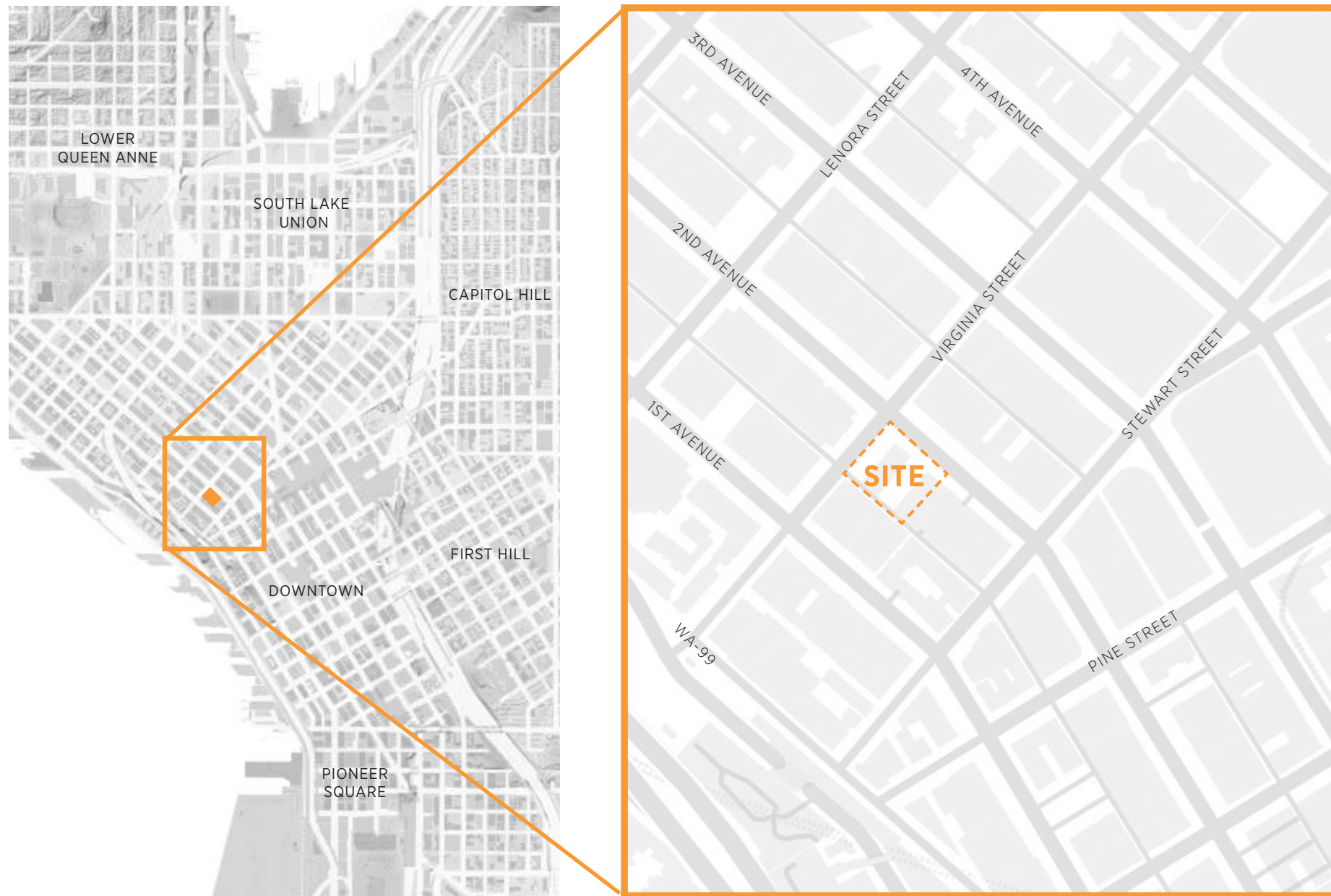




2ND AND VIRGINIA



A new mixed-use hotel, residential, and co-working building will be located at the corner of 2nd Avenue and Virginia Street in downtown Seattle. The building will be a vibrant addition to this key intersection and will take design cues from the existing landmark on site and the urban patterns of buildings and pedestrian experiences that make up the Belltown neighborhood.

The proposed development consists of the following:

- One 42-story structure of approximately 487,100 sq. ft. above ground
- 4 levels of co-work, and 6,700 sf of retail
- 10 levels of hotel with 240 keys
- 26 levels of residential with 200 condo units
- Separate lobbies for hotel and residential uses
- 5 levels of below-grade parking (approx. 175 stalls)
- 2 loading spaces will be accessed through the Alley

SITE INFORMATION

LANDMARKS PRESERVATION BOARD RECAP

The Terminal Sales Annex building is located on the project site as a designated Seattle Landmark. The team has met with the Architectural Review Committee (ARC) on three separate occasions to review massing, integration of the landmark and overall scale.

ARC Meeting 1 - 06.29.2018

The ARC requested further study of the integration of the landmark building.

ARC Meeting 2 - 8.31.2018

The ARC recommended the preferred option move forward into Early Design Guidance.

The ARC was intrigued by the experiential integration of the landmark building, and requested further exploration of how to treat the return party walls.

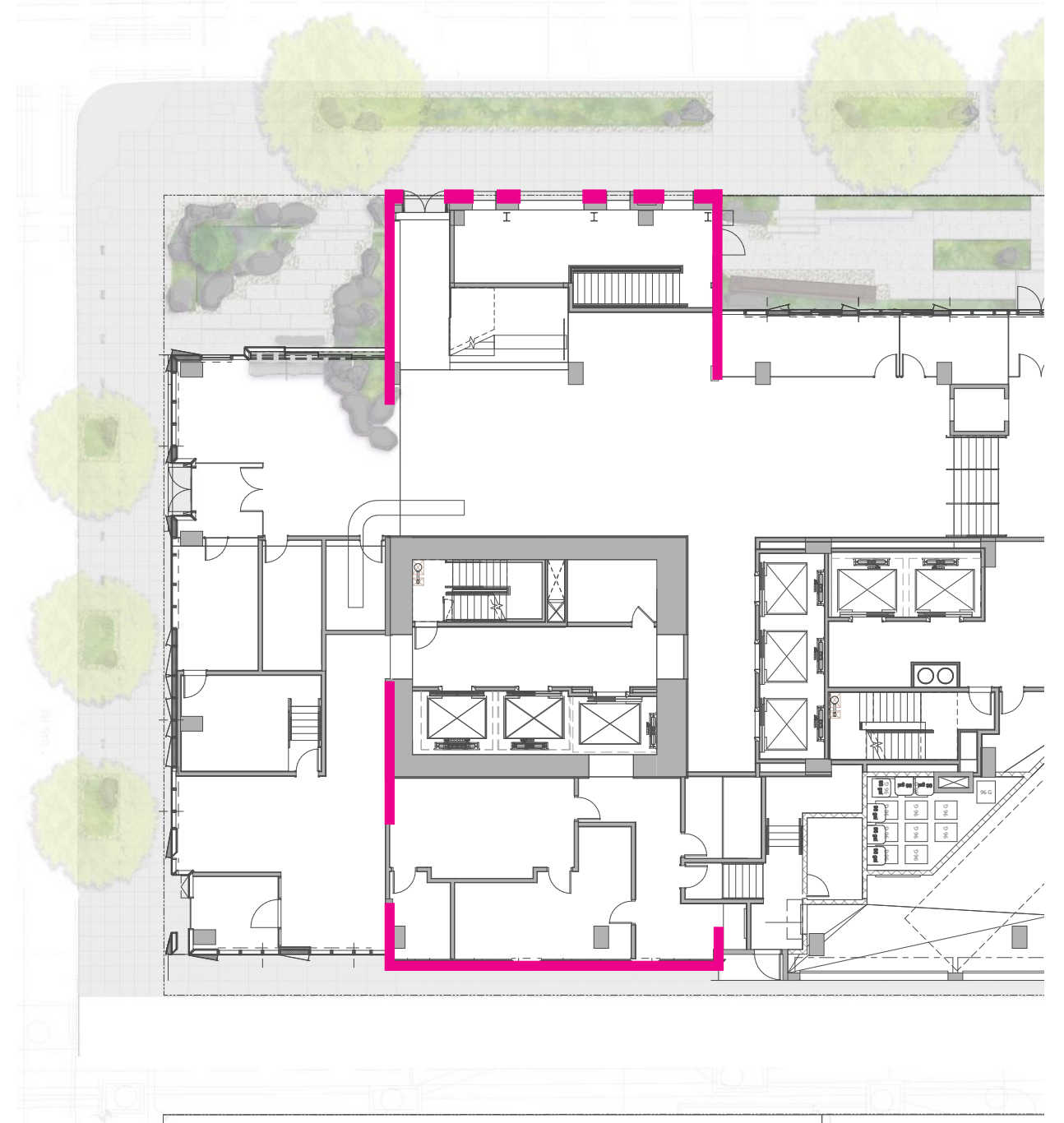
ARC Meeting 3 - 6.28.2019

The ARC found the presentation very responsive to the comments from ARC 2, and recommended the team move forward to a full board briefing.

- The ARC showed support for the art mural treatment of the party wall, suggesting the art could be toned down.
- Team should follow Interior Secretary of Standards and continue to develop the interior volume concept.
- There was some support to retain the existing alley facade.
- The ARC asked the team to bring more comparisons of setbacks and floor plates of the project on site with active Certificate of Approval/MUP.



Photograph of Current Conditions



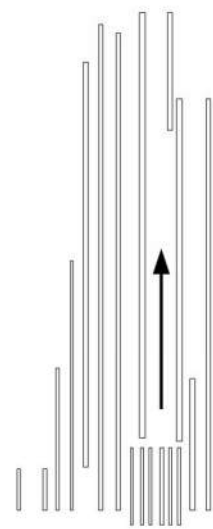
Walls Expressing Volume of Landmark through the Building

LANDMARK PRESERVATION BOARD RECAP

LANDMARK PRESERVATION BOARD PRIORITIES

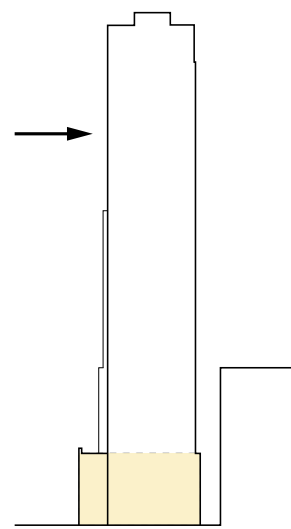
- 1 Tower Expression should draw inspiration from Landmark

There is opportunity to reference the existing massing and embrace what came before. This can be done by using the Terminal Sales Annex to set up the alignments of the tower above.



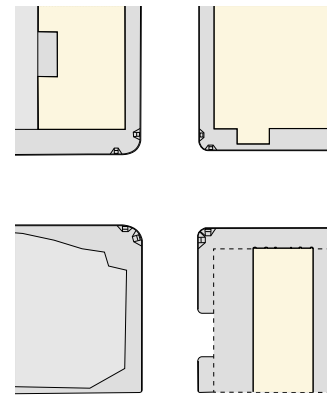
- 2 Set Tower Back from Landmark

Setting the tower back highlights the Landmark Building by giving it necessary breathing room. This allows the Landmark to maintain its presence in the neighborhood.



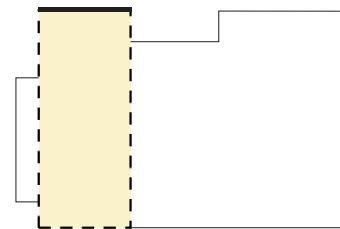
- 3 Landmark to Complete Corner of Landmark Structures

An open corner exposes the landmark to the corner which is defined by other existing landmarks, including Palladian Hotel and Moore Theater & Hotel.



- 4 Express Volume of TSA through Building

The design should imply the sense of the building in the function of the space.



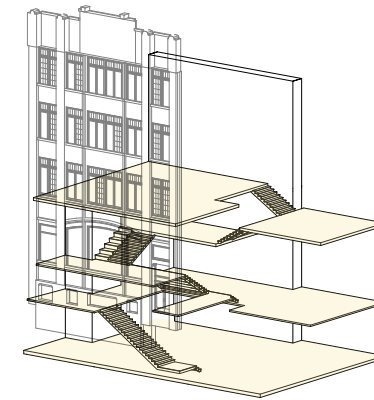
- 5 Retain Existing Entry Door of Landmark

The Landmark building should retain its existing entry.



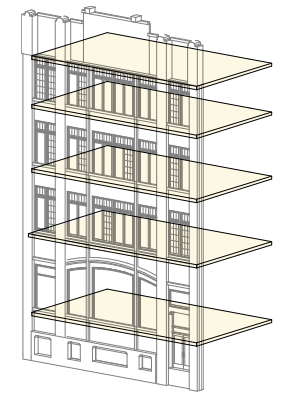
- 6 Create Active Uses within Landmark

There should be active uses filling the space within the Landmark.



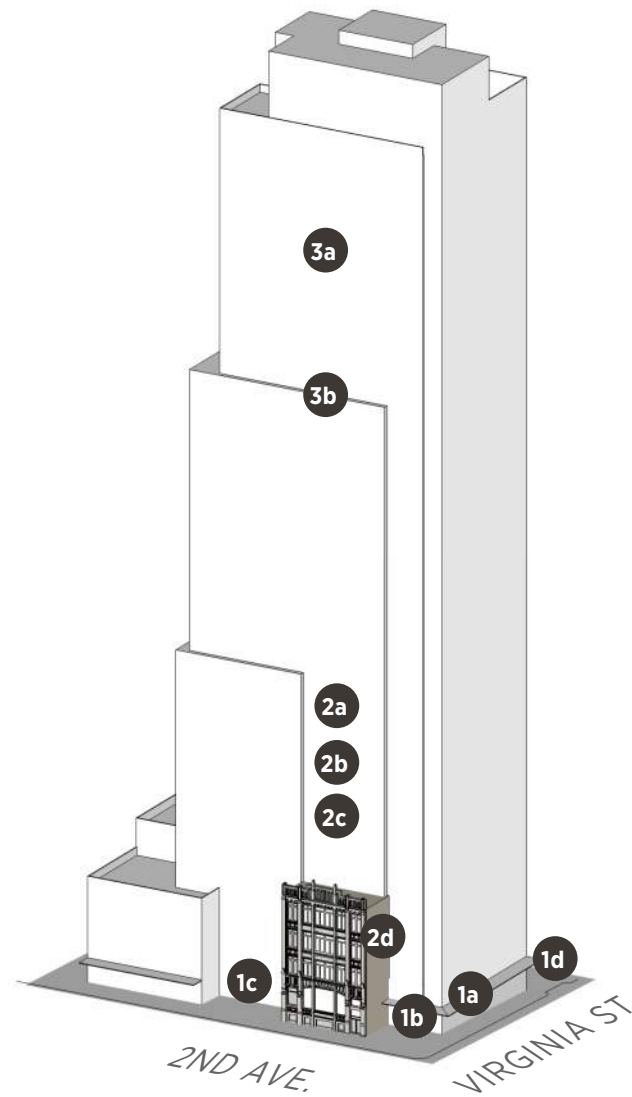
- 7 Match Existing Levels of Landmark

Floors levels should remain the same behind the Landmark. These elevations are important at the windows.



