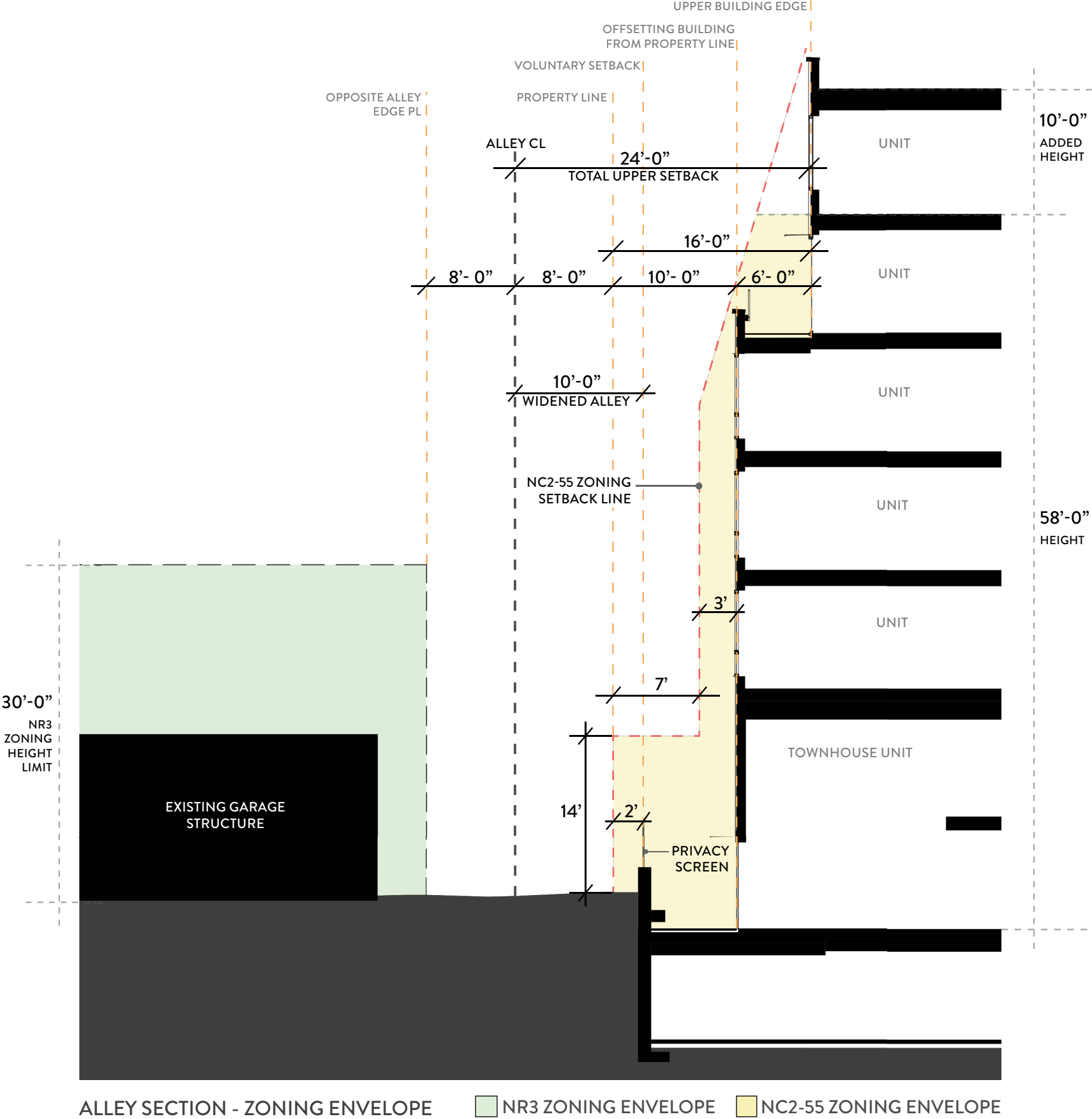
PLAN DIAGRAM

Since the time of the EDG the north facade has developed to respond to the zone transition along East Republican Street. To respond to the Board's comments, the design team has:

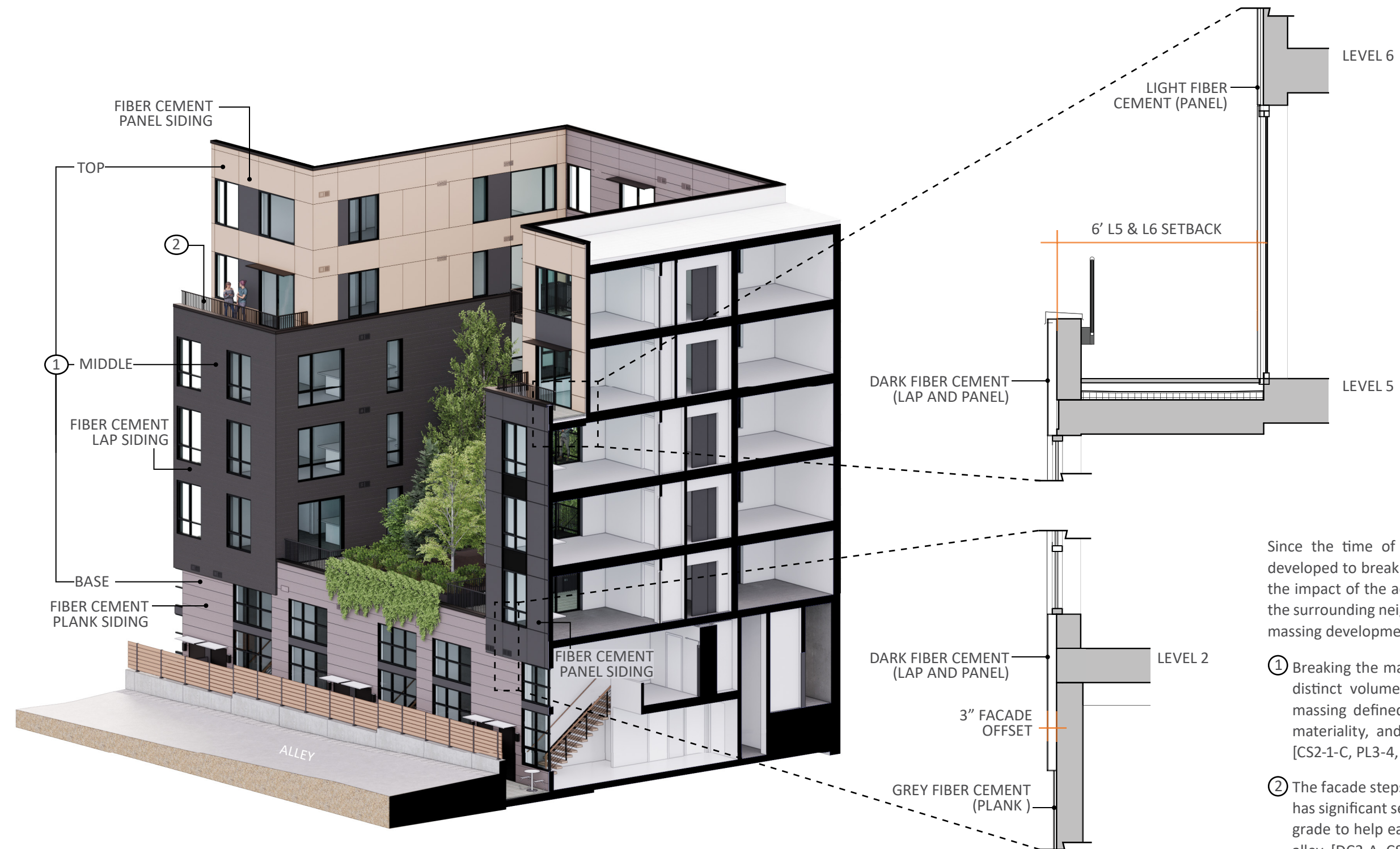
- ① Pulled back the top floor to reduce the perceived mass at the NW corner [CS2-D].
- ② Developed the NE facade material to reflect a more residential scale, breaking down the massing along the alley facade [DC2-4, DC4-1].
- ③ Refined the landscape design to soften the zone transition with greenery, replacing some of the removed trees, and designing planters at a residential scale along the zone transition in the alley [PL3-2, CS1-4].
- ④ Responded to the residential character of the adjacent neighborhood context by extending ground floor residential use further west along East Republican St. This includes matching the adjacent front yard condition across the alley, and preserving the tree canopy [CS2-B, CS2-D-3, DC2-A, PL3-2].
- ⑤ Aligned the proposed curb and sidewalk with the existing curb and sidewalk to create a continuous condition along East Republican Street [PL1-1, CS2-B].

ZONE TRANSITION

ZONING ENVELOPE - ALLEY



ZONE TRANSITION ZONING ENVELOPE - ALLEY COMPARISON



ALLEY MASSING

Since the time of the EDG the alley facade has developed to break down the massing scale, reduce the impact of the additional height, and respond to the surrounding neighborhood context. The building massing development includes:

- ① Breaking the mass of the east facade into three distinct volumes (base, middle, top), with the massing defined by changing the facade color, materiality, and additional facade modulation. [CS2-1-C, PL3-4, DC2-1]
- ② The facade steps back on the top two floors, and has significant setbacks from the property line at grade to help ease the zone transition along the alley. [DC2-A, CS2-D-3]

DESIGN DEVELOPMENT MATERIALS



- WEST
- CANOPY (EXPOSED FRAME)

STOREFRONT (LIGHT-BRONZE ALUMINUM)

BRICK CLADDING (RED)

FIBER CEMENT PLANK: WARM GRAY

BOLT ON BALCONY

FIBER CEMENT PANEL: DARK GRAY

FIBER CEMENT LAP: DARK GRAY

FIBER CEMENT PANEL: LIGHT GREY

BRICK CLADDING (LIGHT)

CANOPY (STEEL PLATE)

BRICK CLADDING (DARK)

STOREFRONT (WOOD)

CANOPY (SLOPED AWNING)

BRICK CLADDING (LIGHT)

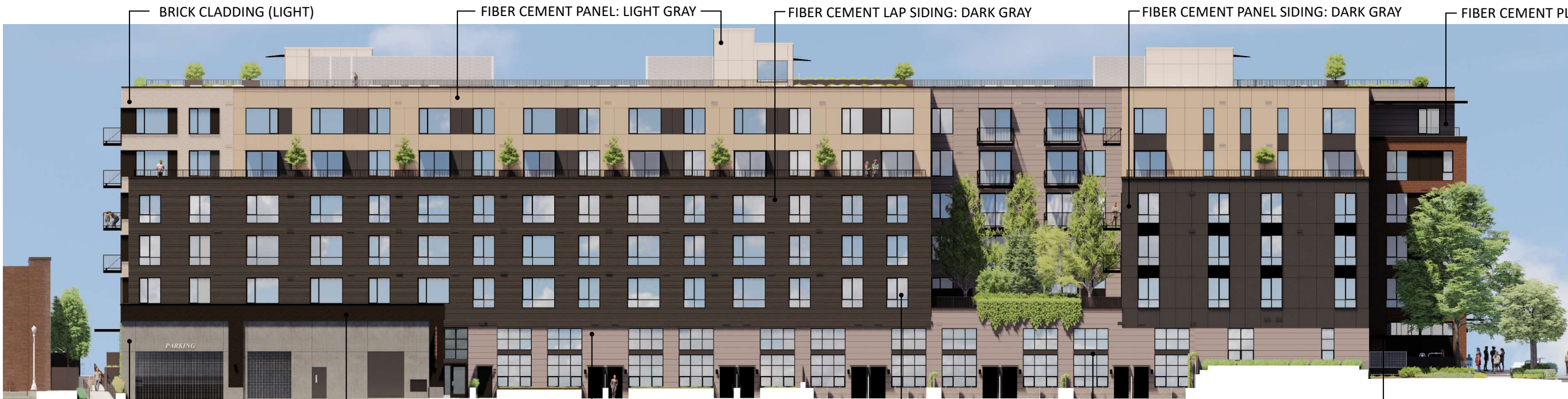
CANOPY (FROSTED GLASS)

STOREFRONT (DARK-BRONZE ALUMINUM, DIVIDED LIGHT)

CANOPY (WOOD-LIKE SOFFIT)

STOREFRONT (DARK-BRONZE ALUMINUM)

BRICK CLADDING (DARK)



- EAST
- BRICK CLADDING (LIGHT)

FIBER CEMENT PANEL: LIGHT GRAY

FIBER CEMENT LAP SIDING: DARK GRAY

FIBER CEMENT PANEL SIDING: DARK GRAY

FIBER CEMENT PLANK: WARM GRAY

CONCRETE (CAST IN PLACE)

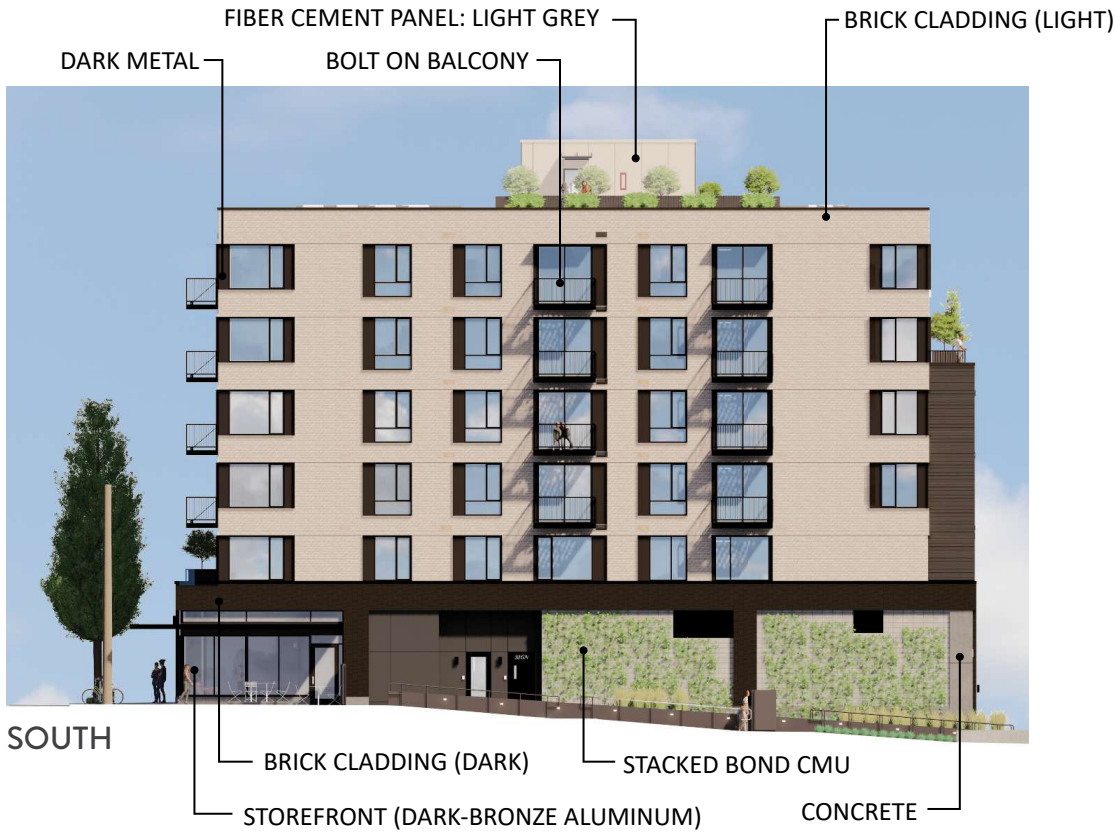
BRICK CLADDING (DARK)