EDG 2 SUMMARY

SUPPORTED AT EDG 2



TOWER

3a. TOWER FORM

Unified, deliberate & rigorous stepping form on all sides

3b. STEPPING MASSING

Massing stepping away from smaller scaled building to south

PODIUM

1a. VIRGINIA STREET

Double height corner at 2nd & Virginia

1b. 2ND AVE. ENTRY PLAZA

1c. 2ND AVE. INTERACTIVE ENTERTAINMENT SPACE

1d. ALLEY SETBACK

LANDMARK

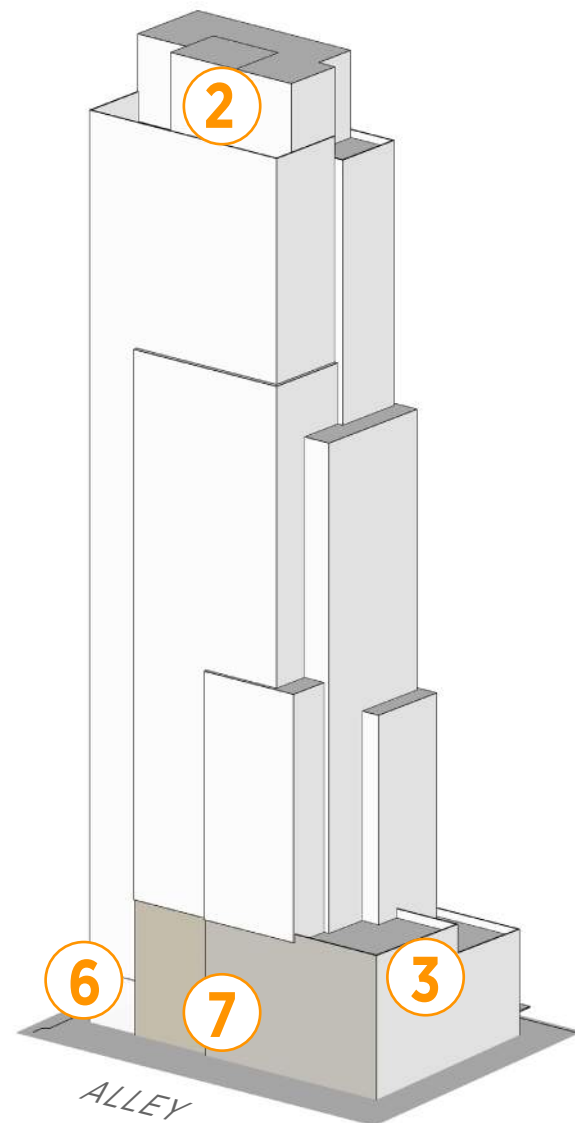
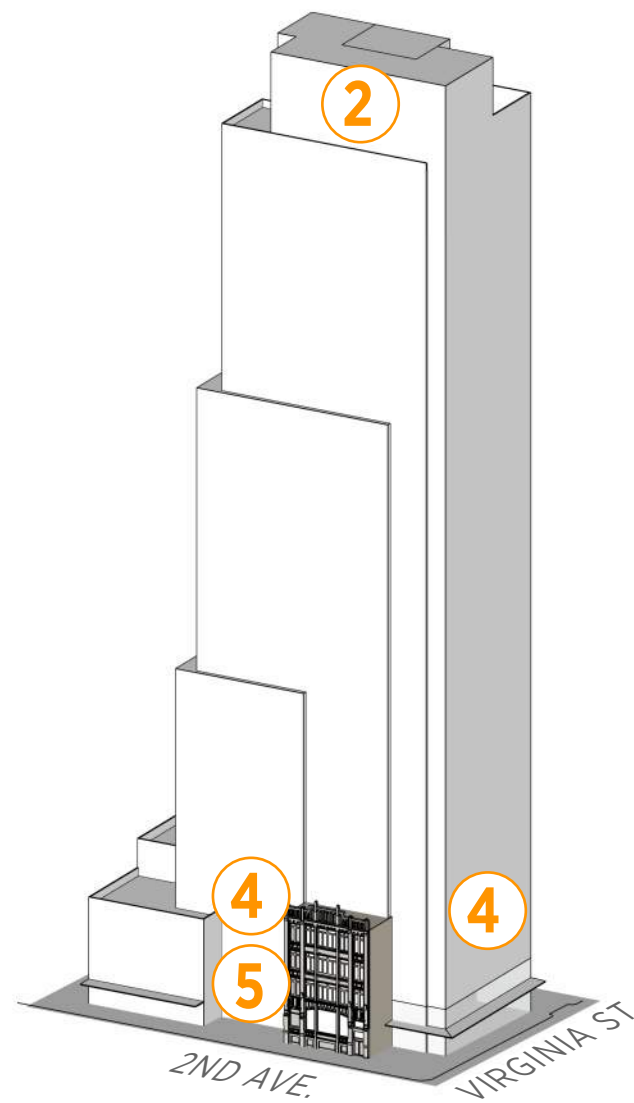
2a. UNIFIED TOWER LANGUAGE

2b. TOWER PROPORTIONS

2c. MATERIAL AND FACADE

2d. LANDMARK VOLUME EXPRESSION

PRIORITIES & BOARD RECOMMENDATIONS



TOWER

- ① 4b. ADDITIONAL VIEWS
- ② 4a. APPROPRIATELY SCALED TOP
- ③ 3c. SOUTH PARTY WALL

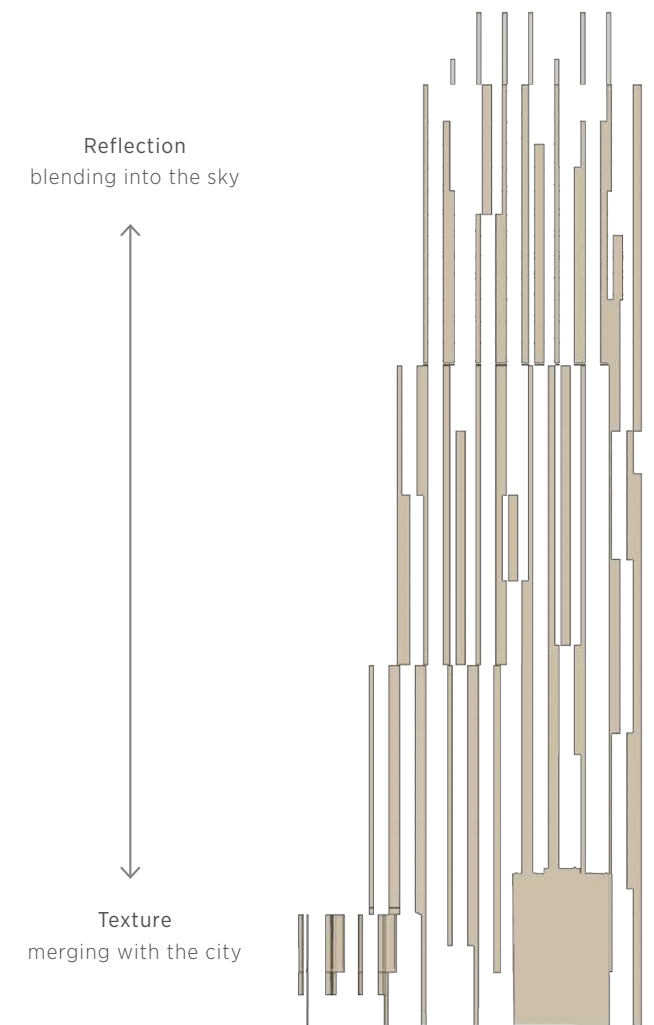
PODIUM

- ④ 1g. VERTICAL LANGUAGE
- ⑤ 1c. 2ND AVE. INTERACTIVE ENTERTAINMENT SPACE
- ⑥ 1e. ALLEY GLASS CORNER
- ⑦ 1f. ALLEY ARTICULATION

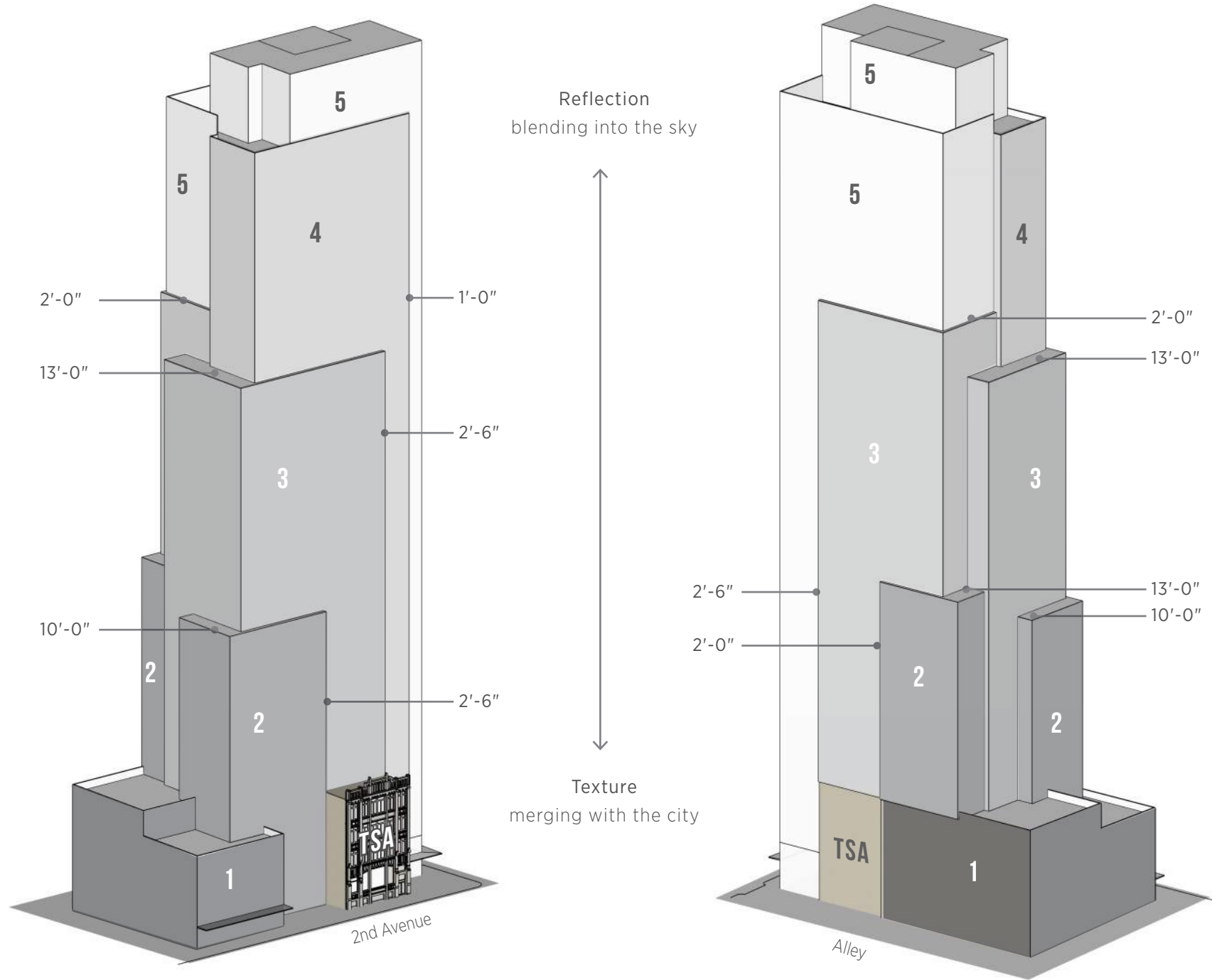
CONCEPT



Starting down at the street level where the remains of Seattle's past exists in many historic buildings, the tower rises upward from the historic Terminal Sales Annex landmark in a series of graded steps into the skyline where it positions itself amongst the new towers that form the future of Seattle. Bebb & Gould's use of terracotta on the Terminal Sales Annex was innovative and new to the city. Each step holds this forward-thinking approach of the Terminal Sales Annex, to a more innovative skyline reflecting the change of the city.

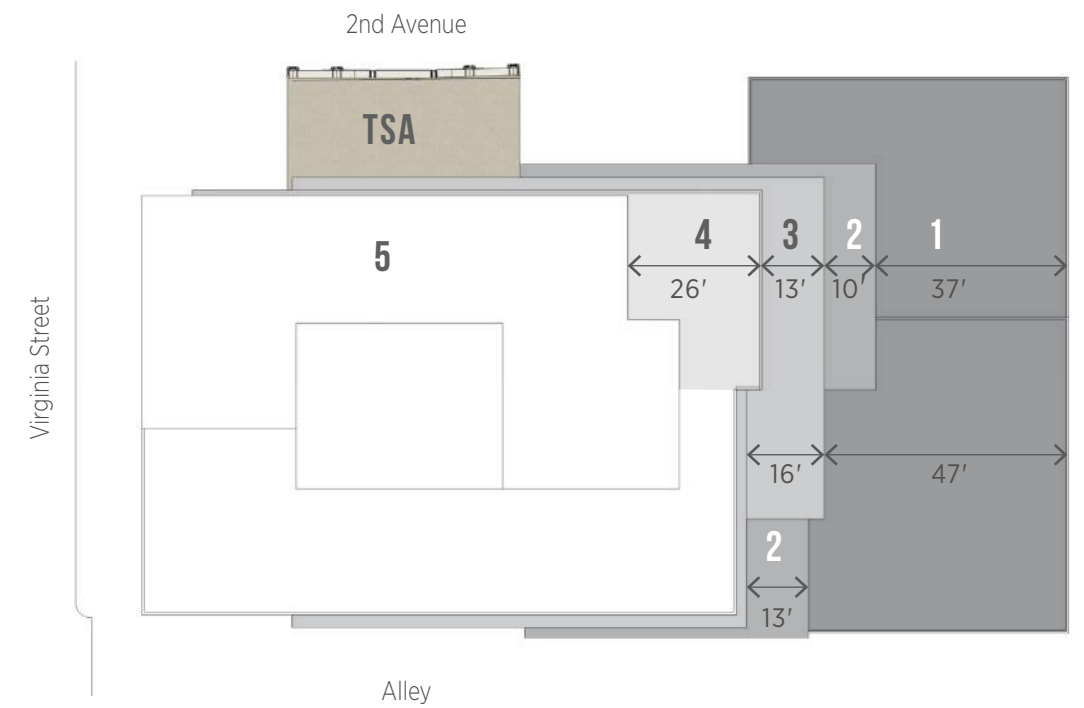


The facade treatment will be composed in a way that strengthens the overall development of the telescoping form - more opaque and textured at the bottom of the tower to more reflective at the top, blending into the sky.



TOWER FORM DIAGRAM: SE CORNER

SW CORNER



TOWER FORM PLAN DIAGRAM

RESPONSE TO BOARD GUIDANCE

TOWER VIEWS

1 ADDITIONAL VIEWS

SUPPORTED AT EDG

BOARD GUIDANCE

The Board appreciated the additional views, page 24 for example, provided by the applicant team. The Board would like to see these views (night and day) provided at the next meeting, illustrating how the tower top has been further enhanced to add to the skyline and support the building identity. The Board would like to see the materiality of adjacent buildings in future images as this will inform guidance related to how the tower sits within the skyline context.

RESPONSE

The design team has provided several overall and tower top renderings in both night and day conditions with the context of adjacent buildings.

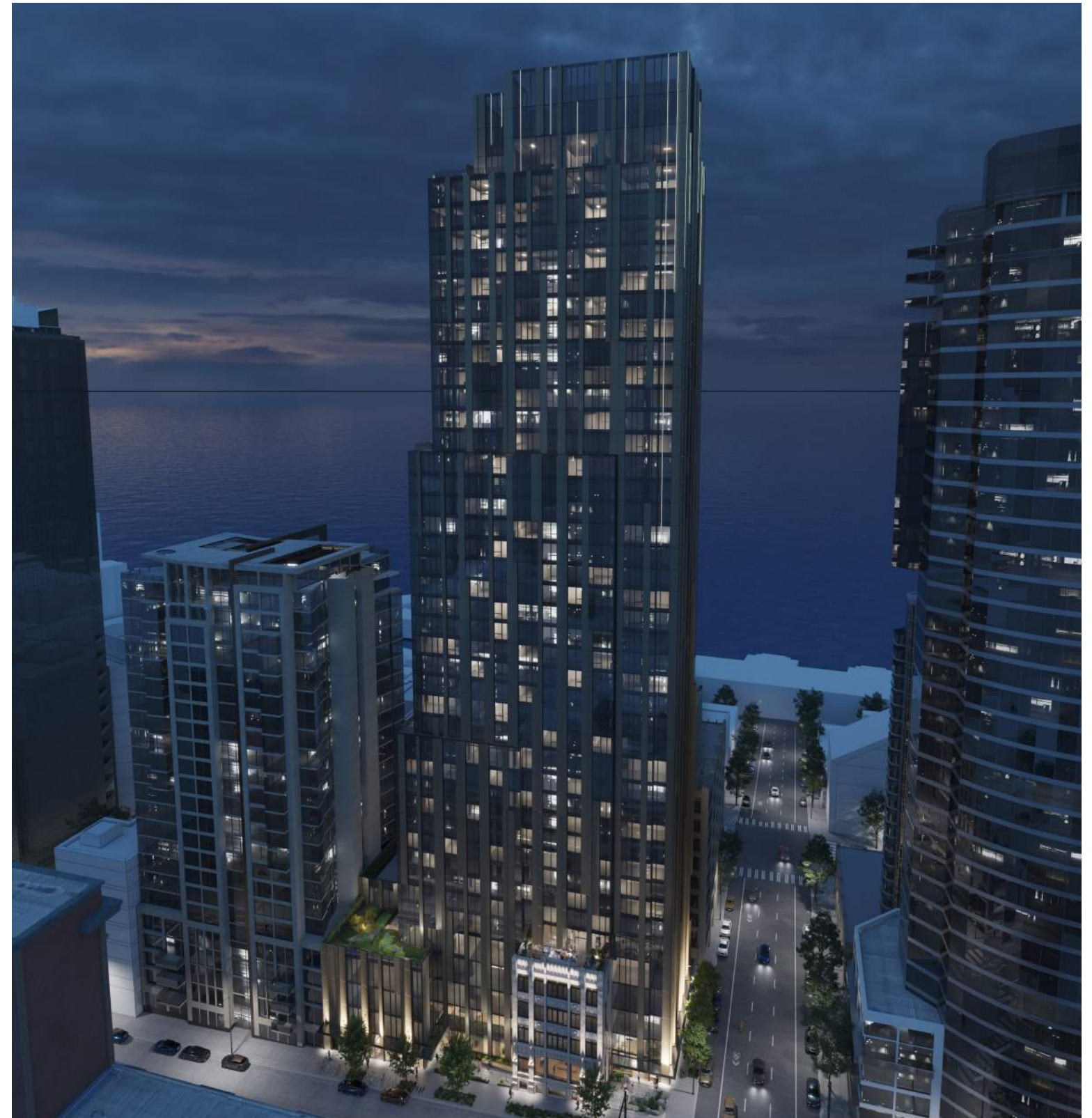
DESIGN GUIDELINES

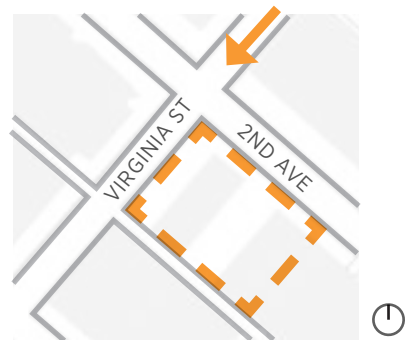
A2 Enhance the Skyline

B2 Create a Transition in Bulk and Scale

B4 Design a Well-Proportioned & Unified Building

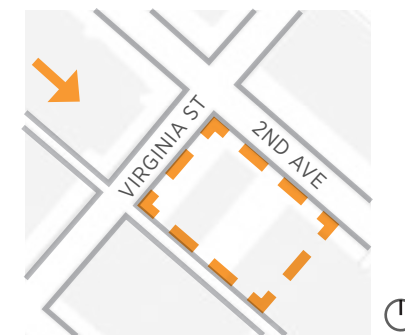
C2 Design Facades of Many Scales

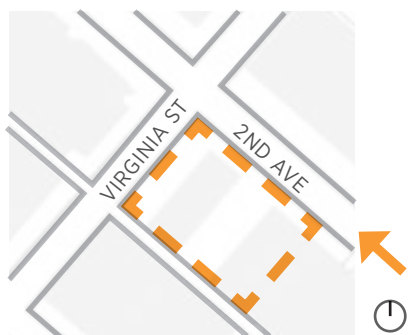




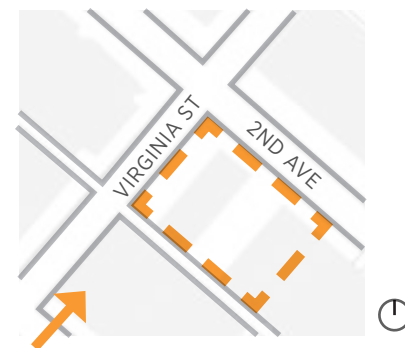
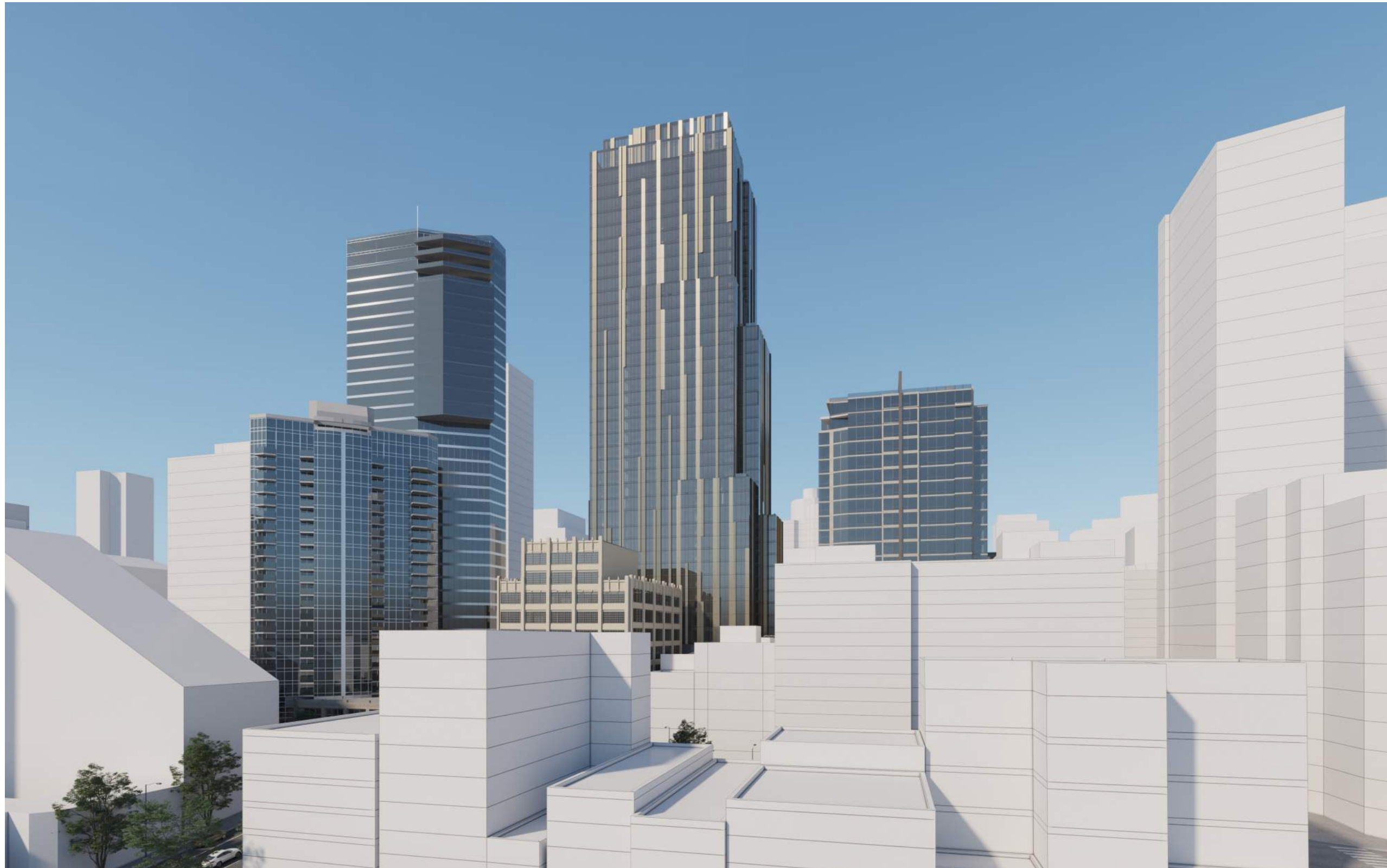
RESPONSE TO BOARD GUIDANCE

TOWER VIEWS

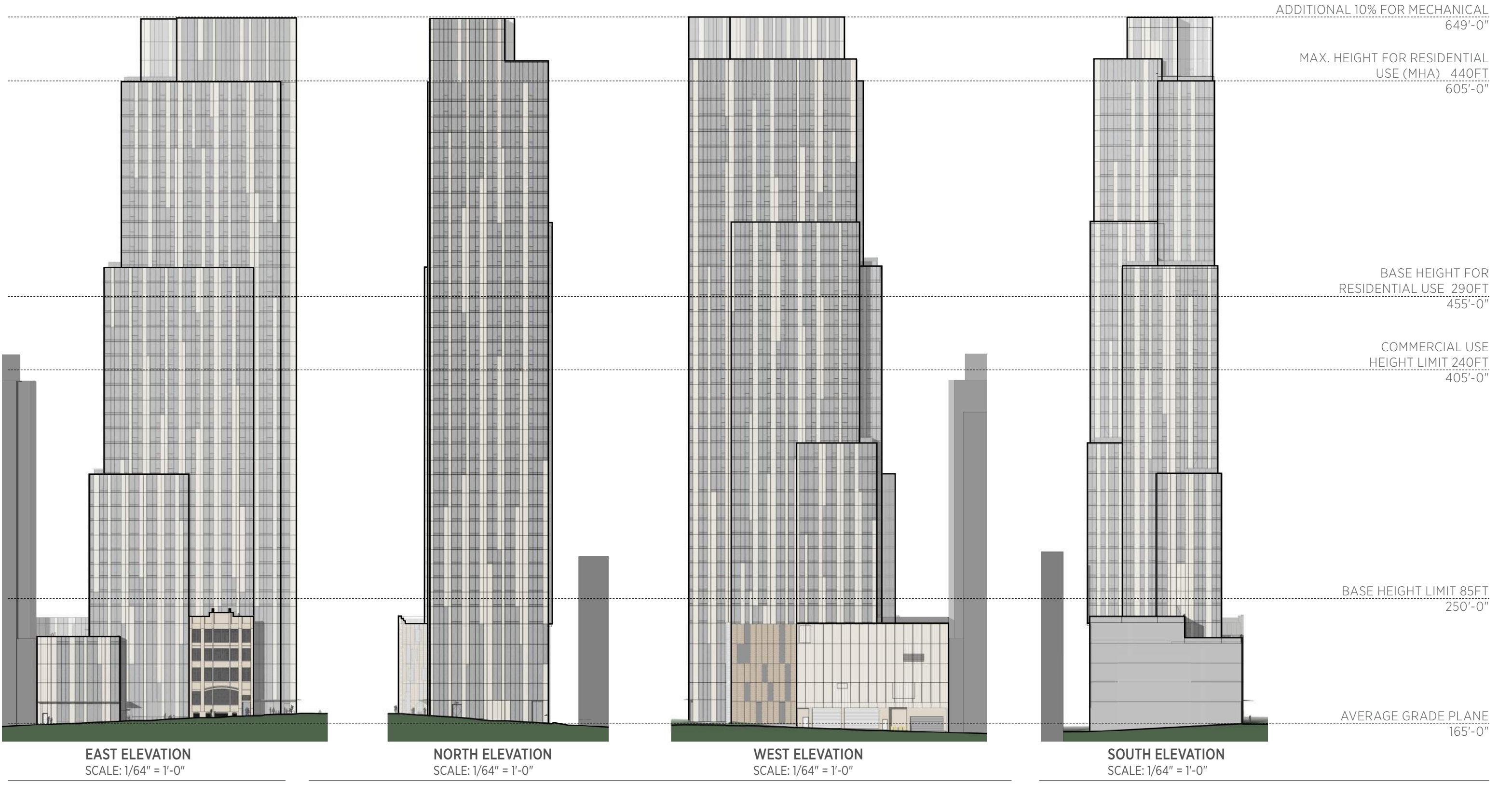




TOWER VIEWS



TOWER OVERALL ELEVATIONS



RESPONSE TO BOARD GUIDANCE

TOWER TOP

2 APPROPRIATELY SCALED TOP

SUPPORTED AT EDG

BOARD GUIDANCE

The Board expressed some concern with the tower's top terminus, providing guidance to further refine the tower top to be more in keeping with the design rules (stepping concept) established by the body of the tower form. The top should be scaled appropriately (large enough massing moves) to create a logical terminus form in keeping with the tower proportions and form.