FIBER CEMENT PLANK (WARM GREY)

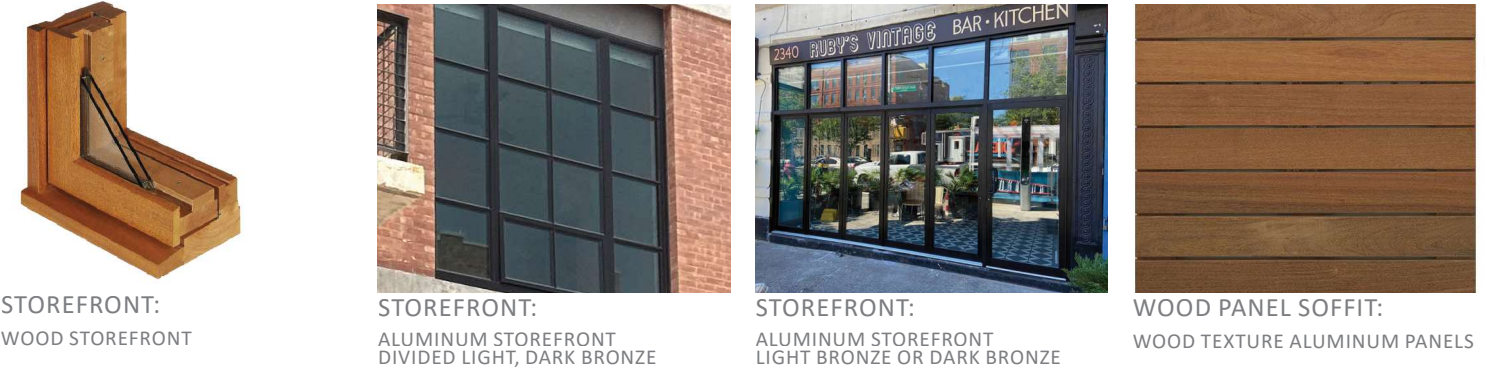
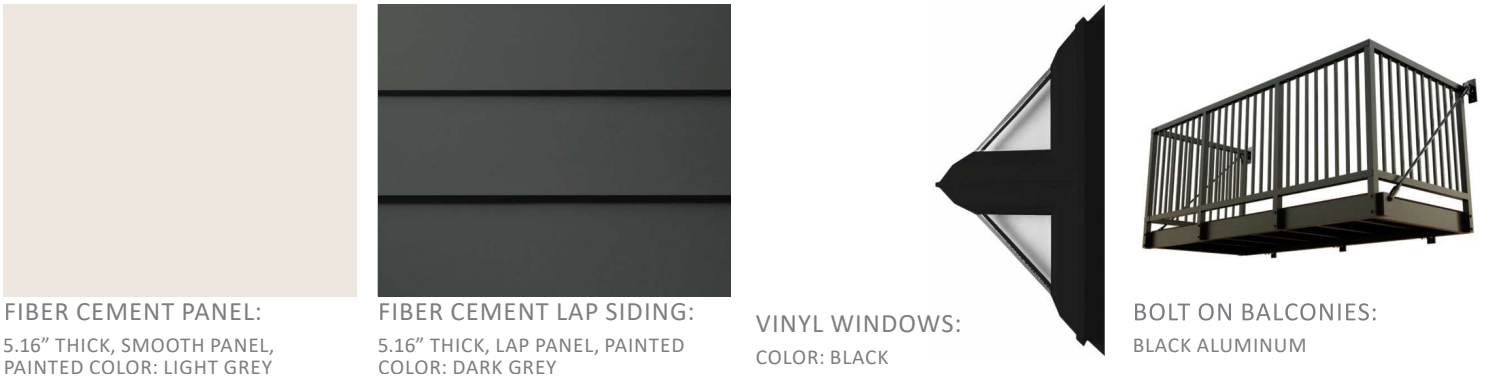
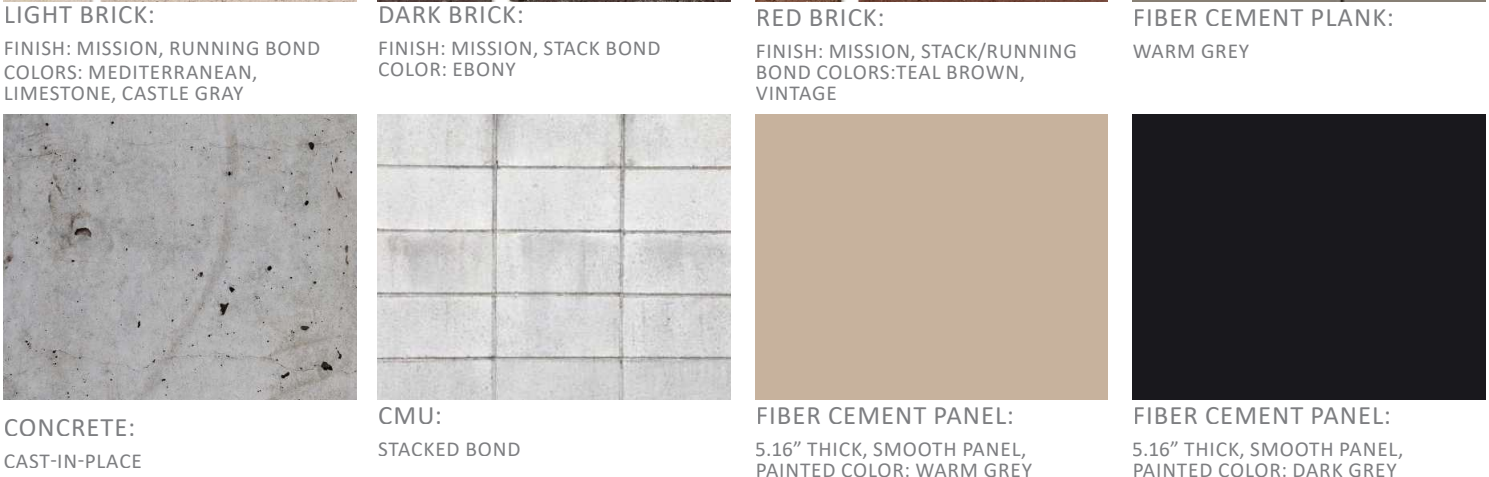
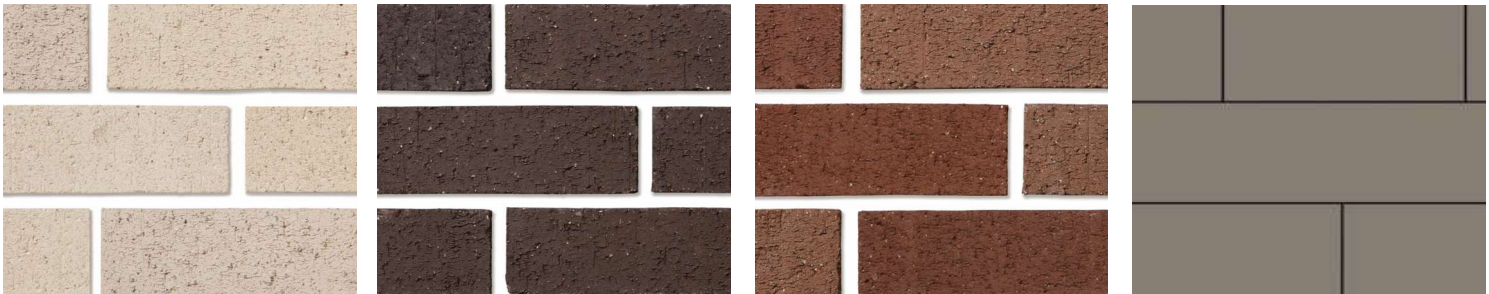
VINYL WINDOWS, TYP.

DARK METAL

BRICK CLADDING (RED)



BASE MATERIALS



DESIGN DEVELOPMENT
LIGHTING PLAN



A Wall Sconce



B Wall Sconce



C Wall Sconce



D Soffit Light



E Planter Wall Light



F LED Bench Light

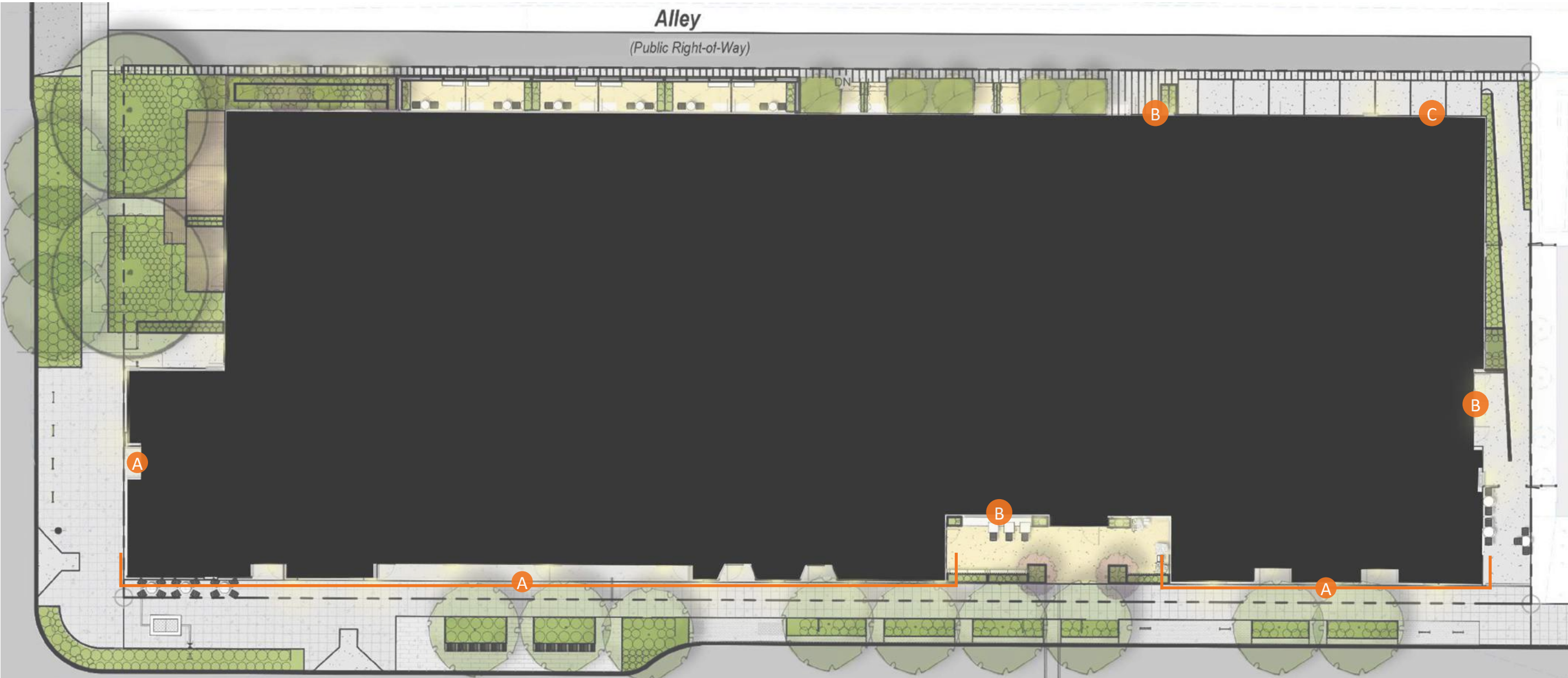


G Signage Light



H LED Accent Light





A RETAIL SIGNAGE



Retail signage will reflect the proposed variety of retail storefronts. Blade style signage may be mounted to the brick piers or suspended from the canopies along 15th.

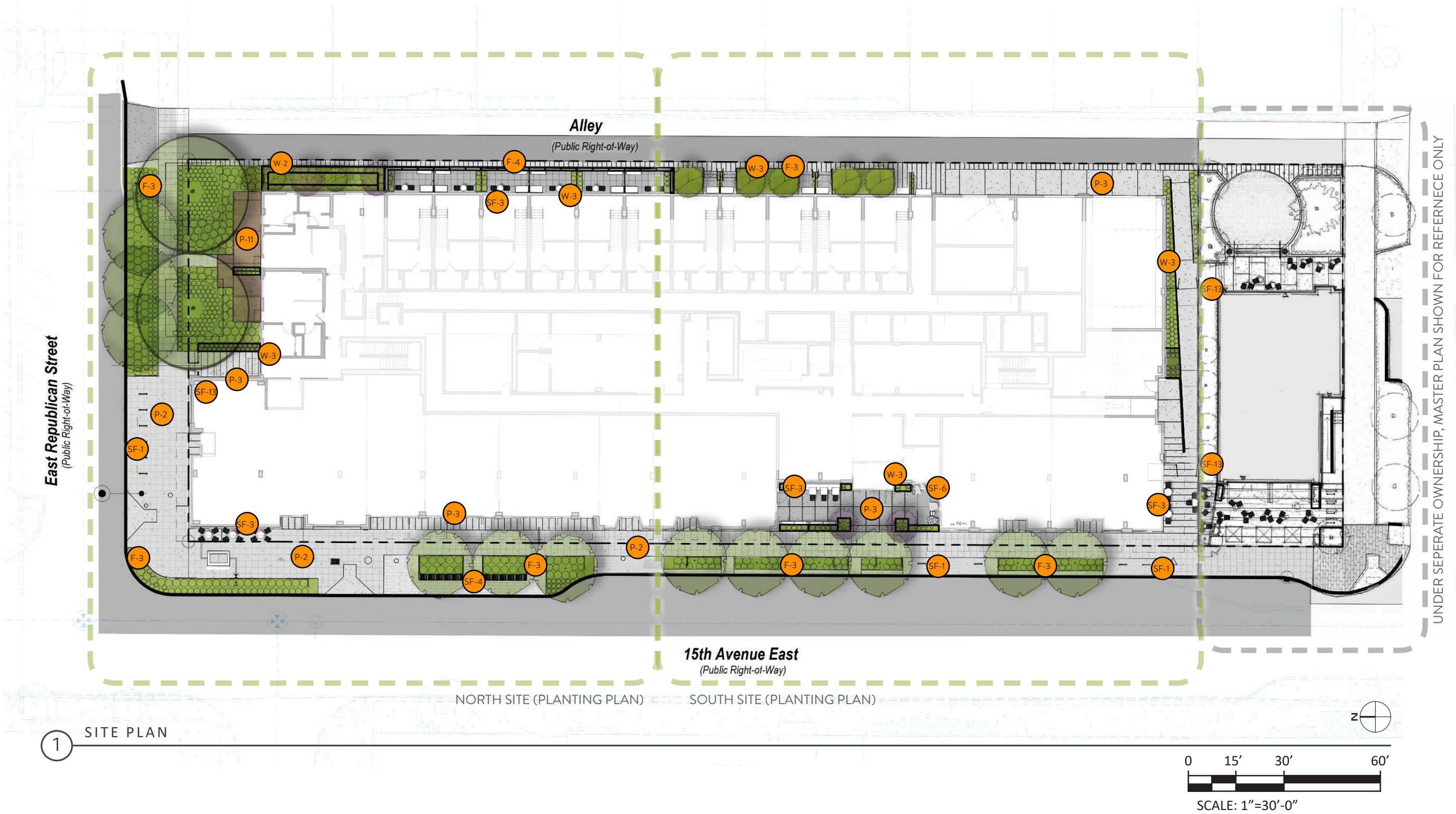
B RESIDENTIAL SIGNAGE



Residential signage will be clear, simple, and bold. Lettering mounted to the cladding at the entry courtyard and beside the door in the alley will mark the residential entrances.

C PARKING SIGNAGE







F-3 18" PLANTING AREA PROTECTION FENCE



F-4 METAL FENCE W/ WOOD PICKETS



W-2 CIP CONCRETE RETAINING WALL



W-3 METAL WALL



P-2 CIP CONCRETE-SDOT STD.



P-3 CIP CONCRETE-ARCHITECTURAL FINISH



P-11 WOOD DECK



SF-1 BIKE RACK



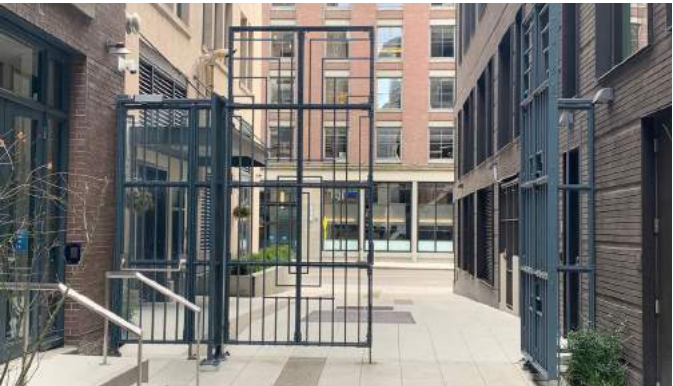
SF-3 MOVEABLE CHAIR AND TABLE



SF-4 BUS LEAN RAIL (CONCEPT)

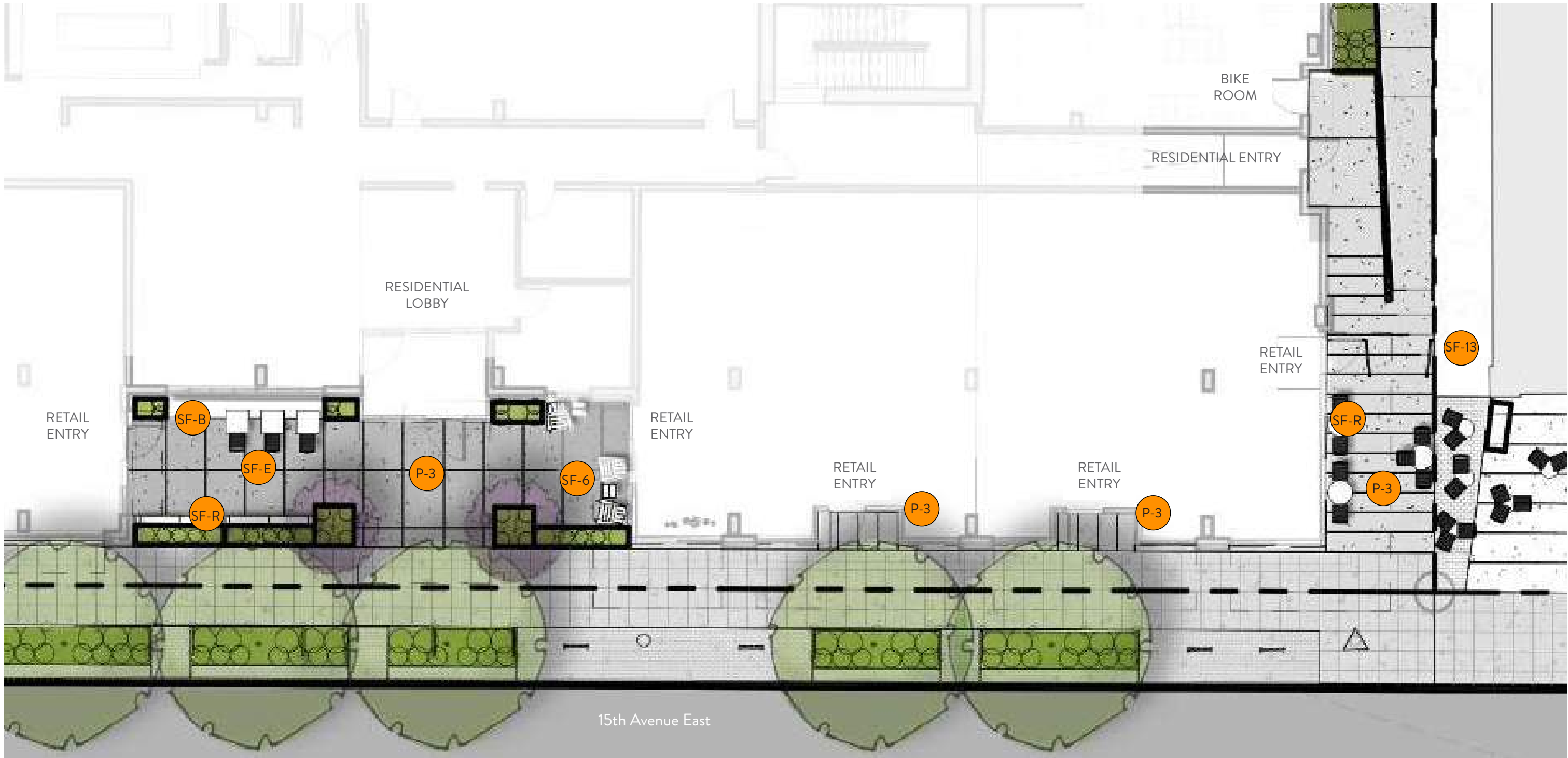


SF-6 LOUNGE CHAIRS

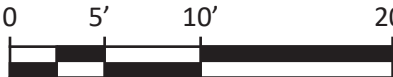


SF-13 PASS THROUGH GATE

ENLARGEMENT
SITE LEVEL - ENTRY | RETAIL



1 ENTRY AND RETAIL ENLARGEMENT



SCALE: 1"=10'-0"

CHARACTER SITE ENTRY & RETAIL



P-3 CIP CONCRETE-ARCHITECTURAL FINISH



SF-B FIXED SEATING AT ENTRY PLAZA



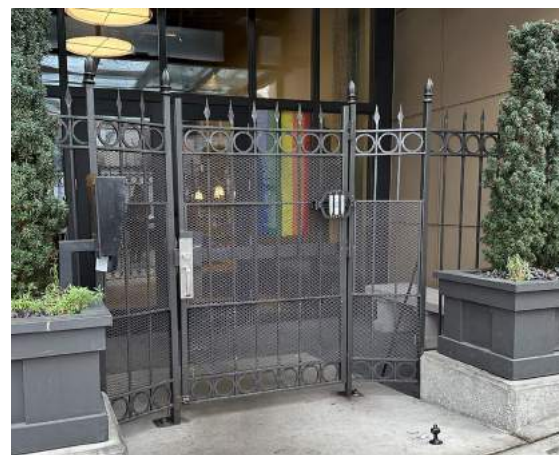
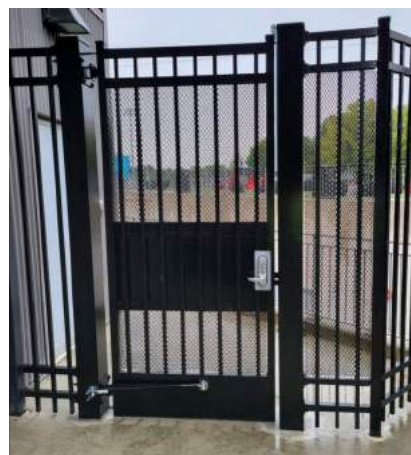
SF-E ENTRY COURTYARD SEATING



SF-R RETAIL SEATING AREAS

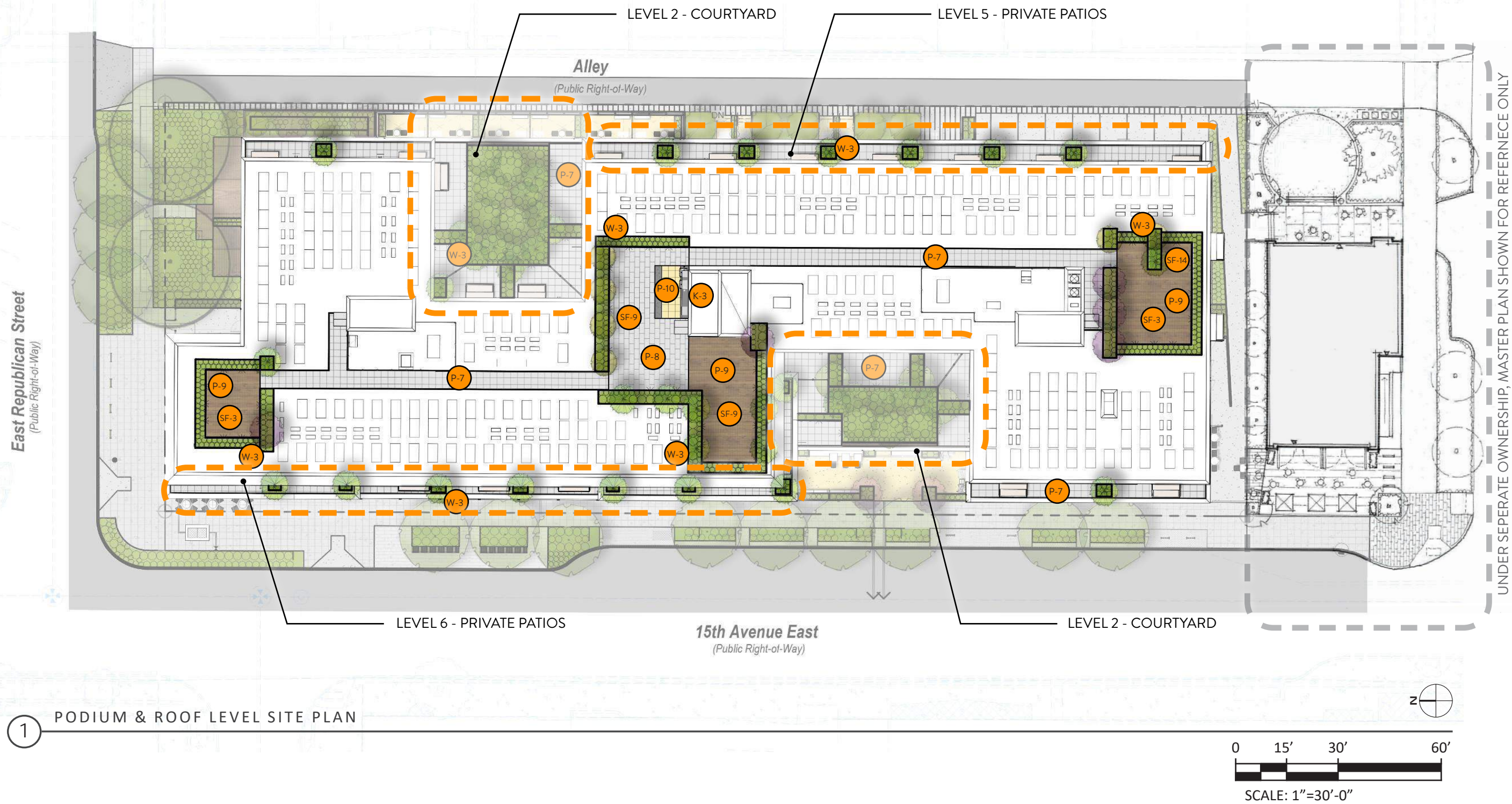


SF-13 PASS THROUGH GATES AND FENCING



W-3 METAL PLANTER

MATERIALS
PODIUM & ROOF LEVEL





W-3 METAL WALL / PLANTERS ON STRUCTURE



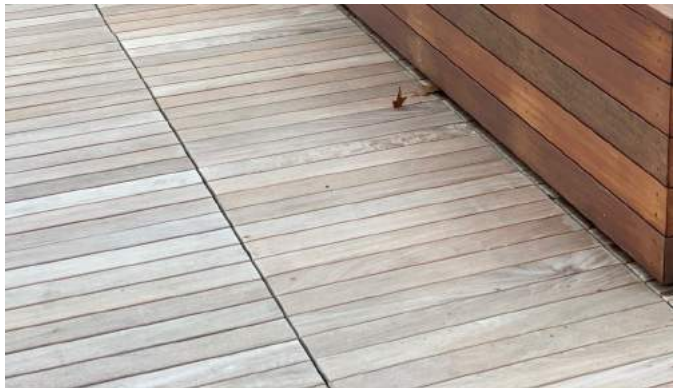
K-3 GRILLING STATION & COUNTER AREA



P-7 PEDESTAL PAVERS (2X2)



P-8 DECORATIVE PEDESTAL PAVERS



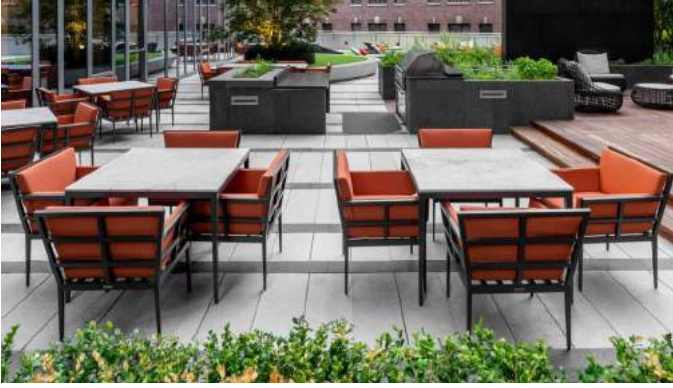
P-9 WOOD PEDESTAL PAVERS (ROOF)



P-10 OUTDOOR KITCHEN PORCELAN PAVERS (ROOF)



SF-3 MOVABLE CHAIR & FIRE PIT (BY OWNER)