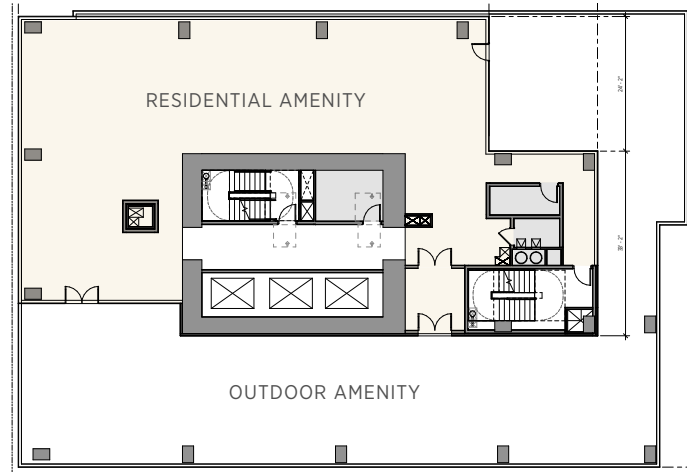
RESPONSE

Massing: In order to simplify the rooftop massing, the facade has been raised to cover the elevator/ core overrun, enclosing all mechanical equipment within one expression, which originates at the ground and extends up toward the sky.

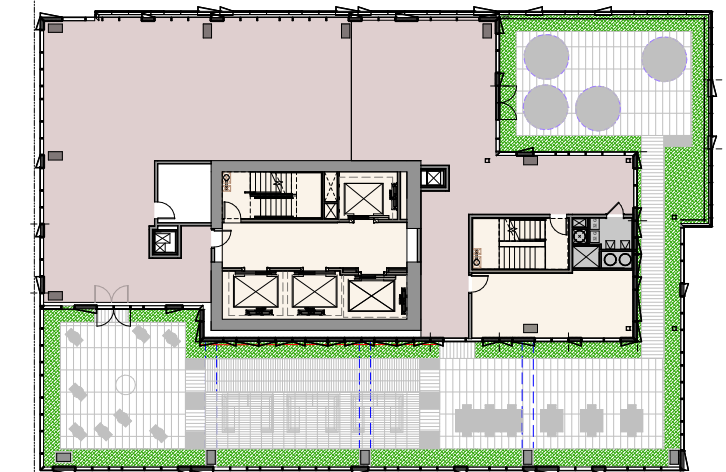
Facade: In contrast to the opacity seen in the previous EDG scheme, the tower terminus' west facade has been broken down into a lighter but still continuous facade treatment. The current design has evolved to bring the solid facade patterning created by the metal panels to the top of the screening wall.

DESIGN GUIDELINES

- A2 Enhance the Skyline
- B2 Create a Transition in Bulk and Scale
- B4 Design a Well-Proportioned & Unified Building
- C2 Design Facades of Many Scales

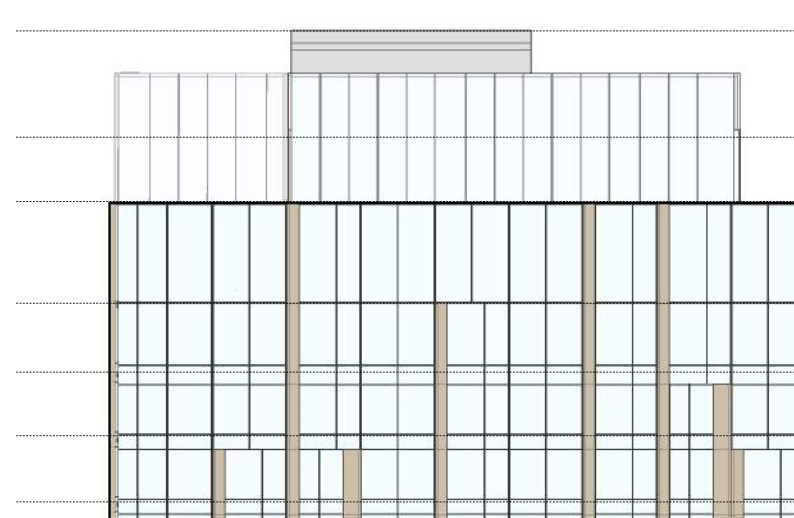


EDG 2 DESIGN LEVEL 42 ROOF AMENITY

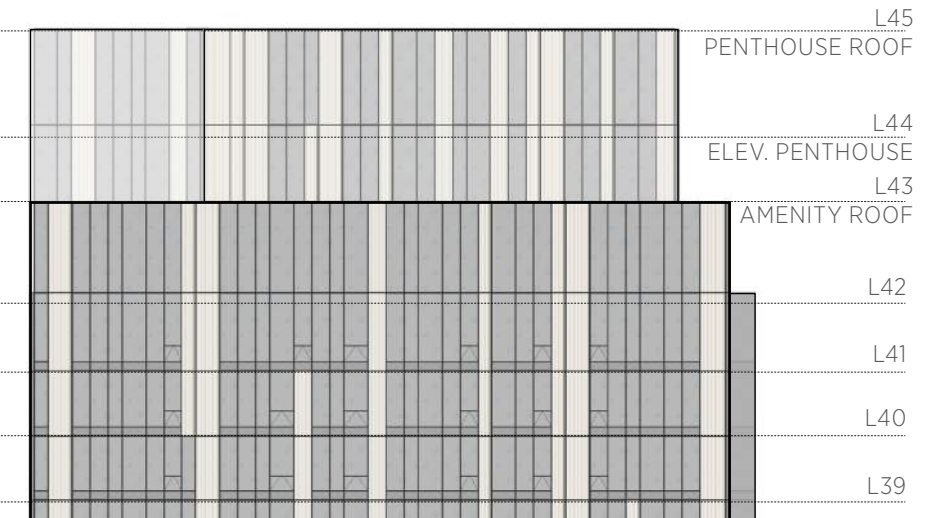


UPDATED

CURRENT DESIGN LEVEL 42 ROOF AMENITY



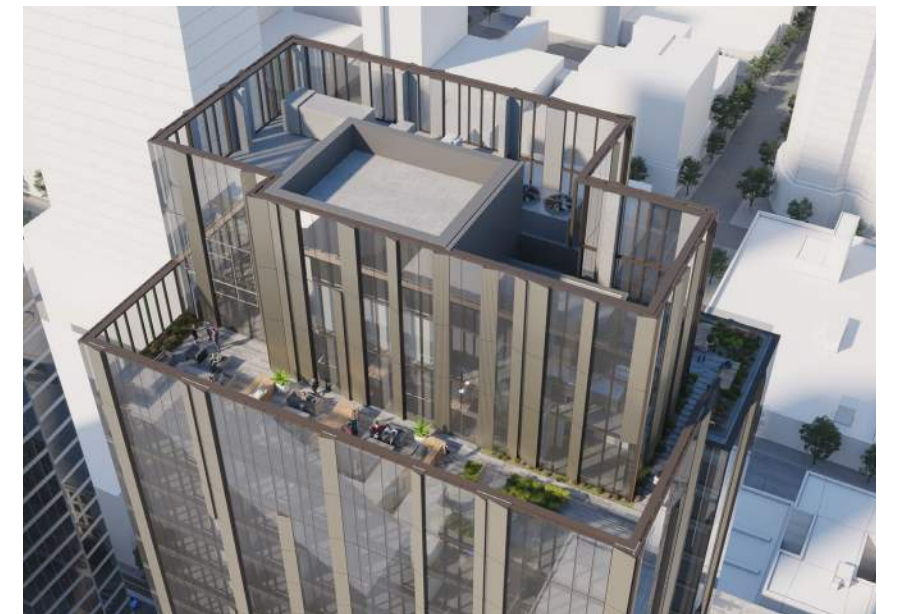
EDG 2 DESIGN WEST ELEVATION



CURRENT DESIGN WEST ELEVATION

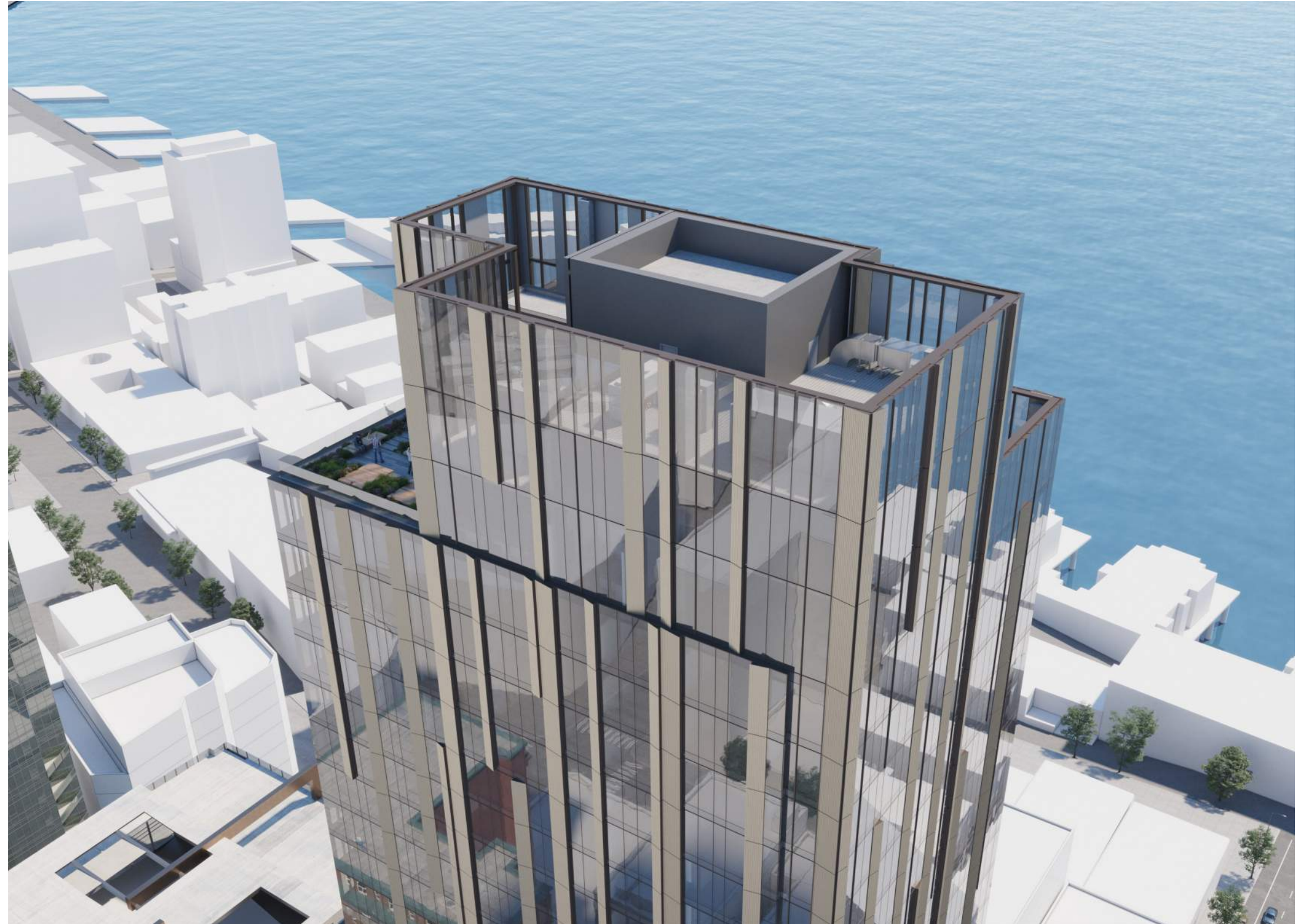
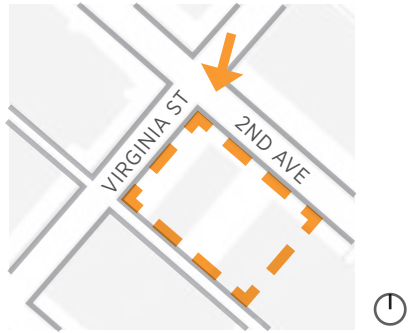


EDG 2 DESIGN



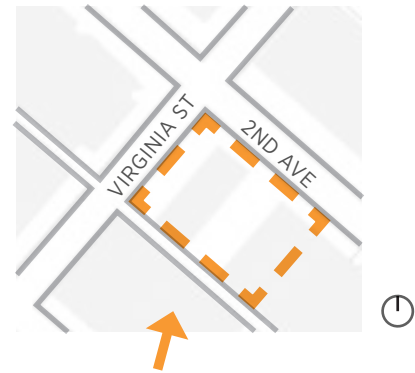
CURRENT DESIGN

TOWER TOP - CURRENT DESIGN



EDG 2 DESIGN

CURRENT DESIGN



EDG 2 DESIGN

CURRENT DESIGN

RESPONSE TO BOARD GUIDANCE

SOUTH PARTY WALL

3 SOUTH PARTY WALL

SUPPORTED AT EDG

BOARD GUIDANCE

At the next meeting, the Board requested more information detailing the resulting party wall condition along the south edge.

RESPONSE

In addition to the tower setback on the south, the design team has made respectful considerations to the abutting property. The project proposes an outdoor roof terrace at the southeast corner on Level 5 with a five feet deep landscape buffer. This buffer will be a pleasant view from the neighboring property, and provide visual and audible privacy between the buildings. As the building steps to Level 6 at the west, another 5' deep landscape buffer is proposed, along with an acoustical barrier to mitigate any mechanical noises transference to the neighboring property. One unit of the neighboring building will face a natural party wall.