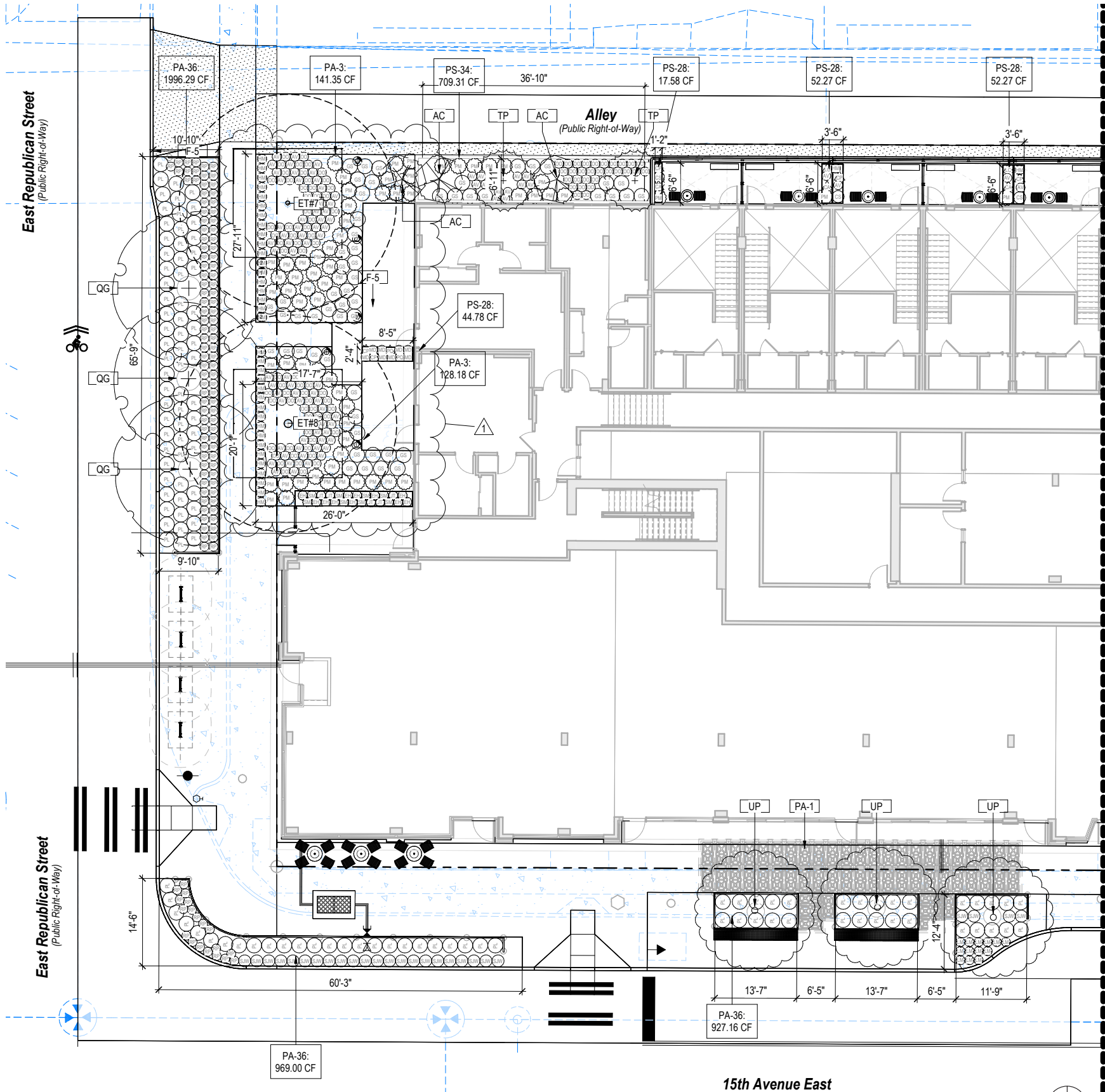


SF-9 MOVABLE TABLES (BY OWNER)



SF-14 MOVABLE HAMMOCK (BY OWNER)

PLANTING
SITE LEVEL



PUBLIC RIGHT-OF-WAY UNDERSTORY PLANTING KEY		
TAG	BOTANICAL NAME	COMMON NAME
SHRUBS		
CO	CHAMAECYPARIS OBTUSA 'NANA GRACILIS'	DWARF HINOKI CYPRESS
PL	PRUNUS LAUROCERASUS 'MT VERNON'	MT. VERNON LAUREL
SC	SARCOCOCCA CONFUSA	SWEET BOX
SJW	SPIRAEA JAPONICA 'WALBUMA'	MAGIC CARPET SPIREA
GROUNDCOVERS		
AV	ARCTOSTAPHYLOS UVA-URSI	KINNIKINNICK
CS	CORNUS SERICEA 'KELSEY'	KELSEY RED TWIG DOGWOOD
IS	IBERIS SEMPERVIRENS	CANDYTUFT
LM	LIRIOPE MUSCARI	CREEPING LILYTURF
FERNS		
PM	POLYSTICHUM MUNITUM	WESTERN SWORD FERN
PS	POLYSTICHUM SETIFERUM	SHIELD FERN
GRASSES		
HM	HAKONECHLOA MACRA	JAPANESE FOREST GRASS

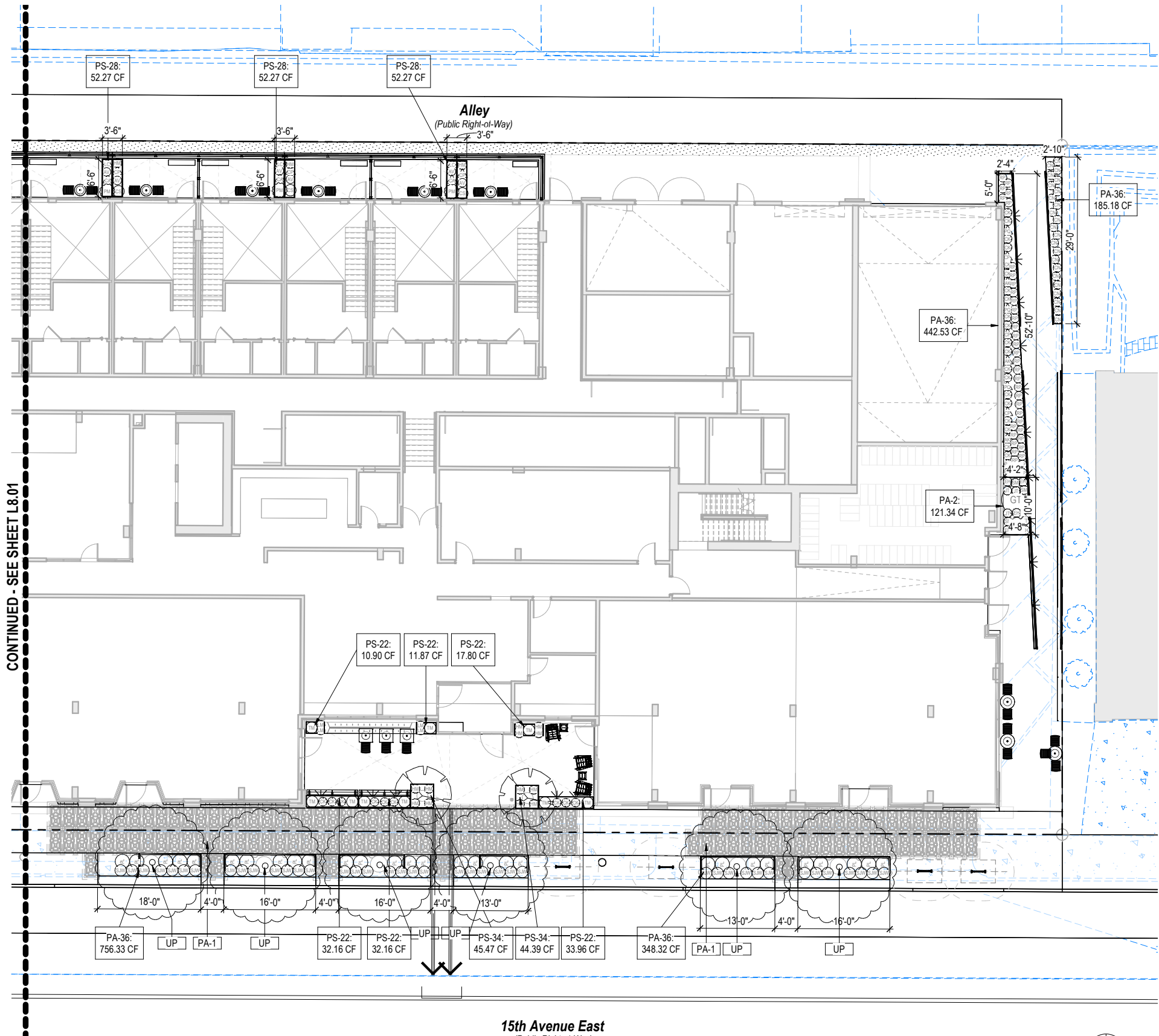
IN-BLOCK UNDERSTORY PLANTING KEY		
TAG	BOTANICAL NAME	COMMON NAME
SHRUBS		
GT	GUNNERA TINCTORIA	GUNNERA
ICC	ILEX CRENATA 'COMPACTA'	COMPACT JAPANESE HOLLY
MA	MAHONIA AQUIFOLIUM	TALL OREGON GRAPE
PL	PRUNUS LAUROCERASUS 'MT VERNON'	MT. VERNON LAUREL
RM	RHODODENDRON MACROPHYLLUM	EVERGREEN HUCKLEBERRY
SC	SARCOCOCCA CONFUSA	SWEET BOX
SD	SPIRAEA DOUGLASII	DOUGLAS SPIREA
SJW	SPIRAEA JAPONICA 'WALBUMA'	MAGIC CARPET SPIREA
TM	TAXUS X MEDIA 'HICKSI'	HICK'S YEW
VO	VACCINIUM OVATUM	EVERGREEN HUCKLEBERRY
GROUNDCOVERS		
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CV	CALLUNA VULGARIS	HEATHER
CS	CORNUS SERICEA 'KELSEY'	KELSEY RED TWIG DOGWOOD
GS	GAULTHERIA SHALLON	SALAL
IS	IBERIS SEMPERVIRENS	CANDYTUFT
LM	LIRIOPE MUSCARI	CREEPING LILYTURF
LP	LUPINUS POLYPHYLLUS	GARDEN LUPINE
MN	MAHONIA NERVOSA	LOW OREGON GRAPE
MD	MAIANTHEMUM DILATATUM	FALSE LILY-OF-THE-VALLEY
OO	OXALIS OREGANA	REDWOOD SORREL
RP	RUBUS PENTALOBUS	CREEPING RASPBERRY
SS	SALVIA SPATHACEA	HUMMINGBIRD SAGE
FERNS		
PM	POLYSTICHUM MUNITUM	WESTERN SWORD FERN
GRASSES		
BG	BOUTELOUA GRACILIS	BLUE GRAMA GRASS
CN	CALAMAGROSTIS NUTKAENSIS	PACIFIC REEDGRASS
CK	CALAMAGROSTIS X ACUTIFLORA 'KARL FOERSTER'	KARL FOERSTER FEATHER REED GRASS
HM	HAKONECHLOA MACRA	JAPANESE FOREST GRASS
NT	NASELLA TENUISSIMA	MEXICAN FEATHERGRASS
EMERGENTS		
EH	EQUISETUM HYEMALE	SCOURING RUSH
JT	JUNCUS TENUIS	SLENDER RUSH
SM	SCRIPUS MICROCARPUS	SMALL-FRUITED BULRUSH
VINES		
AK	ACTINIDIA KOLOMIKTA	VERIEGATED KIWI VINE
CP	CLEMATIS 'PINK FLAMINGO' ATRAGENE GROUP	PINK ALPINE CLEMATIS
CA	CLEMATIS ARMANDII SP.	EVERGREEN CLEMATIS
LC	LONICERA CILIOSA	ORANGE HONEYSUCKLE
PQ	PARTHENOCISSUS QUINQUEFOLIA	VIRGINIA CREEPER

PUBLIC RIGHT-OF-WAY TREE PLANTING SCHEDULE					
TAG	BOTANICAL NAME	COMMON NAME	SIZE	SPACING-TREE	Comments
TREES					
QG	QUERCUS GARRYANA	GARRY OAK	36" BOX	PER PLAN	STREET TREE. SEE STREET TREE PLANTING NOTES
UP	ULMUS PROPINQUA 'JFS BIEBERICH'	EMERALD SUNSHINE ELM	2.5" CAL	MIN 20' OC	STREET TREE. SEE STREET TREE PLANTING NOTES

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BP	BETULA PAPYRIFERA	PAPER BIRCH	5 GAL	PER PLAN	
TP	THUJA PLICATA 'EXCELSA'	WESTERN RED CEDAR	10-12' HT	PER PLAN	

1 SITE PLANTING - NORTH

SCALE: 1"=20'-0"



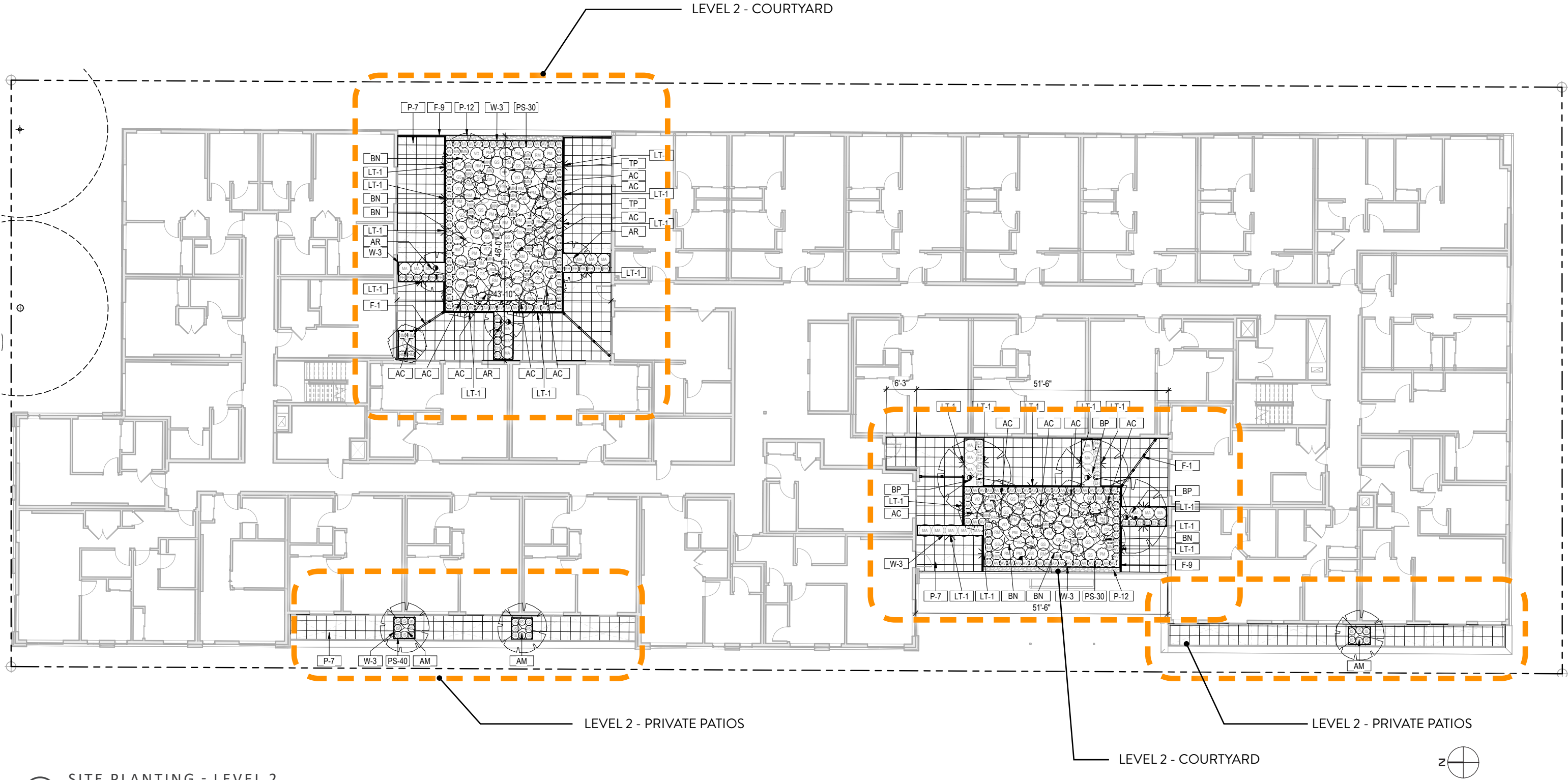
PUBLIC RIGHT-OF-WAY UNDERSTORY PLANTING KEY		
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SHRUBS		
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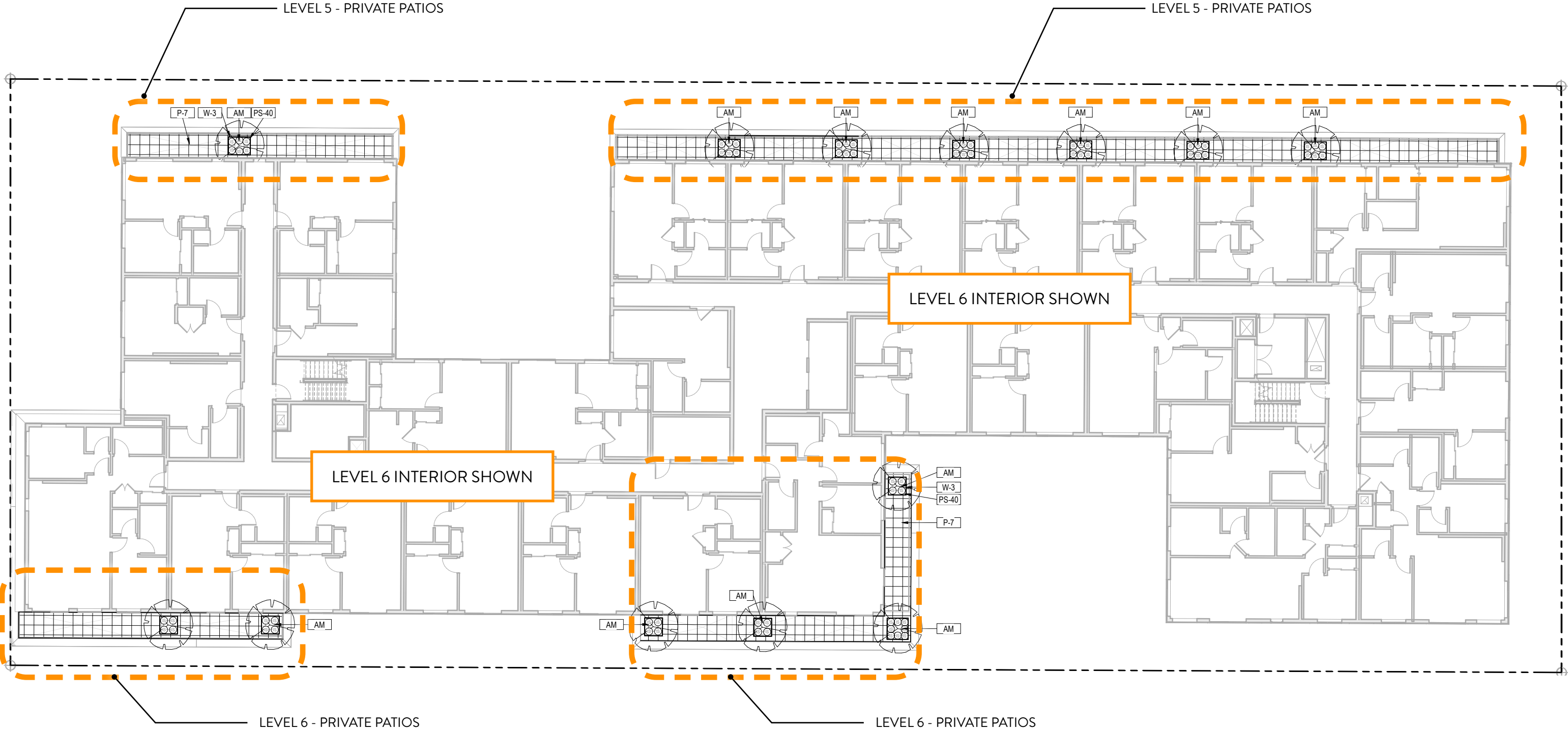
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PLANTING PODIUM LEVEL 2