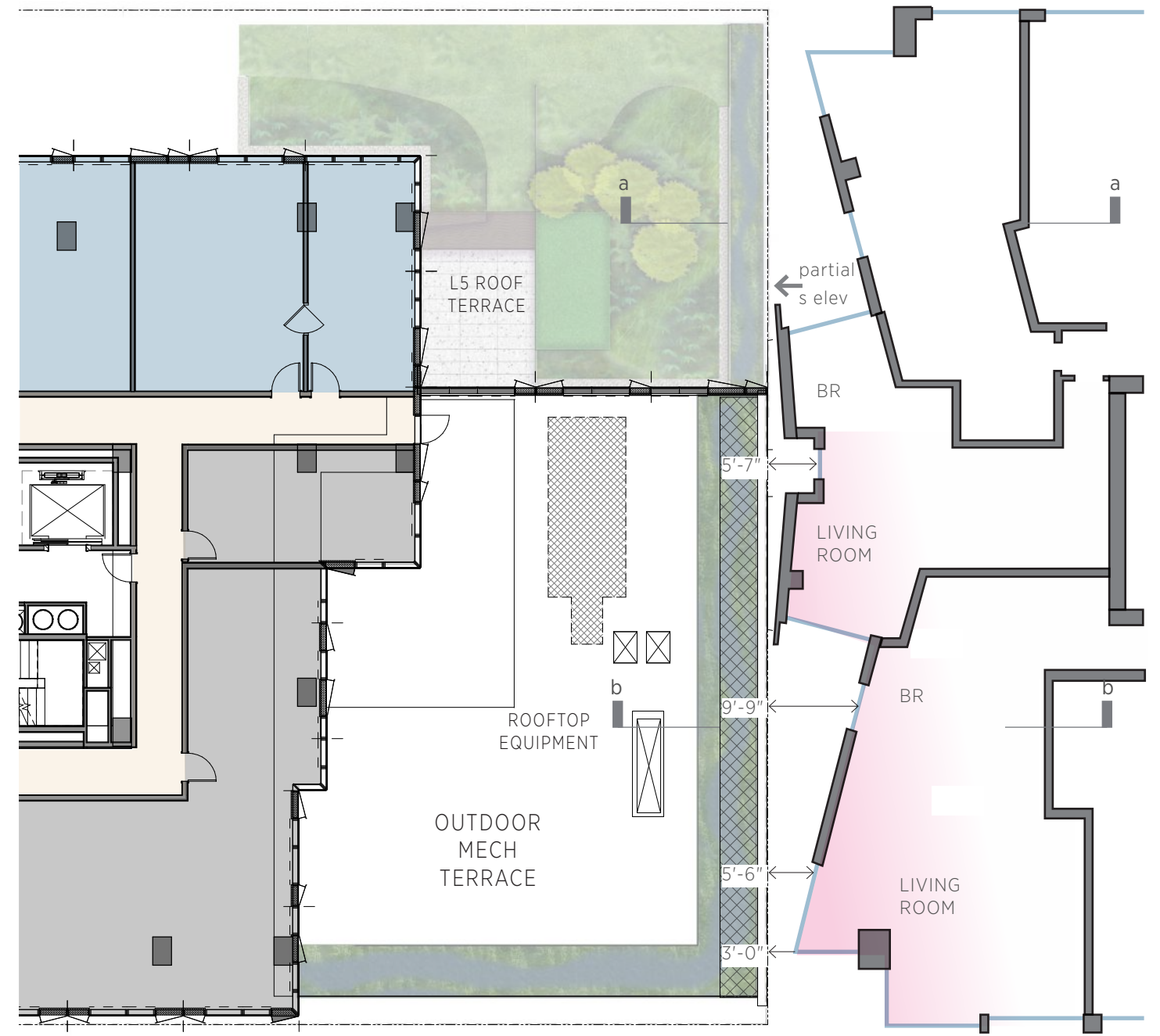
DESIGN GUIDELINES

B1.1. Compatible Design





LEVEL 6 EDG 2
SCALE: 1/16" = 1'-0"



LEVEL 6 PROPOSED DESIGN
SCALE: 1/16" = 1'-0"

RESPONSE TO BOARD GUIDANCE

SOUTH PARTY WALL

LOCATIONS AND ELEVATIONS OF VIKTORIA WINDOWS SHOWN ALONG SOUTH FACADE

LEVEL 6 MECHANICAL SCREEN APPROX. 8' IN HT., PAINTED TO MATCH



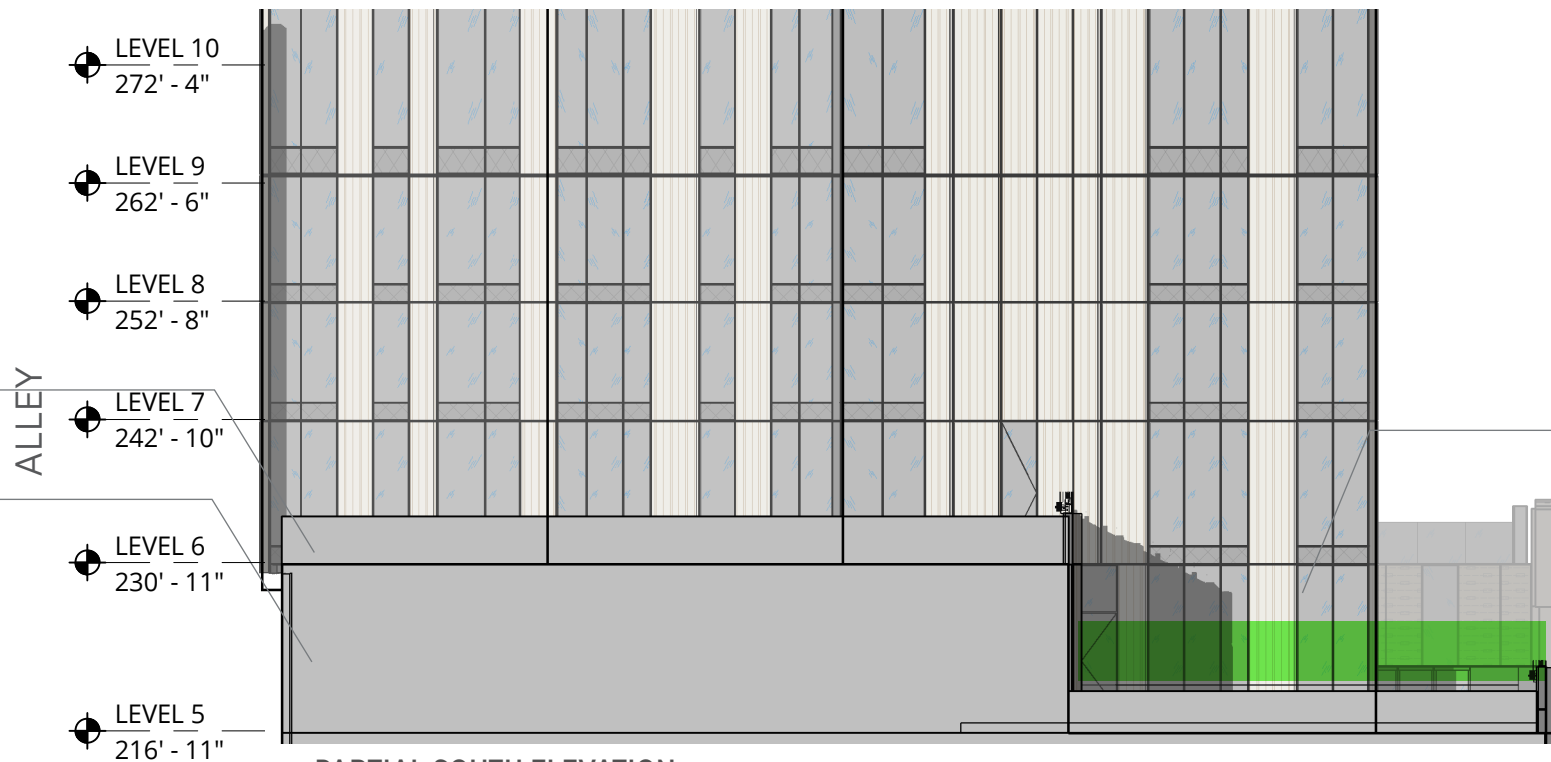
PARTIAL SOUTH ELEVATION - VIKTORIA WINDOW OVERLAY
SCALE: 1/16" = 1'-0"

VIKTORIA
FLOOR LEVELS AND WINDOW DATUM FOR REFERENCES

- LEVEL 12 230'-4"
- LEVEL 11 259'-4"
- LEVEL 10 249'-8"
- LEVEL 9 240'-4"
- LEVEL 8 230'-4"
- LEVEL 7 220'-8"

LEVEL 6 MECHANICAL SCREEN APPROX. 8' IN HT., PAINTED TO MATCH

SOUTH PARTY WALL, PAINTED CONCRETE



PARTIAL SOUTH ELEVATION
SCALE: 1/16" = 1'-0"

LANDSCAPE BUFFER, TREES BEYOND PLANTED AT TERRACE

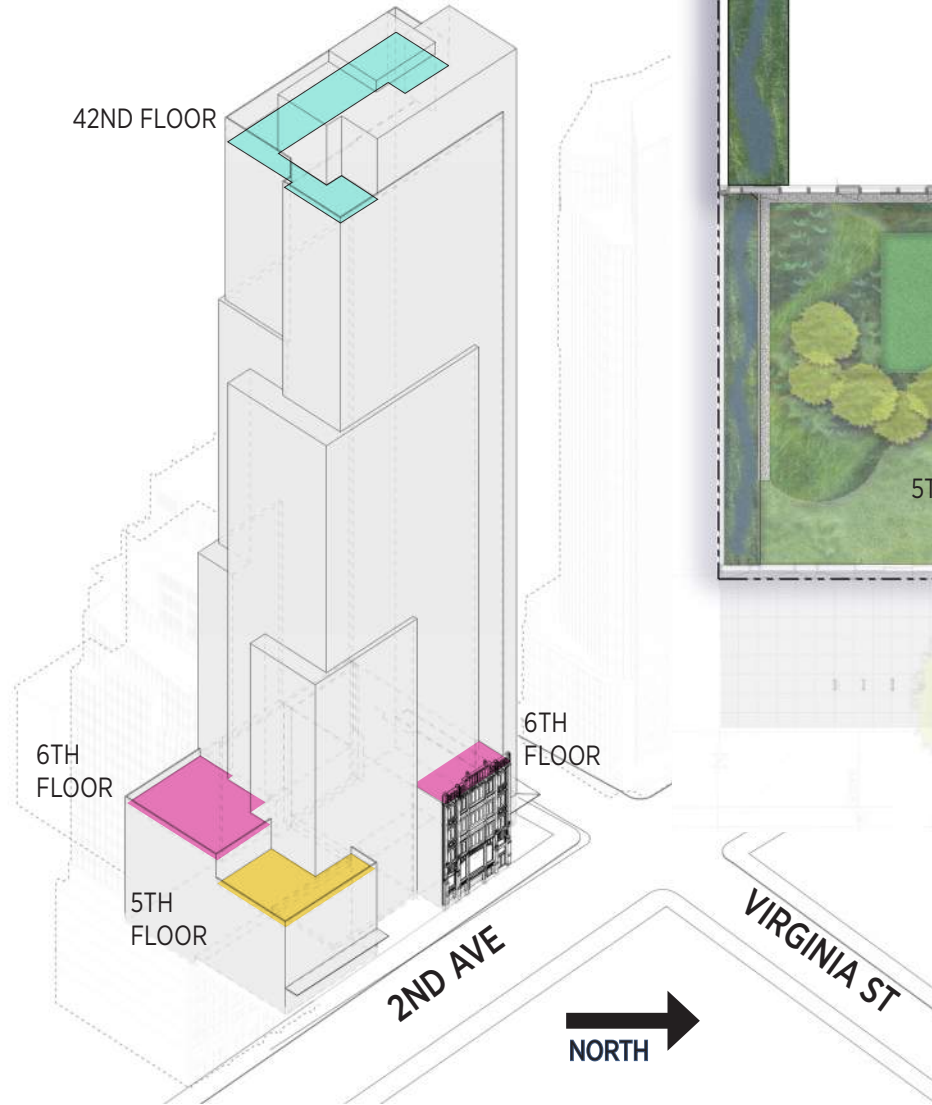
Tower Terrace Concept

5th, 6th, and 42nd Floors

The tower carries various occupiable and visually accessible terraces. The concept for these spaces is flexible and modular, anchored with green edges and views of the City and Sound.

Residential, hotel, and public terraces provide outdoor respite for occupants. A family of design language will be used to create design continuity throughout the project.

Stormwater management practices will be integrated throughout.



5th Floor Terrace

The 5th floor terrace accommodates a small dog run and small paved area for residents. Planting includes an occupiable meadow and bioretention planter treating level 6 south. Planting/hardscape ratio allows level 5 to self-mitigate stormwater.

6th Floor Terraces

The 6th floor south terrace is primarily for building function, including a sizable bioretention planter treating the tower.

The 6th north terrace, designed by others, functions as public bar spill out space.

42nd Floor Terrace

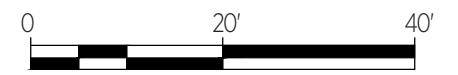
Tall wind screens will protect the majority of this residential amenity space. A difficult area for plants to thrive, this terrace will focus on hardscape moves accented with simple, rugged plant species to create outdoor rooms such as a spa area, yoga terrace, barbeque area, and gathering space connecting to the interior.

VIRGINIA ST

2ND AVE



NORTH



SCALE: 1" = 20'

2ND AVENUE

4 VERTICAL LANGUAGE

SUPPORTED AT EDG

BOARD GUIDANCE

Moving forward, the Board provided guidance to demonstrate how the vertical language of the tower would be carried down through the street-level expression.