1 SITE PLANTING - LEVEL 2

SCALE: 1"=20'-0"

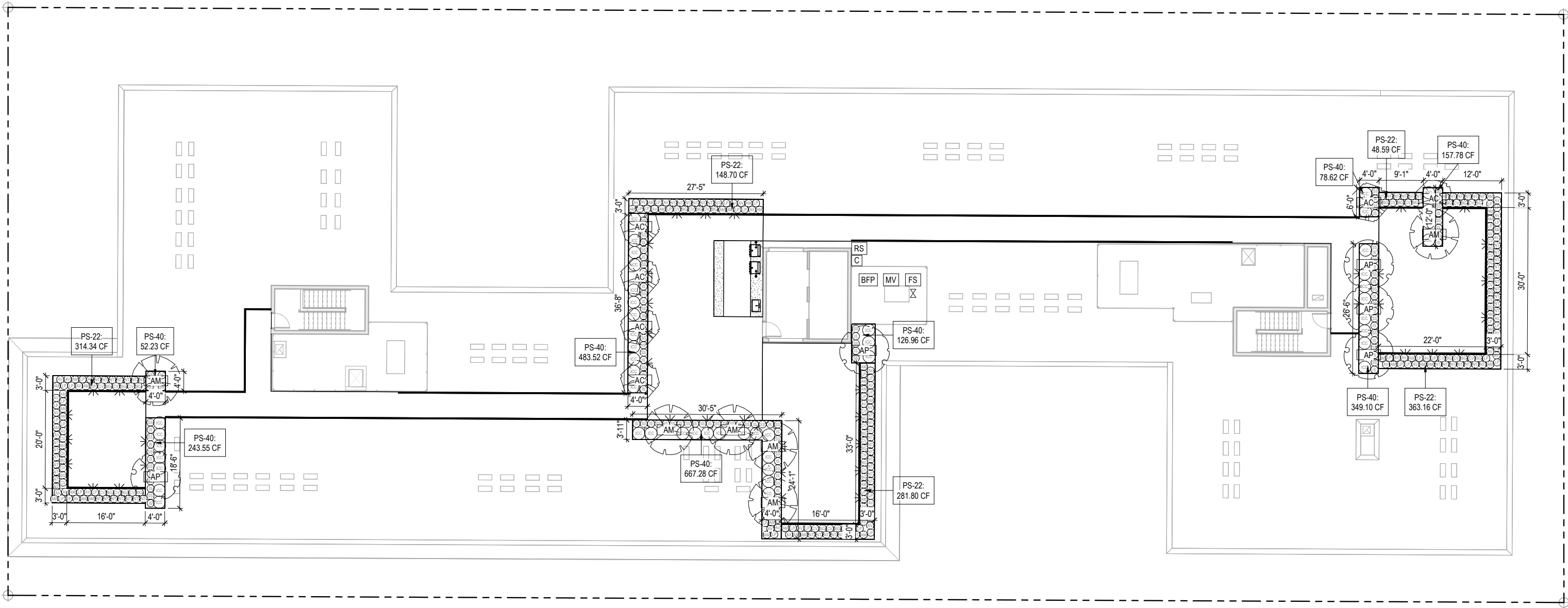


1 SITE PLANTING - FLOOR 5 AND FLOOR 6



SCALE: 1"=20'-0"

PLANTING ROOF LEVEL



1 SITE PLANTING - ROOF



SCALE: 1"=20'-0"

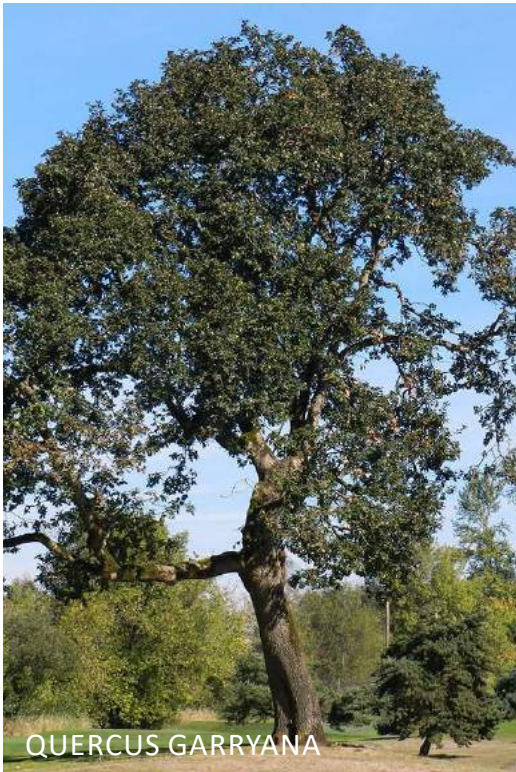
PLANTING

PRECEDENT IMAGES - TREES



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PLANTING
PRECEDENT IMAGES - UNDERSTORY

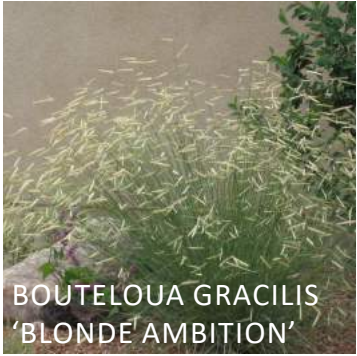
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SHRUBS



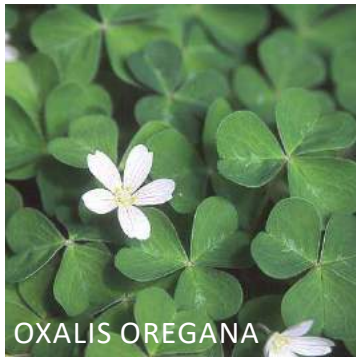
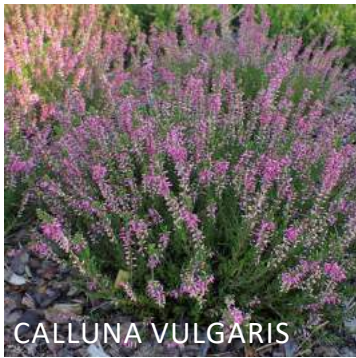
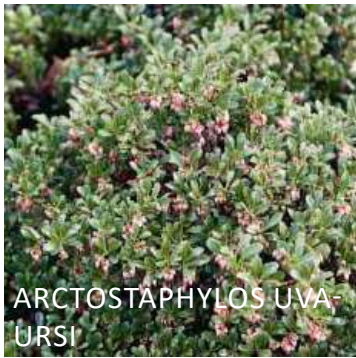
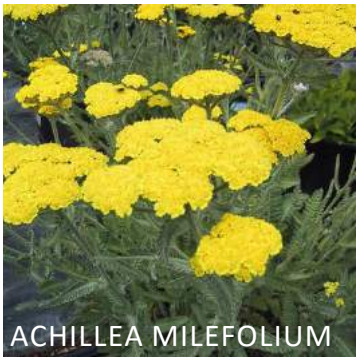
GRASSES



VINES



GROUNDCOVERS



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DEPARTURES

SUMMARY - OVERHEAD WEATHER PROTECTION

DEPARTURE REQUEST CODE REQUIREMENTS

23.47A.008.C.4 OVERHEAD WEATHER PROTECTION

Continuous overhead weather protection is required along at least 60% of the street frontage of a structure on a principal pedestrian street. The covered area shall have a minimum width of 6 feet, unless there is a conflict with existing or proposed street trees, or utility poles, in which case the width may be adjusted to accommodate such features as provided in subsection 23.47A.008.C.4.F. The overhead weather protection must be provided over the sidewalk, or over a walking area within 10 feet immediately adjacent to the sidewalk. The lower edge of the overhead weather protection shall be a minimum of 8 feet and a maximum of 12 feet above the sidewalk.

PROPOSED CONDITION

To reduce the coverage of code compliant overhead weather protection from 60% to 45%. The project provides a total of 78.12% of overhead weather protection along the 15th Ave street frontage. Of that total coverage, 45.8% is code compliant with a minimum depth of 6’ (184 L.F. required, 140.89 L.F. proposed). The project proposes an additional 32.32% of overhead weather protection that does not meet the minimum 6’ depth requirement (99.26 L.F.).

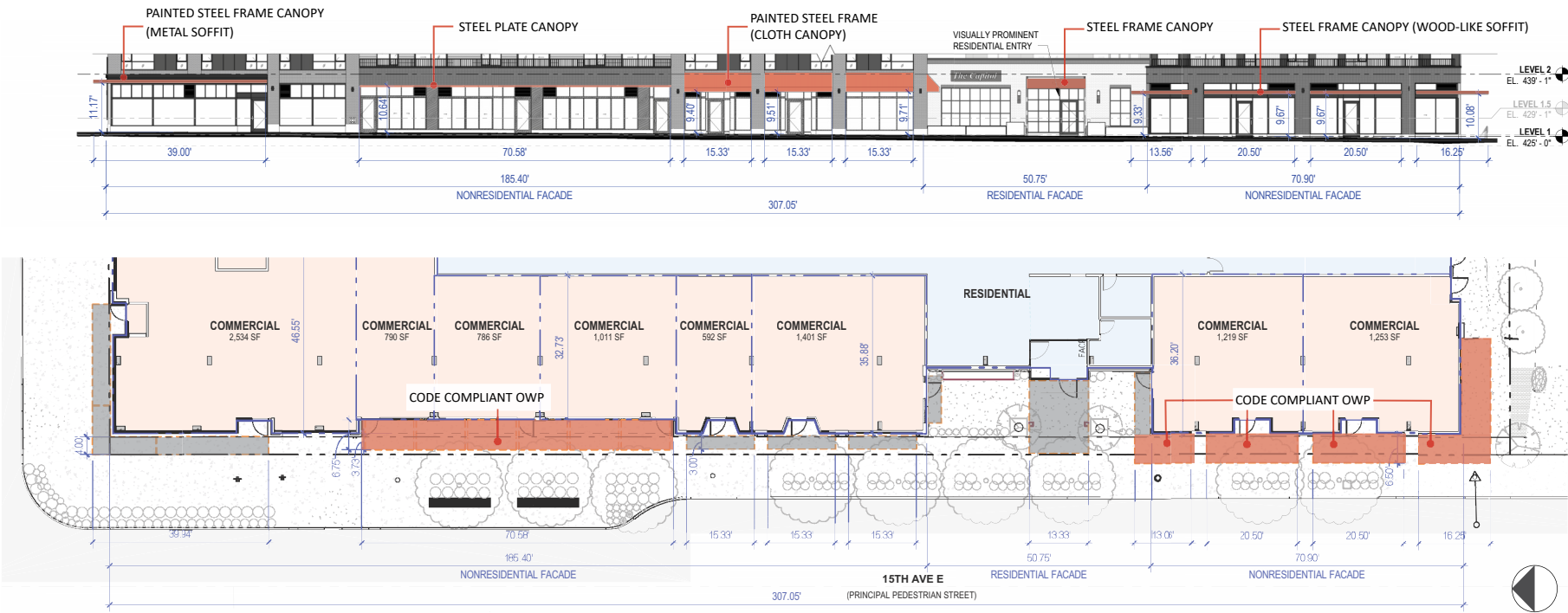
ASSOCIATED GUIDELINES

- CS2 Urban Pattern & Form
- CS3 Architectural Context & Character
- PL2 Walkability

BOARD COMMENTS

- At EDG, the Board was concerned with maintaining overhead weather protection at retail entries; they could support a reduction in depth but not length (frontage extent).