RESPONSE

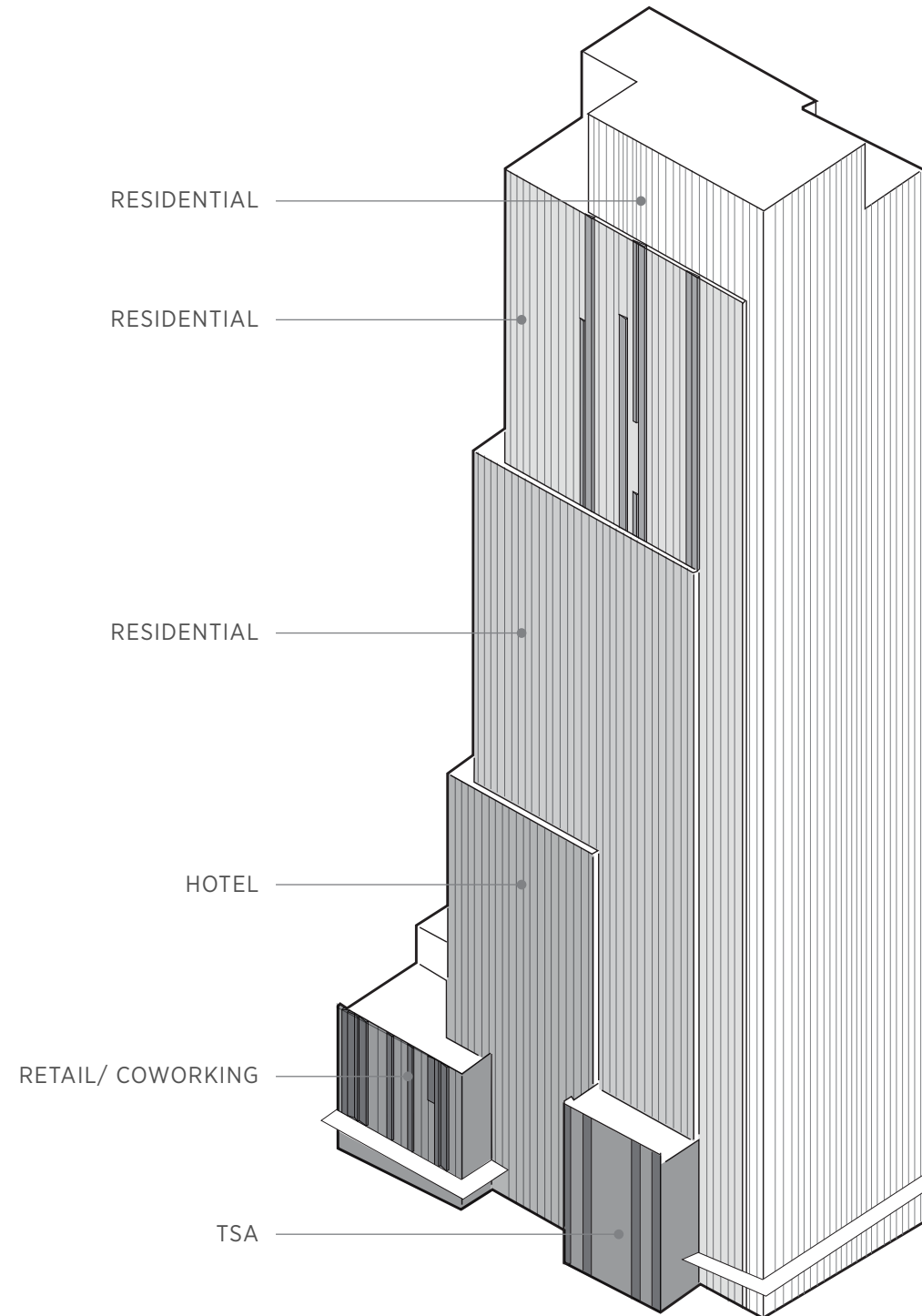
The tower's telescoping form is informed by programmatic transitions throughout the tower's mass, and expresses its three key uses: retail/ coworking, hotel and residential, emphasizing the tower's mixed-use nature.

Angled facade panels reference historic pilasters on the landmark Terminal Sales Annex Building below, further reinforcing the tower's connecting to street level as well as its past. Subtle changes in the panel texture reinforce the massing strategy.

With its unique facade panels, originating at the ground and reaching vertically up to the sky, the project strives to become a wayfinding device within the city.

DESIGN GUIDELINES

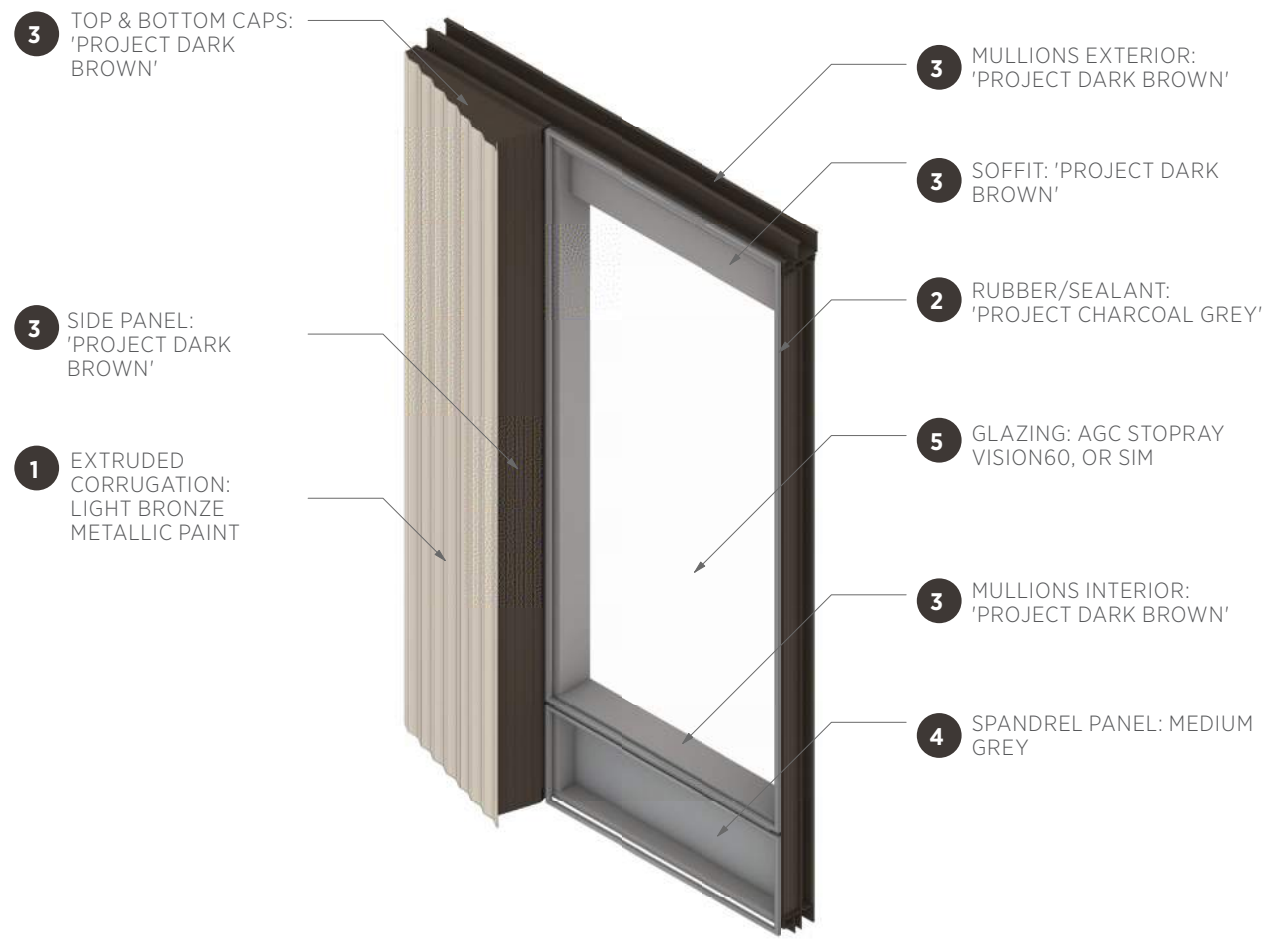
B4 Design a Well-Proportioned & Unified Building



TOWER CURTAIN WALL



TOWER CURTAIN WALL - TYPICAL CONDITION



TYPICAL CONDITION



CORRUGATED PANEL



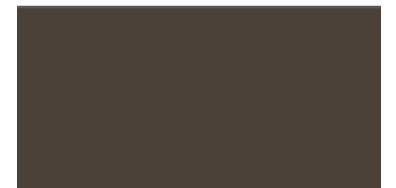
TYPICAL GLAZING



1 KYNAR METALLIC COATING: LIGHT BRONZE COLOR



2 'PROJECT CHARCOAL GREY' BEN. MOORE: WROUGHT IRON 2124-10



3 'PROJECT DARK BROWN' BEN. MOORE: NIGHT HORIZON 2134-10



4 SPANDREL GREY BEN. MOORE STEEL WOOL 2121-20



5 AGC STOPRAY VISION60

RESPONSE TO BOARD GUIDANCE

INTERACTIVE ENTERTAINMENT SPACE

5 2ND AVE. INTERACTIVE ENTERTAINMENT SPACE

SUPPORTED AT EDG

BOARD GUIDANCE

The Board was intrigued by the proposed interactive entertainment space (potential radio station use), and would like more information on what physical design features are proposed to support this use and space as an activated and engaging use along the street. The response should demonstrate how the design will remain engaging and active if this use changes.

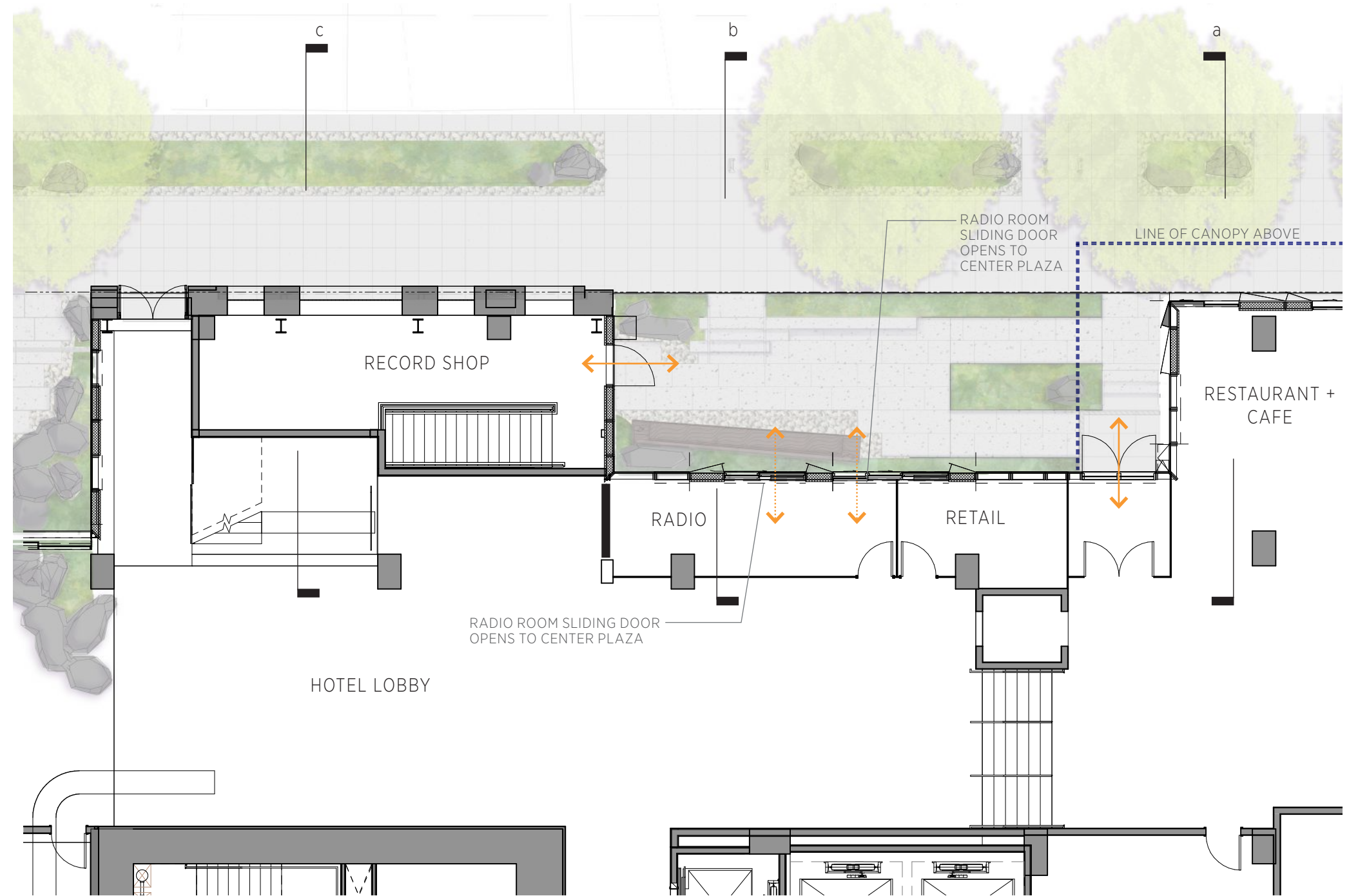
RESPONSE

The ground floor encourages foot traffic in and out of the building by bringing downtown amenities (a coffee stand and restaurant) to workers, visitors, and residents alike. Open space and seating, framed by lush native plantings, promote use of the plaza by the public and provides spaces to meet or relax while listening to live broadcasts from the Radio.

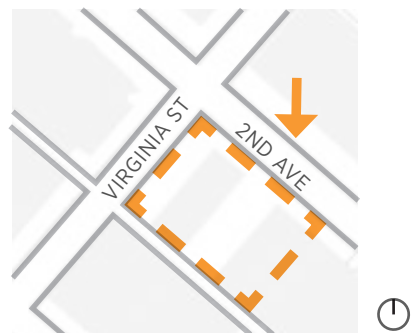
As well, by extending the sidewalk paving pattern into the plazas, the project widens the sidewalk at these Strategic moments of the ground floor, easing the transition. Finally, full height, sliding glass panels at the Radio strengthen the connection.

DESIGN GUIDELINES

C1.V. Pedestrian Attraction

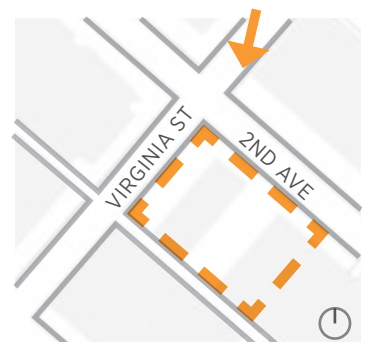


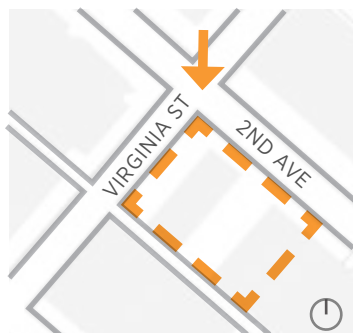
PARTIAL PLAN L1
SCALE: 3/32" = 1'-0"



RESPONSE TO BOARD GUIDANCE

2ND AVENUE





LANDSCAPE

Street Level Concept

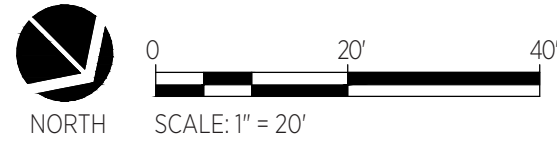
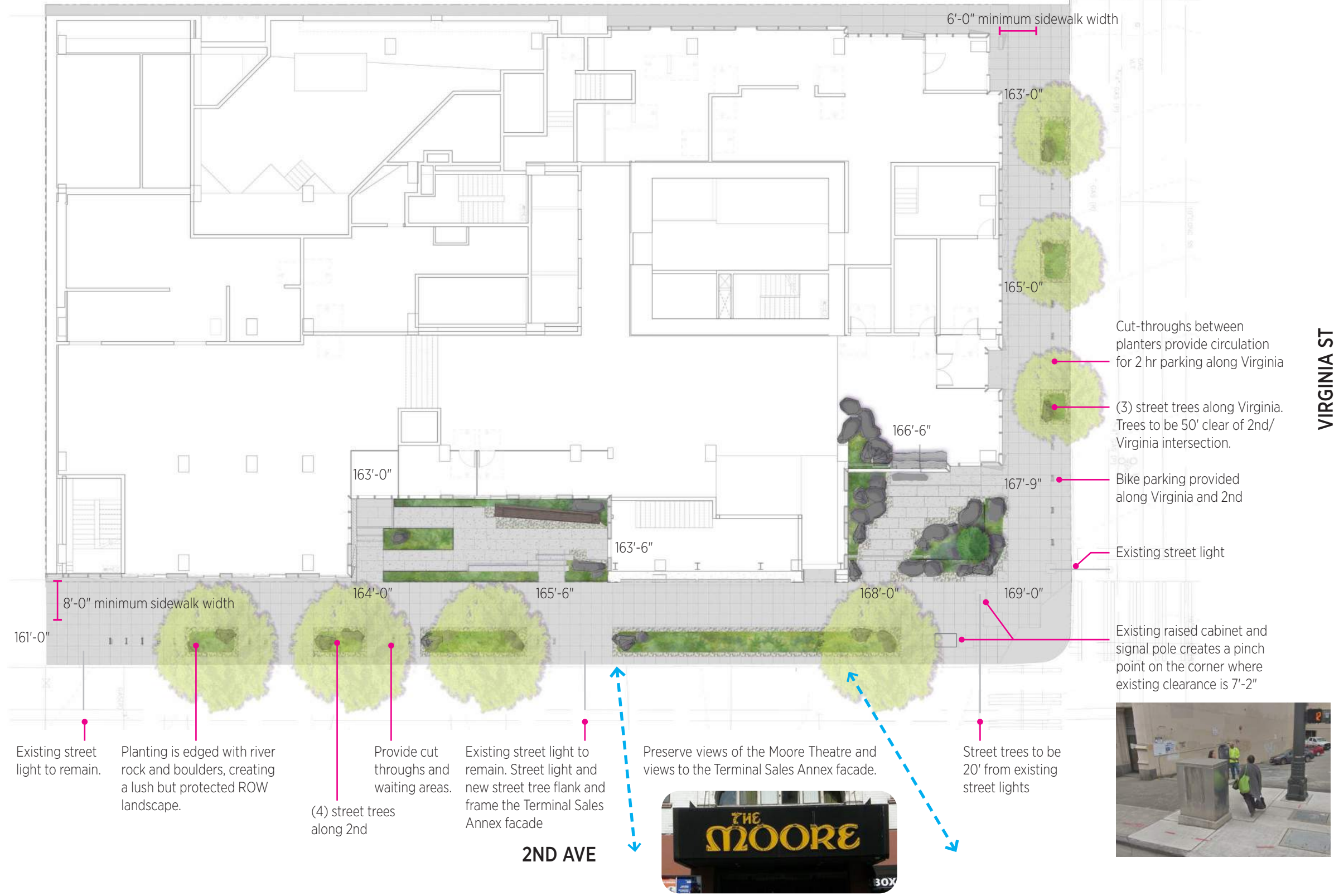
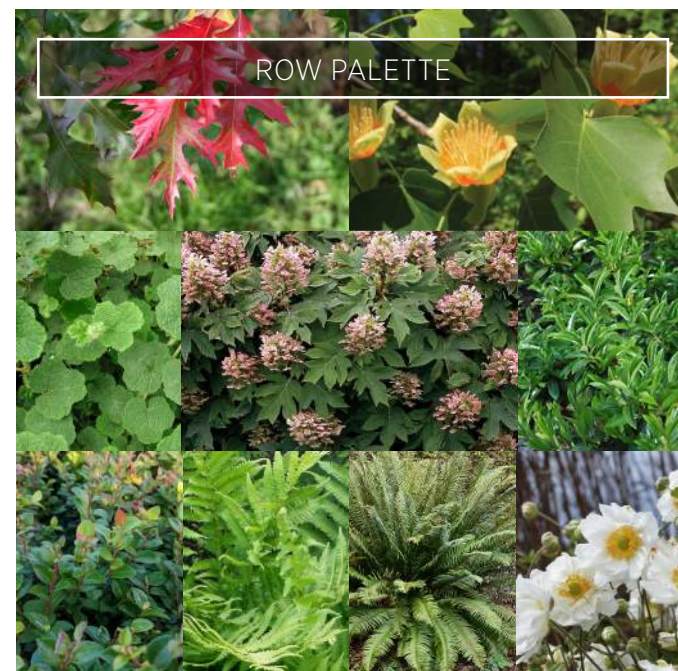
Street level design is driven by fluid transitions between streetscape, plazas, and interior spaces.

ROW, 2nd Ave Plaza

Right of Way

The ROW concept carries design moves including planting, materials and paving across the sidewalk creating continuity from inside the building to plaza spaces to the public realm. The ROW concept acknowledges that there will be high demand for ride share services and access from curbside to main building entries and provides frequent pedestrian cut-throughs.

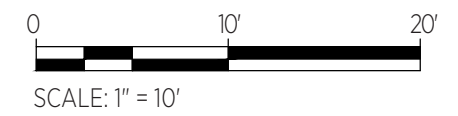
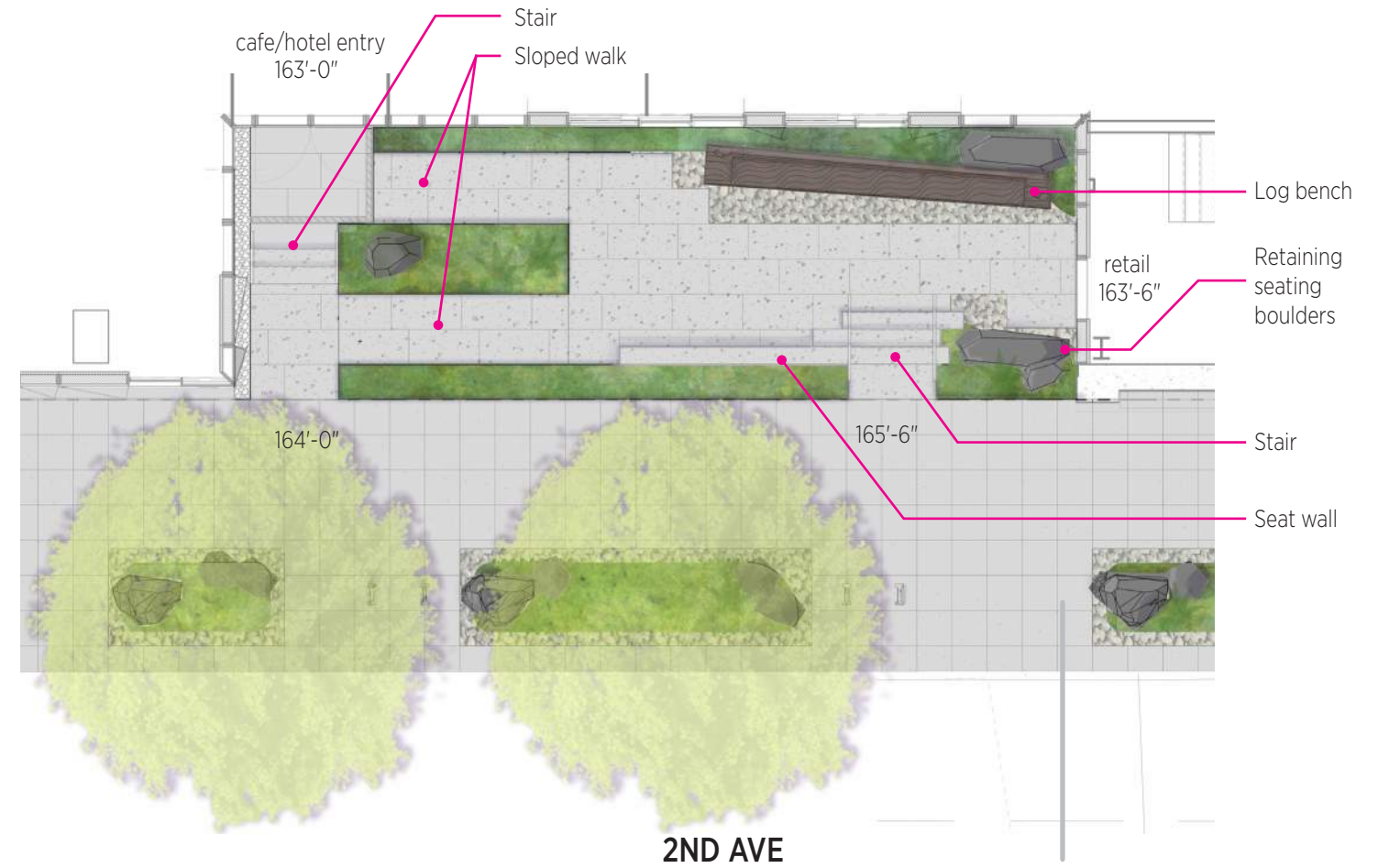
Existing utilities, bike parking requirements, and requirements for sidewalk widths / planting strips set the framework for new tree and planter locations.



2nd Ave Plaza Concept

Materials and lush planting create continuity from NE corner and provide indoor/outdoor continuity.

The plaza provides direct and ADA accessibility to building entries and sloping sidewalk, and provides seating perches to view activity and provide amenity for retail patrons and all.



LANDSCAPE

NE Plaza Concept

Virginia Street is activated with main hotel and residential entries, indoor/outdoor visual access, and an interesting, permeable corner gathering space.

The corner plaza creates visual interest from the sidewalk and from inside the building while providing a welcoming area to travel through or gather. Permeability to the streets and building edges is a primary driver of this space.

The plaza and sidewalk connections are ADA accessible, and further activated with operable windows that open up to a stair descending into the building and take-away coffee stand.

Paving pattern is a linear play off the 2x2 Seattle standard sidewalk grid, and flows from the building entries into the sidewalk grid. Paving dissolves into rock and seating/retaining boulders edge lush planting. Lush bioretention is integrated into the corner, treating roof terraces.



6 ALLEY GLASS CORNER

SUPPORTED AT EDG

BOARD GUIDANCE

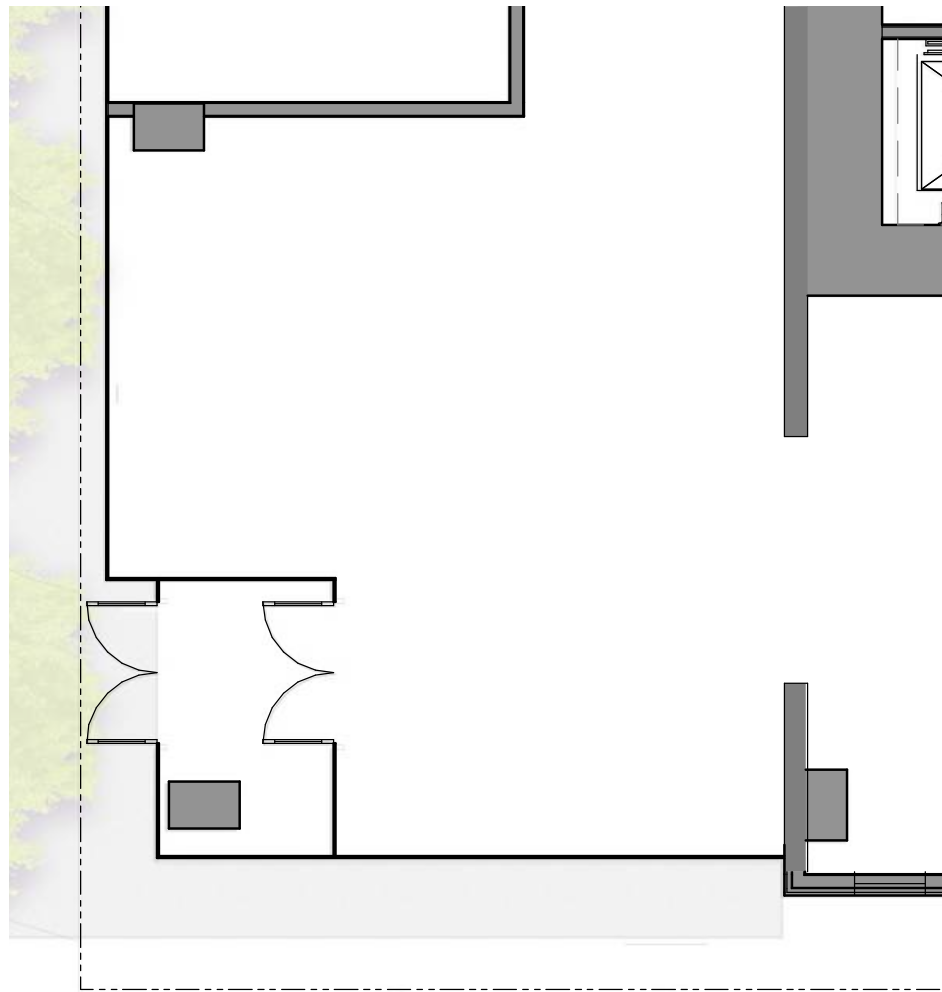
The Board gave guidance to further look at practicality of the glass corner and indicated whether this material would require bollards or column to protect the corner.

RESPONSE

The design has evolved since EDG to remove the glass corner and have a solid, protected corner at the alley entry.