JUSTIFICATION FOR DEPARTURE REQUEST



CANOPY DEPARTURE

- The proposed overhead weather protection has been designed with varying widths and styles to contribute to the existing variety of pedestrian scaled storefronts along the 15th Avenue East Corridor. In addition to the 45.8% of code compliant overhead weather protection, the project proposes 32.32% of overhead weather protection that is less than 6’ wide (99.26 L.F.) The variety of proposed canopies will help break down the scale of the 15th Ave. E. frontage and contribute to a varied pedestrian experience (**CS2.1.C - 15TH AVENUE CORRIDOR; PL2.C.2 - DESIGN INTEGRATION**).
- Where the overhead weather protection is less than 6 feet wide, the design proposes recessed retail entries which provide at least 6 feet of protection in conjunction with the adjacent overhead weather protection to prevent gaps at the commercial entries (**PL2.3.A - WEATHER PROTECTION; PL2.C.1 - LOCATIONS AND COVERAGE**).
- The requested departure better aligns with neighborhood guidance to create a pedestrian experience along 15th Ave that reflects the character of the distinctive street and existing retail character (**CS2.1C - 15TH AVENUE CORRIDOR**) by encouraging facade detailing that contributes to intimate and varied retail character.
- We are proposing a range of storefront sizes in this project to mimic the existing variety of scale, color and materiality already present along the street (**CS3 - ARCHITECTURAL CONTEXT AND CHARACTER**).

DEPARTURE REQUEST CODE REQUIREMENTS

23.54.030.B.1.B PARKING SPACE REQUIREMENTS

When more than five parking spaces are provided, a minimum of 60% of the parking spaces shall be striped for medium vehicles. The minimum sizes for a medium parking space shall also be the maximum size. 40% of the parking spaces may be striped for any size category in subsection 23.54.030.A provided that when parking spaces are striped for large vehicles, the minimum required aisle shall be as shown for medium vehicles.

PROPOSED CONDITION

The project has 101 residential parking stalls. This means 61 of them must be striped for medium vehicles to meet the 60% requirement. The project proposes 49 compact, and 49 medium stalls, lowering the percentage of medium stalls to 48%

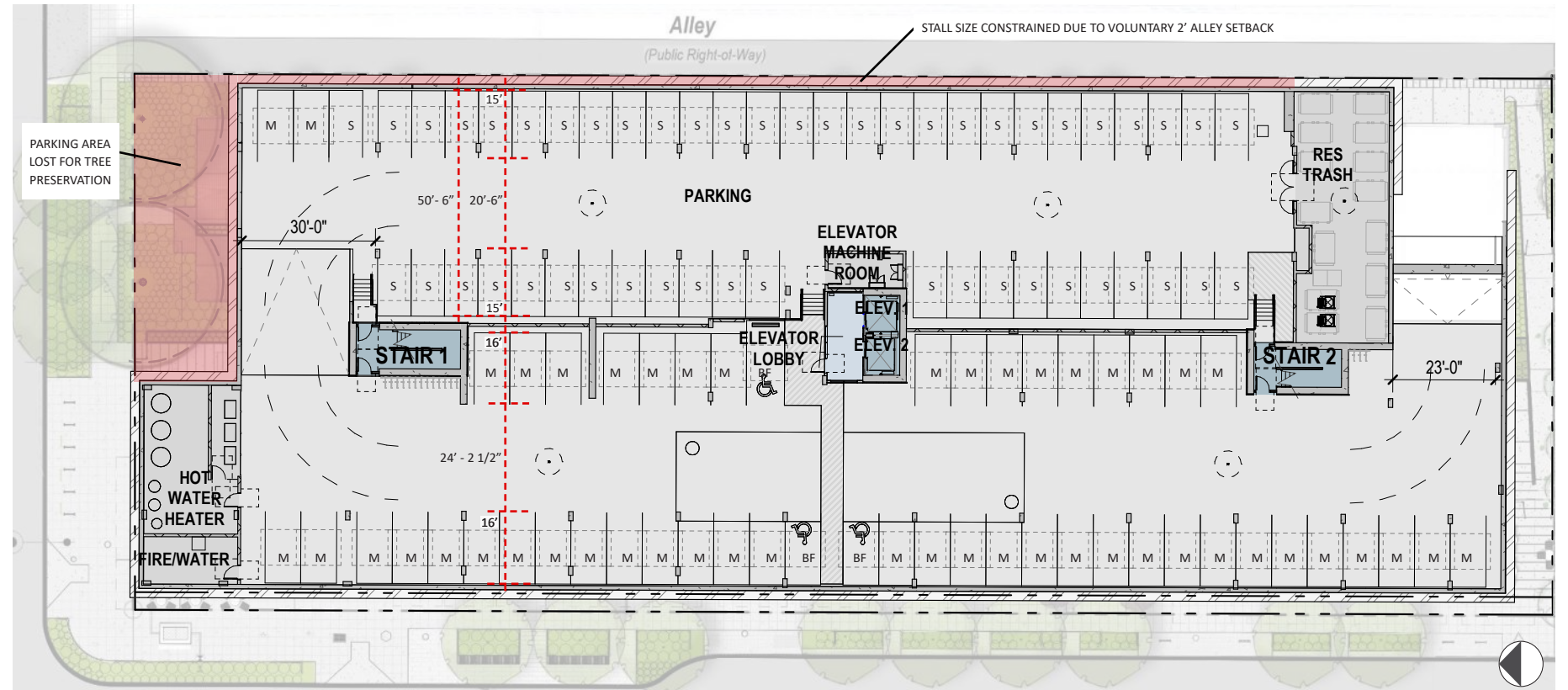
ASSOCIATED GUIDELINES

- CS2 Urban Pattern & Form
- DC1 Project Uses & Activities
- PL3 Street-Level Interaction

BOARD COMMENTS

- The need for this departure was not known at the time of EDG review.

JUSTIFICATION FOR DEPARTURE REQUEST



PARKING STALL SIZE DEPARTURE

- The reduction of medium striped parking stalls below grade prioritizes the NE corner setback at grade, which is needed for the proposed tree preservation and helps ease the zone transition at the NE corner (**CS2.D.3 - HEIGHT, BULK, AND SCALE: ZONE TRANSITIONS**).
- The space for medium striped stalls is reduced further due to the proposed voluntary 2' setback from the alley. This setback will help increase safety and security for pedestrians and vehicles (**DC1.B - VEHICULAR ACCESS AND CIRCULATION**), and increases the setback between residential units at grade and the property line (**PL3.2.A - RESIDENTIAL EDGES**).
- The required measurement for medium stalls is 8 feet in width and 16 feet in length. The required measurement for small stalls is 7.5 feet in width and 15 feet in length [Per 23.54.030.A]. The required measurement for the drive aisle serving medium striped stalls is 22 feet [Per 23.54.030.E]. The width of the parking aisle and stalls along the east facade is 50'-6" so it cannot accommodate the required sizes for medium striped stalls and the drive aisle.

DEPARTURE REQUEST CODE REQUIREMENTS

23.41.012.B.10 FLOOR AREA RATIO

- Departures of up to an additional 0.5 FAR may be granted if the applicant demonstrates that: 1) the departure is needed to protect a tree that is located on the lot that is either a Tier 2 tree, as defined in section 25.11.020, or a tree greater than 2 feet in diameter measured 4.5 feet above the ground; and 2) avoiding development in the tree protection area will reduce the total development capacity of the site.
- FAR Calculation:
Setback to Preserve Tree = 1,566 x 5 Stories = 7,830 SF (Approx. 12 Units)
FAR Lost To Preserve Tree = 7,830/38,129 = .20 FAR

23.41.012.B.11 STRUCTURE HEIGHT

Departures of up to 10 feet of additional height may be granted if the applicant demonstrates that: 1) the departure is needed to protect a tree that is located on the lot that is either a Tier 2 tree, as defined in section 25.11.020, or a tree greater than 2 feet in diameter measured 4.5 feet above the ground; and 2) avoiding development in the tree protection area will reduce the total development capacity of the site.

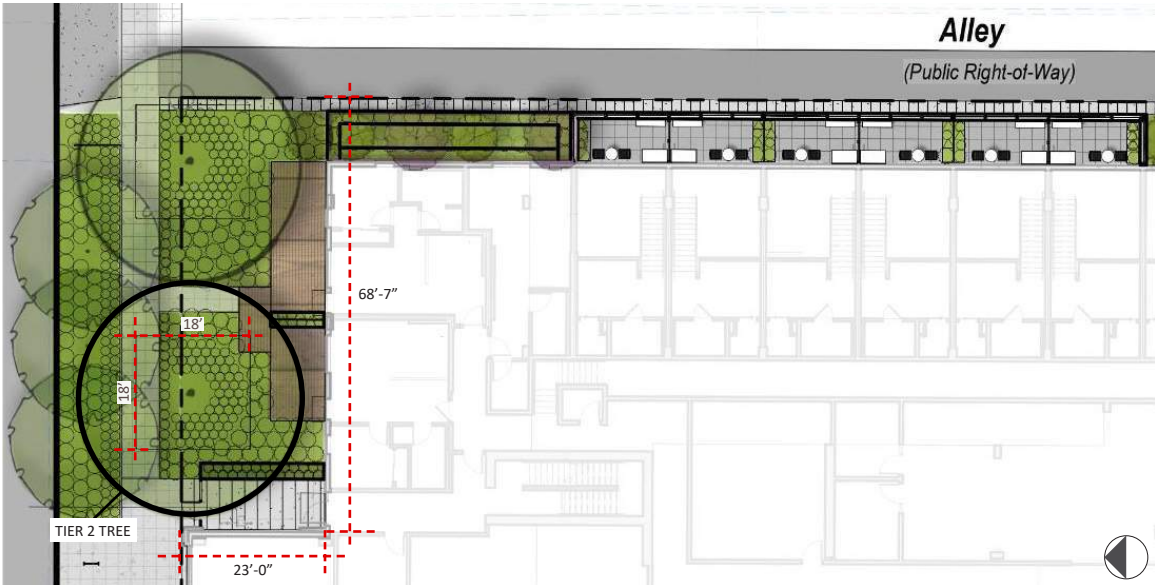
PROPOSED CONDITION

- The project proposes increasing the height limit by 10'-0" and the maximum FAR from 3.75 to 4.3 to allow for a 6th story. This will minimize the development impact from preserving a Tier 2 tree located on the northeast boundary of the site.

ASSOCIATED GUIDELINES

- CS1 Natural Systems & Site Features
- CS2 Urban Pattern & Form
- PL3 Street Level Interaction
- DC2 Architectural Concept
- DC3 Open Space Concept

JUSTIFICATION FOR DEPARTURE REQUEST

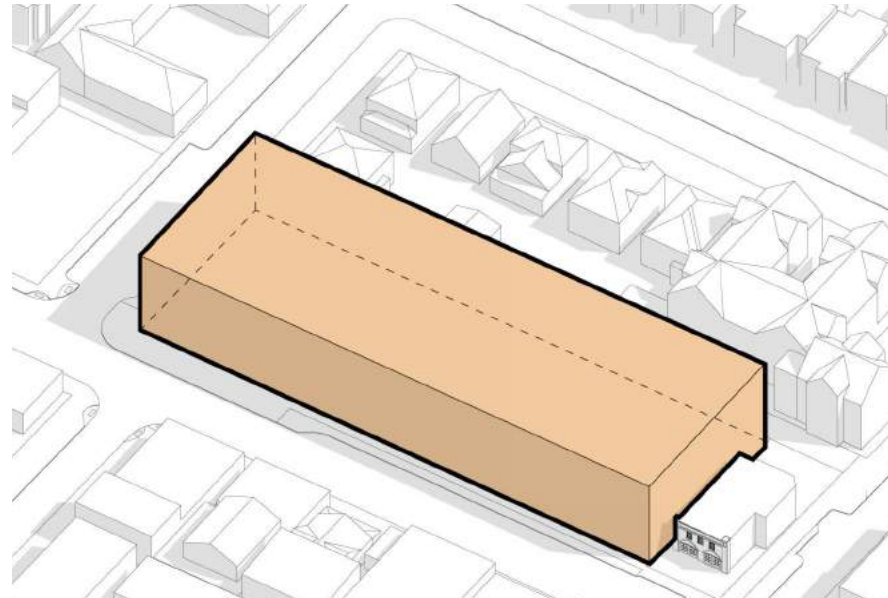


PRESERVATION OF TIER 2 TREE

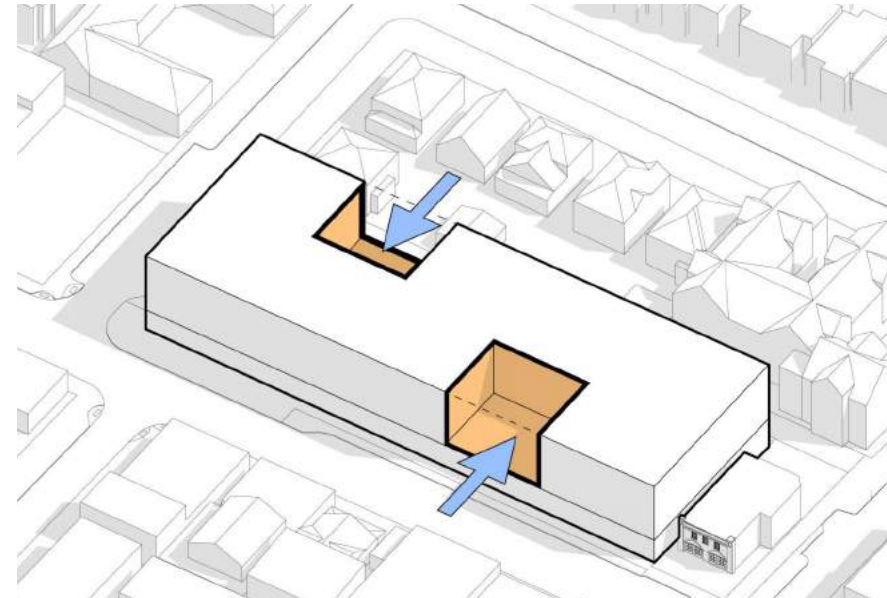
- The proposed design better aligns with neighborhood design guidance to preserve existing tree canopy by preserving two existing trees along the northern site boundary. This design protects the health and longevity of existing trees and benefits the public realm by enhancing the street space for all, not just open space privatized for the building residents (**CS1.4.E - PLANTS & HABITAT; CS1.D.1 - ON SITE FEATURES: EXISTING TREES**).
- Preserving the trees creates a massing break that will help ease the zone transition between commercial and residential uses along E. Republican Street by creating setbacks along the street frontage and alley (**CS2.D.3 - ZONE TRANSITIONS**). The massing response acknowledges the unique street character created by the zone transition along E. Republican Street (**CS2.D.4 - MASSING CHOICES; CS2.D.5 - RESPECT FOR ADJACENT SITES; DC2.A.1 - SITE CHARACTERISTICS AND USES; DC2.A.2 - REDUCE PERCEIVED MASS**).
- This design creates an active corner retail condition (**CS2.C - 15TH AVENUE CORRIDOR**) at the NW corner that eases with setbacks and landscaping to a quieter residential edge at the NE corner. The additional height and perceived bulk is eased by significant setbacks adjacent to the alley, pulling back the sixth floor along each facade (**CS2.D.3 - HEIGHT, BULK, AND SCALE: ZONE TRANSITIONS**). The frontage design generated by preserving the trees creates an opportunity to carry the adjacent residential front yard setback and wide planting strip across the alley to our site, which carries the residential open space pattern further west along E. Republican Street (**DC3.2.A - OPEN SPACE**).
- Preserving the tier 2 tree would help create a layered transition using the trees and additional landscaping setback to buffer the ground floor residential units along the north facade (**PL3.2.A - RESIDENTIAL EDGES; PL3.1.A.D - INDIVIDUAL ENTRIES TO GROUND RELATED HOUSING; DC3.C.1 - REINFORCE EXISTING OPEN SPACE**).

BOARD COMMENTS

- The Board was supportive of the departures requested to preserve the tree at the time of EDG, provided the added height was modulated to support the zone transition.

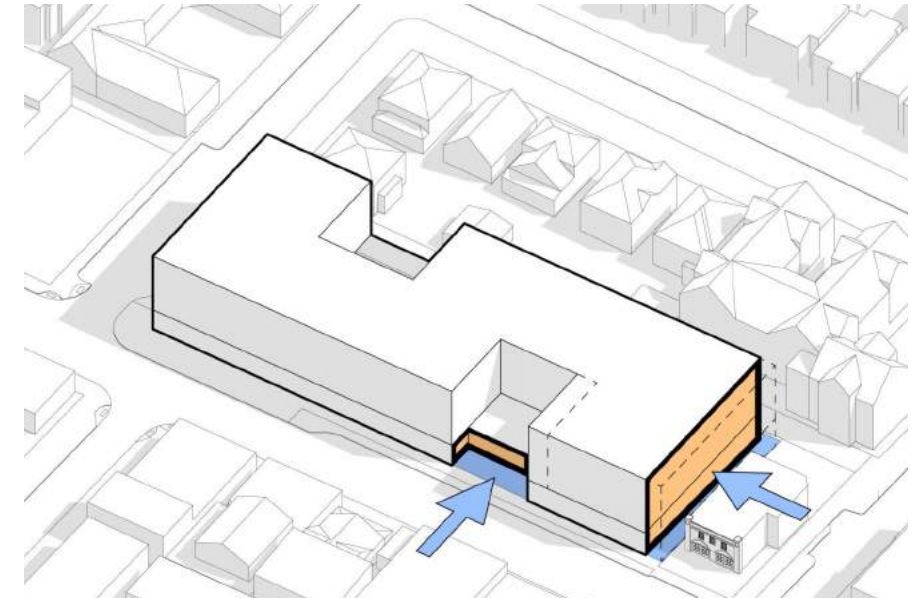


① Lot Perimeter & Allowable Height



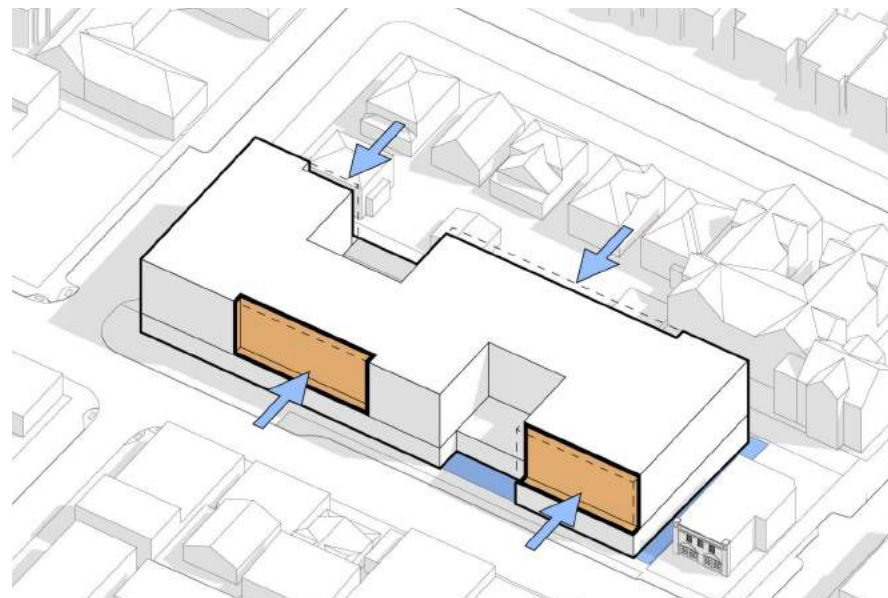
② Carve volume to provide massing relief

Design Guidelines: CS2.1, DC2.3



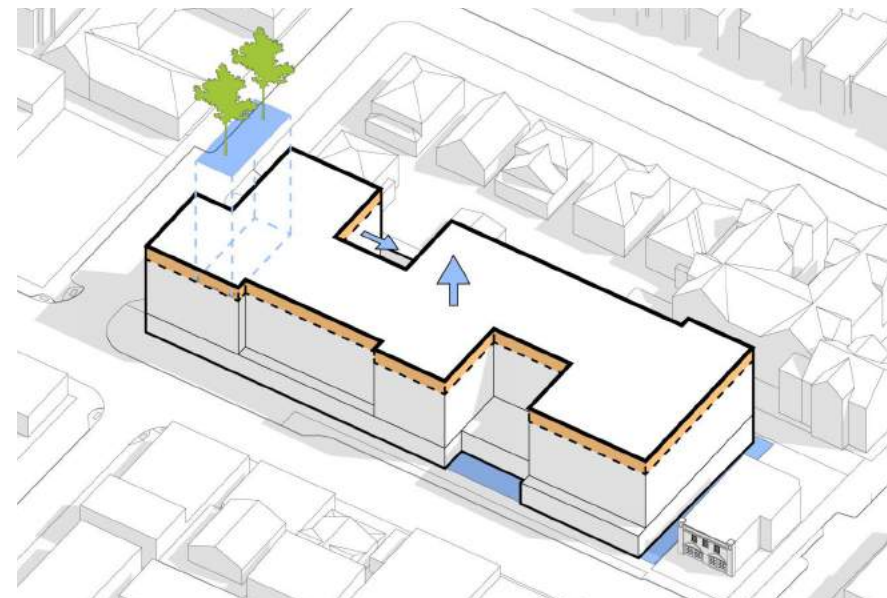
③ Street level open space creates opportunity for pedestrian through block circulation and exterior gathering space with excellent solar exposure

Design Guidelines: PL1.1, PL1.3, PL3.1, DC3.1, DC3.2, DC2.1, DC2.2



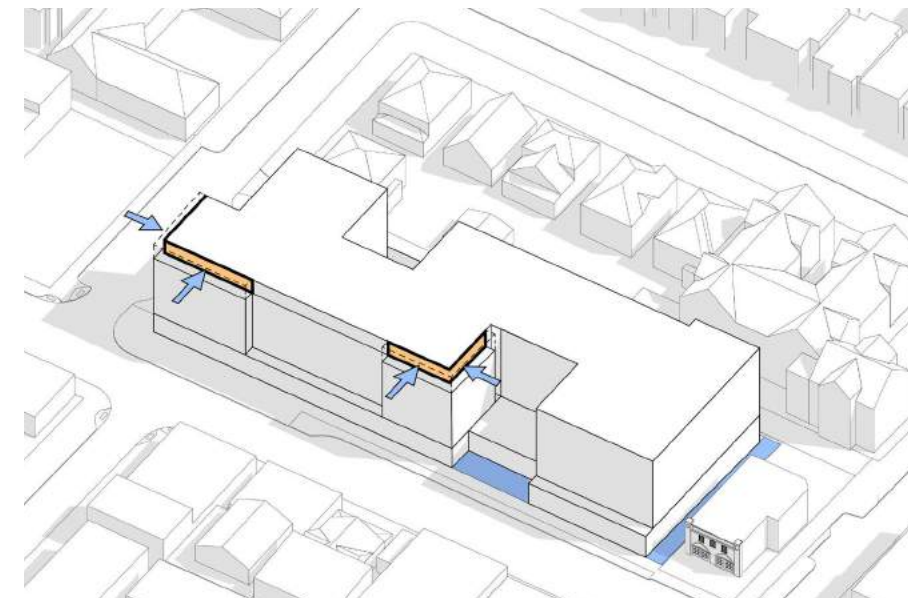
④ Modulate facade to break down scale and create upper story setbacks

Design Guidelines: CS2.1, DC2.3



⑤ Preserve existing tree canopy, responding to zone transition

Design Guidelines: CS1.4, CS2.2, PL3.2



⑥ Step upper floor at NW massing to reduce height, bulk, and scale along 15th Ave. E. (additional setback since EDG).

Design Guidelines: CS2-D, DC2-A, CS2-1-C

DEPARTURES

SETBACK DIAGRAMS

The distribution of the added height has been studied to reduce the perceived mass and respond to surrounding neighborhood context. Massing setbacks and height considerations for the sixth floor include:

- 1

A ground to sky setback along the NE facade to preserve trees and ease the alley zone transition.
- 2

A ground to sky 10' setback from the eastern property line along the alley. This provides massing relief for the buildings east of the project site along the zone transition.
- 3

An additional 6' setback along the top two floors along the eastern facade. This will reduce shading impacts from the additional height on the adjacent buildings to the east, and reduce the perceived mass
- 4

A ground to sky 10' setback from the southern property line. This provides massing relief for the adjacent fire station building and the trees along the southern property line. The added height at the southwest corner massing along 15th creates a backdrop for the adjacent fire station building.
- 5

Upper level setbacks along the western facade to break down the scale of the massing along 15th Ave E.
- 6

Podium level courtyards break down the scale of the massing as a response to the surrounding neighborhood.
- 7

Additional setbacks that have been added since EDG to the top floor along the north west building mass help reduce perceived height.

Design Guidelines:

- CS2.D.3 - Height, Bulk, and Scale: Zone Transitions

CS1.D.1 - On-site Features: Existing Trees
- CS2.D.3 - Height, Bulk, and Scale: Zone Transitions
- CS2.D.3 - Height, Bulk, and Scale: Zone Transitions
- CS2-C - Relationship to the Block

DC2.A.1 - Site Characteristics and Uses
- DC2.A.2 - Reduce Percieved Mass
- CS2-D - Height, Bulk, and Scale

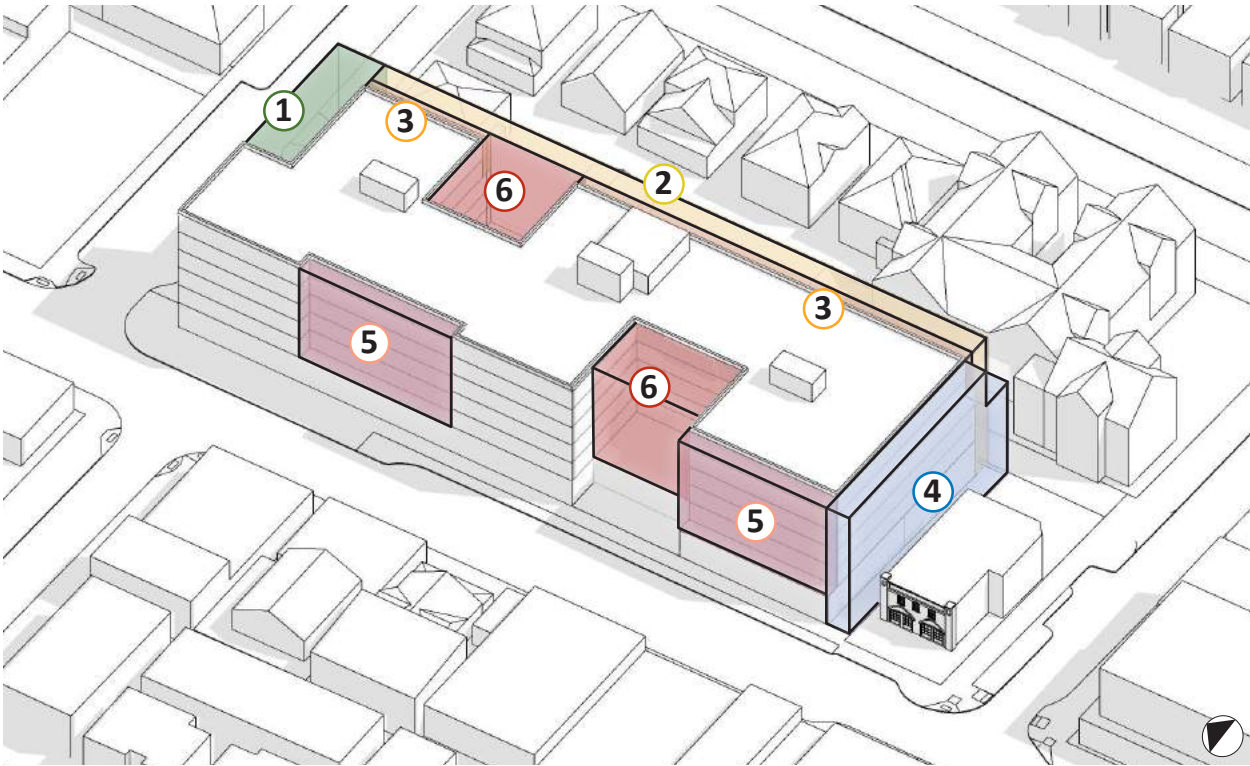
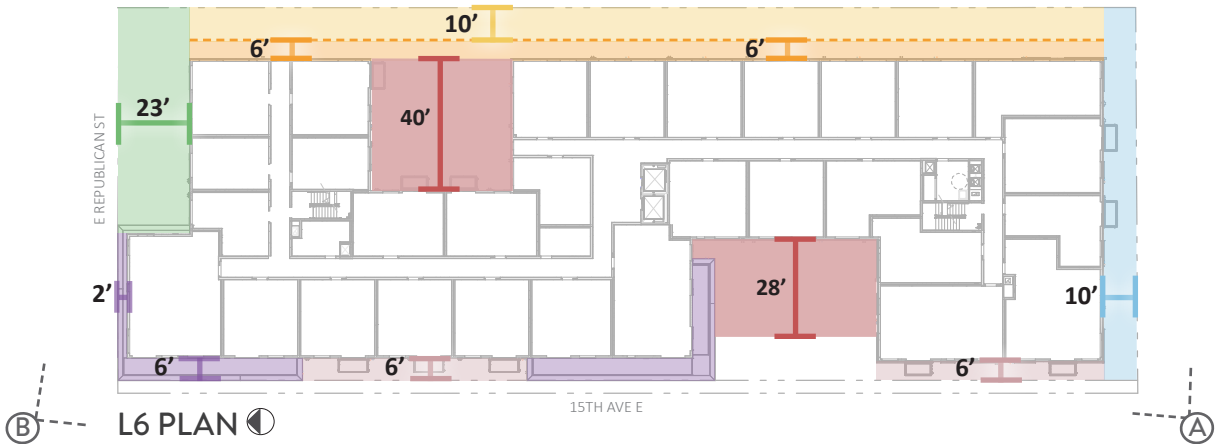
CS2-C - Relationship to the Block
- DC2.A.2 - Reduce Percieved Mass

Development area impact:

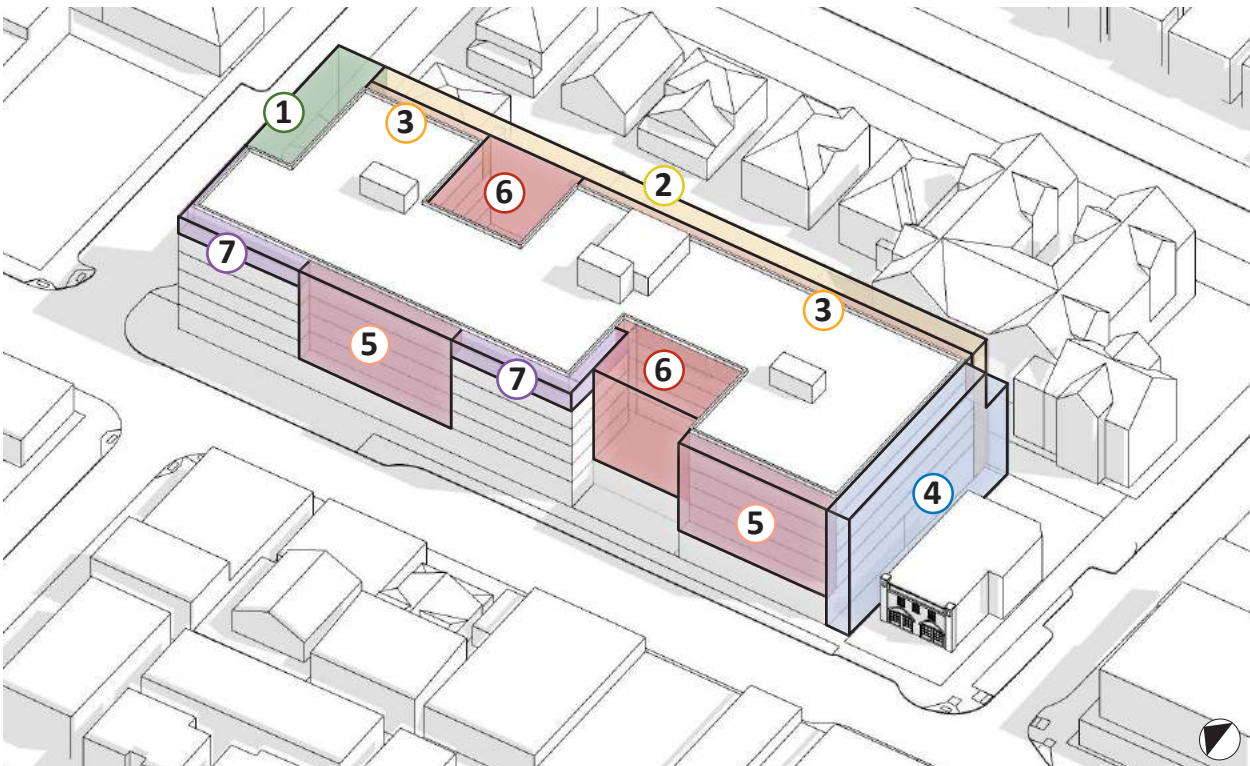
- 9,378 SF (L1-L6)
- 2,866 SF (L1)
- 2,874 SF (L5-L6)
- 7,206 SF (L1-L6)
- 4,970 SF (L2-L6)
- 18,495 SF (L2-L6)
- 1,116 SF (L6)

TOTAL SETBACK AREA:
46,905 SF
FAR YIELD OF SETBACK
AREA: 1.23

LEVEL 6 PROPOSED
AREA: 23,306 SF

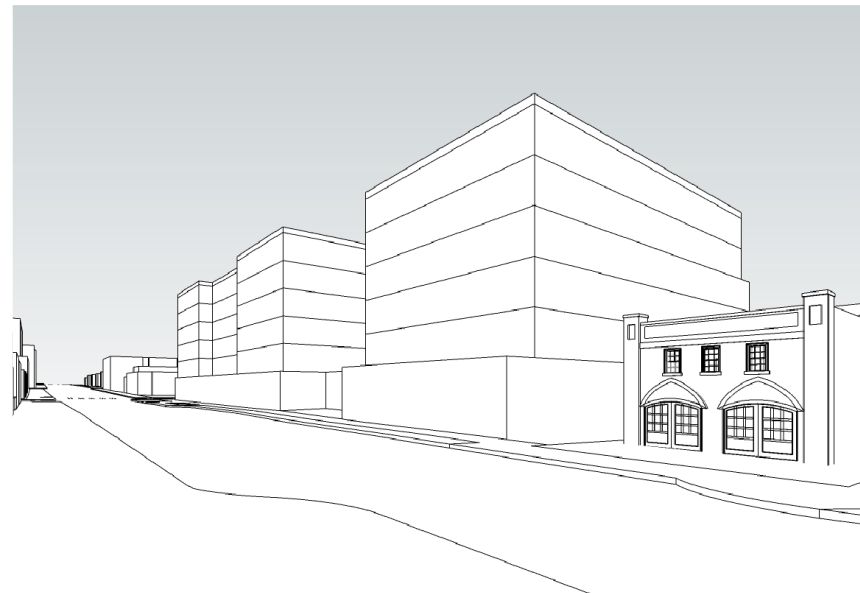


A EDG AERIAL VIEW AT SOUTHWEST CORNER



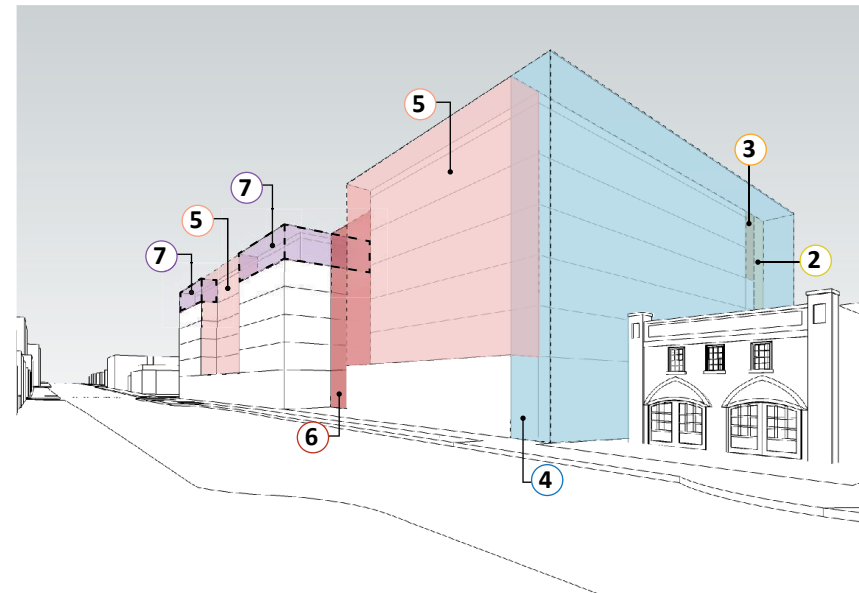
A DRB AERIAL VIEW AT SOUTHWEST CORNER

EDG PREFERRED MASSING



Ⓐ SOUTHWEST CORNER

DRB MASSING

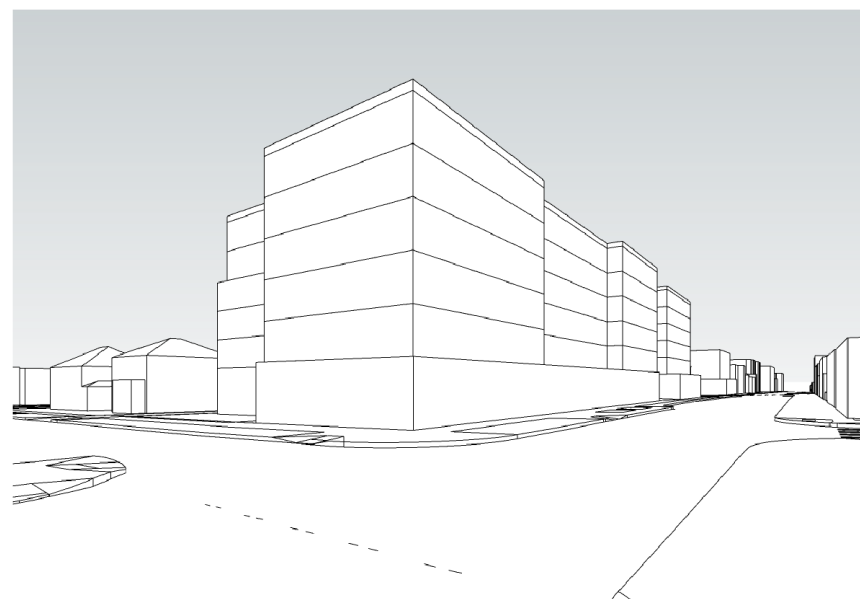


Ⓐ SOUTHWEST CORNER

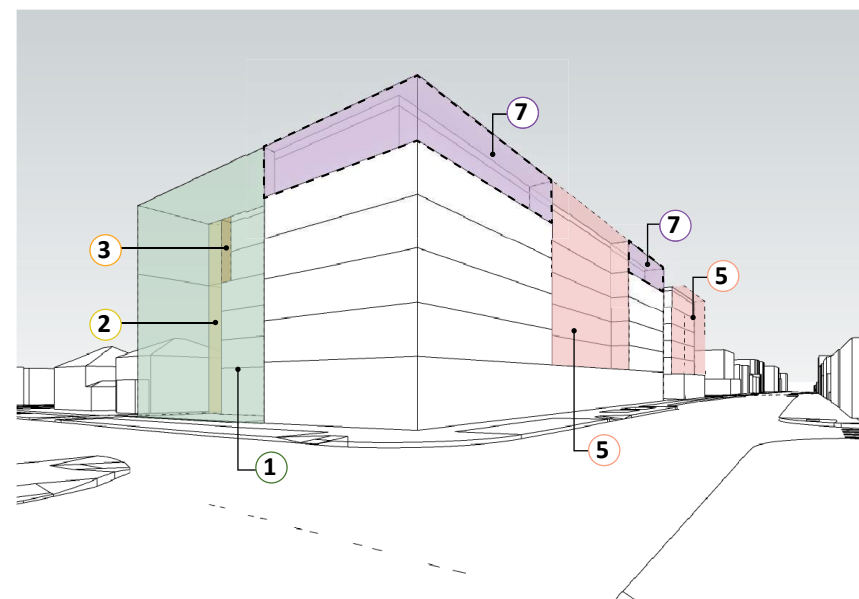
RENDERING



Ⓐ SOUTHWEST CORNER



Ⓑ NORTHWEST CORNER



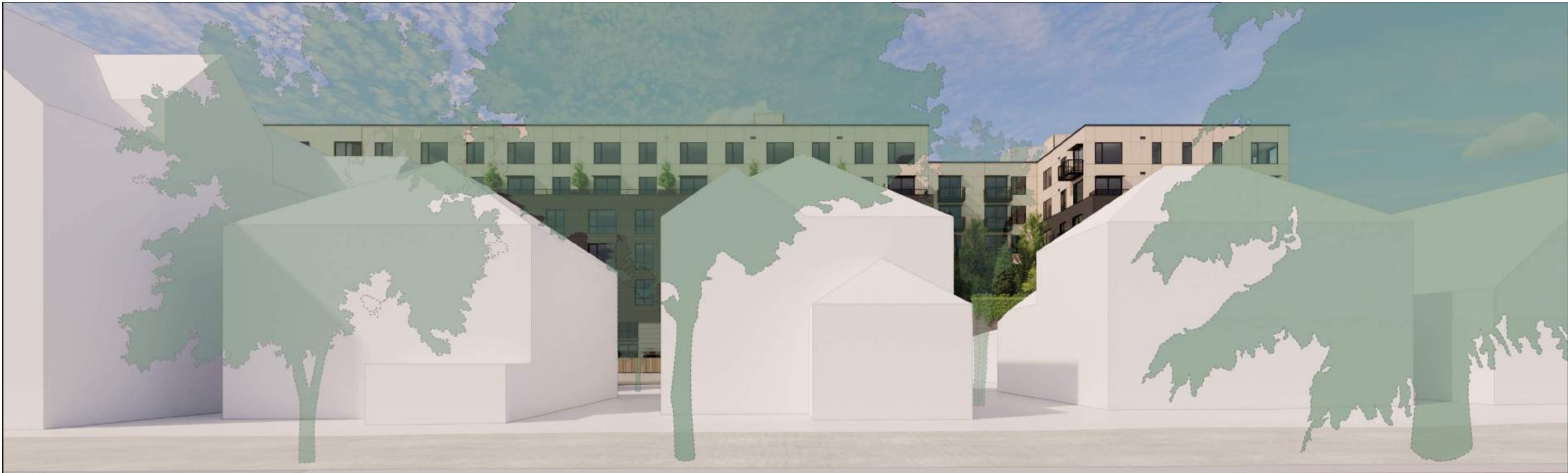
Ⓑ NORTHWEST CORNER



Ⓑ NORTHWEST CORNER

DEPARTURES
COMPARISON FROM 16TH AVE E

6 STORY SCHEME



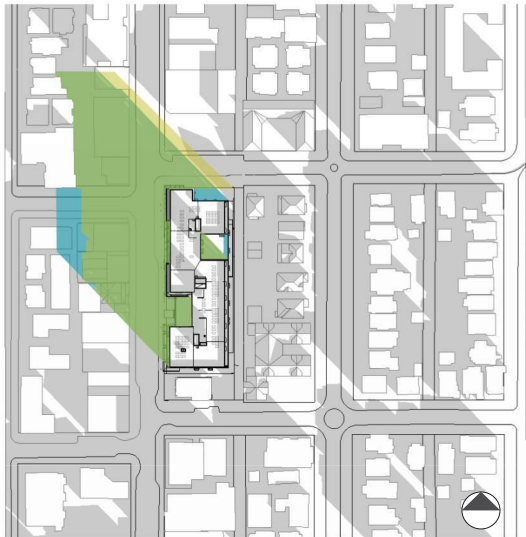
5 STORY SCHEME



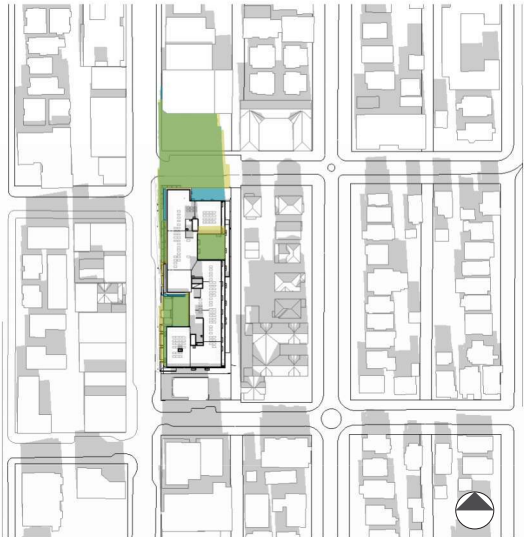
SHADE STUDIES COMPARISON (5 STORY VS 6 STORY)

WINTER SOLSTICE

9 AM



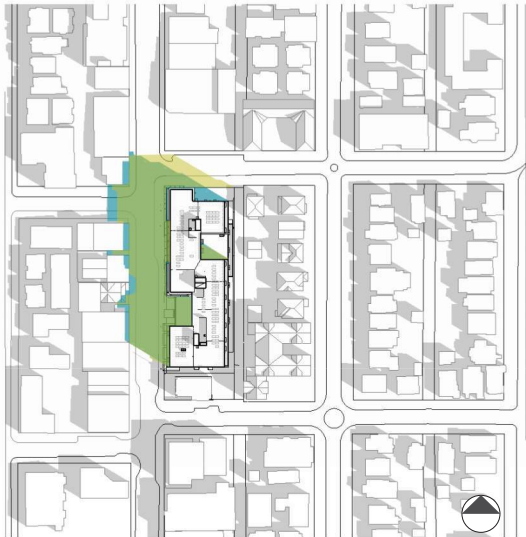
12 PM



3 PM



EQUINOX



SUMMER SOLSTICE



OVERLAP 5 STORY 6 STORY

DEPARTURES

PERSPECTIVE COMPARISON - 5 STORY VS 6 STORY



NW CORNER (5 STORY)



NW CORNER (6 STORY)



NE CORNER (5 STORY)



NE CORNER (6 STORY)



416 15TH AVE. E.

RENDERINGS
NORTHWEST PERSPECTIVE - 15TH AVE



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APPENDIX