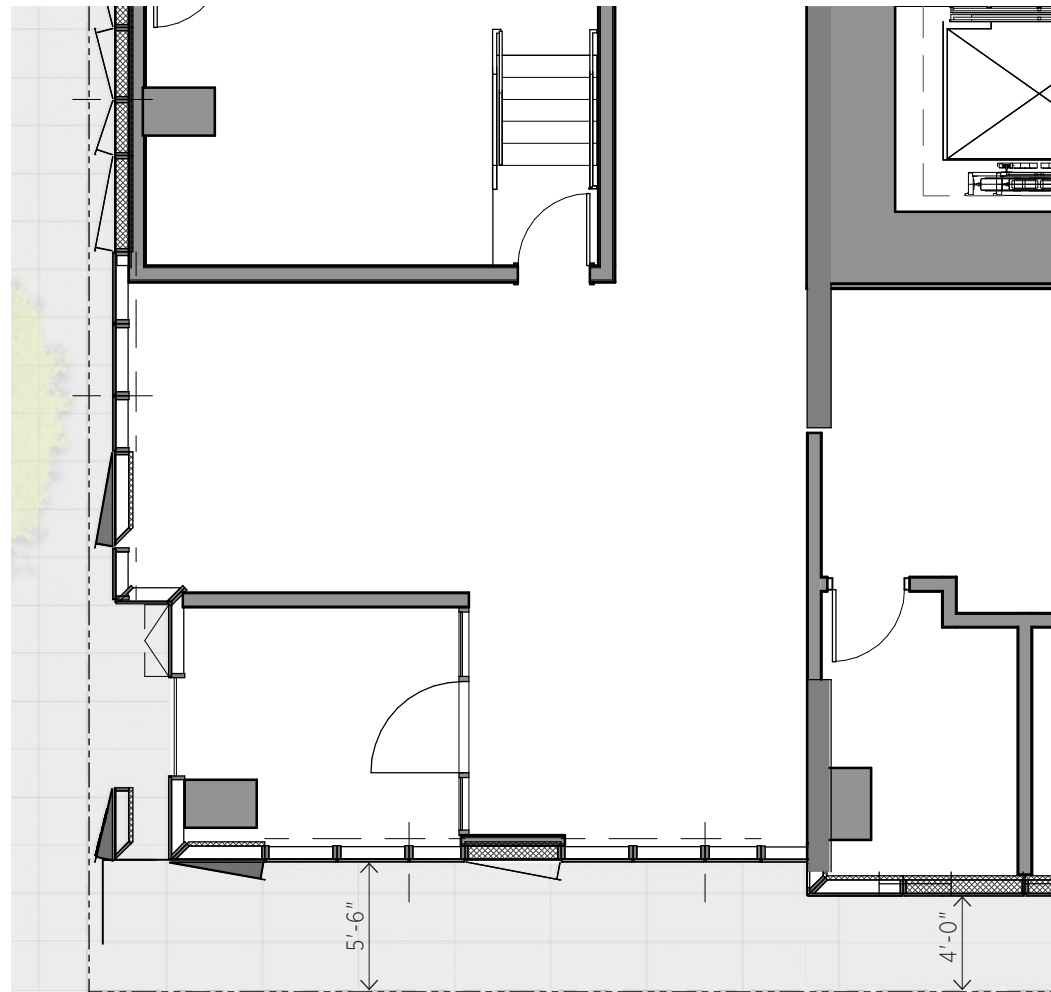
DESIGN GUIDELINES

C6 Develop the Alley Facade



ALLEY

EDG 2 DESIGN
SCALE: 1/8" = 1'-0"



ALLEY

CURRENT DESIGN
SCALE: 1/8" = 1'-0"

RESPONSE TO BOARD GUIDANCE

ALLEY

7 ALLEY ARTICULATION

SUPPORTED AT EDG

BOARD GUIDANCE

The Board acknowledged the utilitarian use of the southwest corner, however, requested more information on how the articulation of the alley will reinforce the overall tower concept. Specifically, the Board requested the design of the loading dock doors be considered.

RESPONSE

The design of the alley is very much an extension of the overall tower concept. At the northwest corner, the tower articulation comes all the way down to the ground. Moving into the middle of the alley, the design transitions to capture the footprint of the Terminal Sales Annex before becoming more utilitarian on the south. The loading dock doors will be high-cycle, vertical stacking, sectional doors. An image is provided on the next page TSA facade provides new transparency at ground floor.

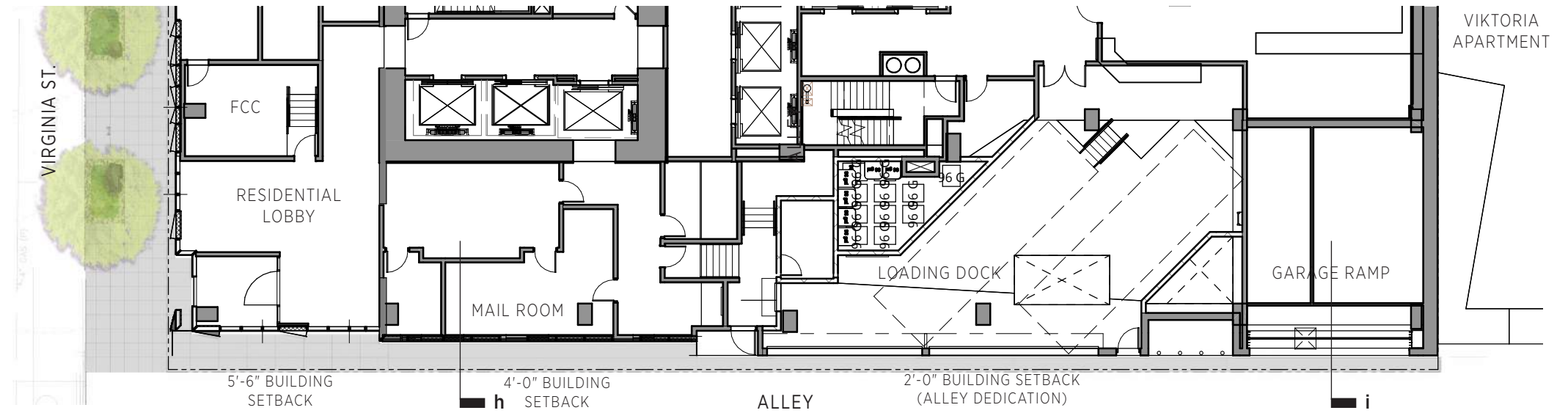
Recycling and waste are located below grade, away from alley.

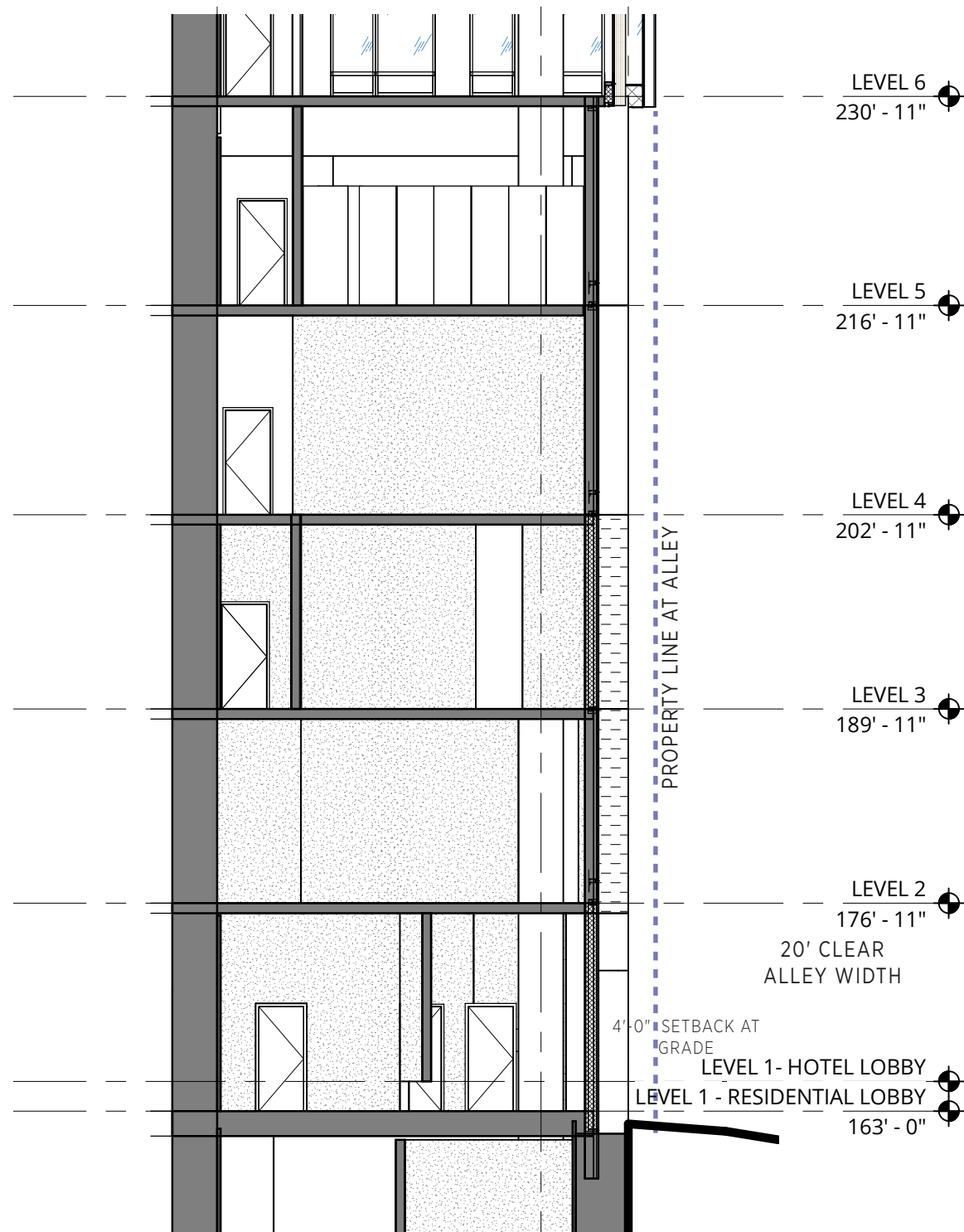
DESIGN GUIDELINES

C6 Develop the Alley Facade

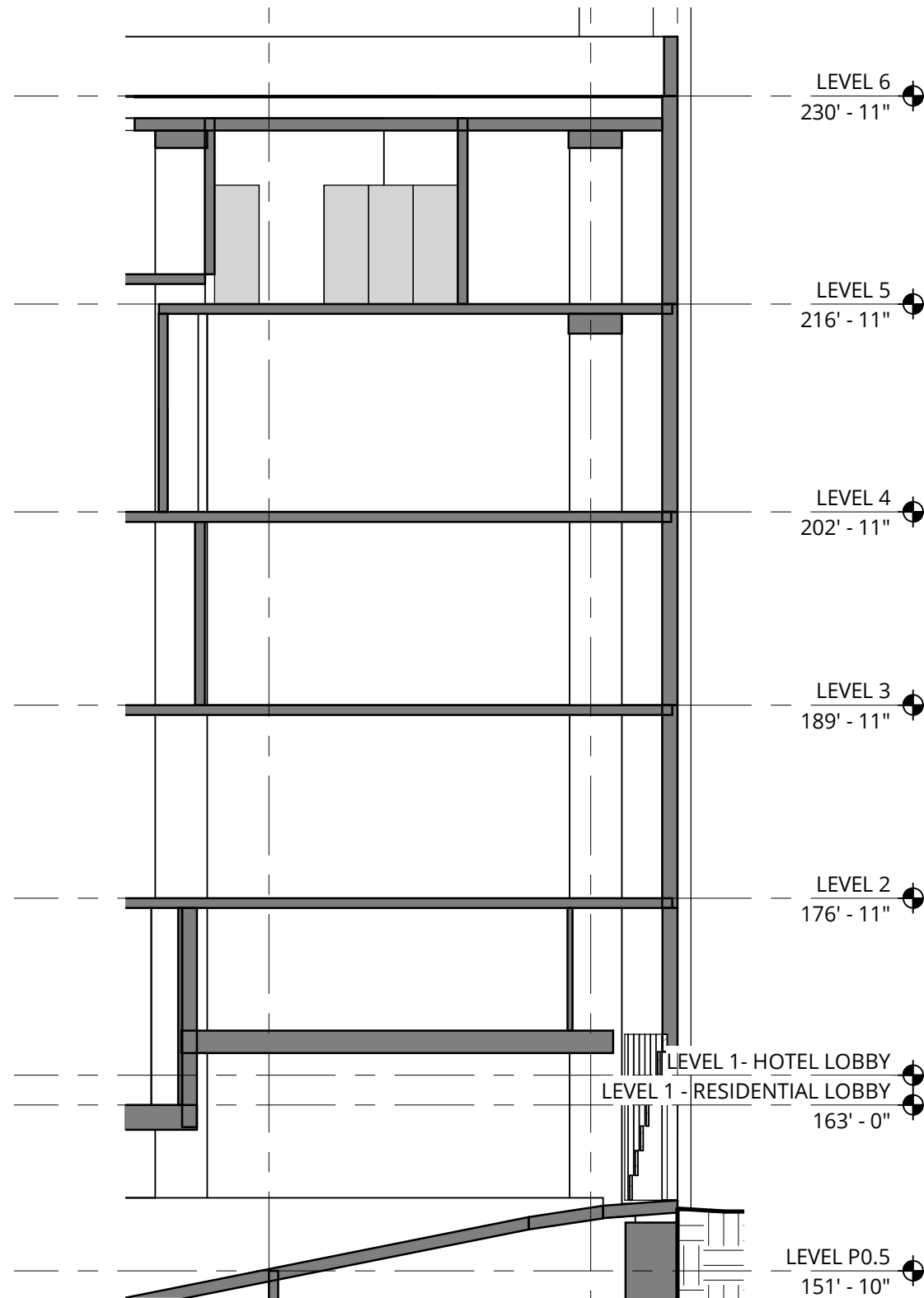
E1.1 Vehicle Access Considerations

C6.III Architectural Concept





h. SECTION THRU BOH AT ALLEY
SCALE: 3/32" = 1'-0"



i. SECTION THRU PARKING GARAGE
SCALE: 3/32" = 1'-0"



GARAGE DOOR DESIGN, PAINT TO MATCH SURROUNDING WALL COLOR



ALLEY

DEPARTURES

DEPARTURES

DEPARTURE 1

Street Facade, Landscaping, and Street Setback Requirements

SUPPORTED AT EDG

SMC 23.49.056

"Along 2nd Avenue, facades between 15 and 35 feet above sidewalk grade shall be located within 2-feet of the street lot line, except as permitted below:

- Maximum setback is 10 feet"

REQUEST:

A departure is requested to setback the facades surrounding the Terminal Sales Annex (TSA) greater than 10'. We are requesting 15' on the south and 20.5' on the north.

JUSTIFICATION:

South of the TSA: The #1 priority of the Landmarks Preservation Board (LPB) is to set back the new tower from the landmark structure. In conversations with the ARC over the years, a 15' setback is a reasonable starting point for tower setback from the Landmark. In this location, which supports our overall design concept, the tower form comes down the ground thus creating the 15' setback.

North of the TSA: As the tower begins to telescope and step back from the Landmark, the setback dimensions transition from 15' to 22' at the corner. This tower setback allows visibility to the Terminal Sales Annex and creates a grand open space to denote entry and respite for pedestrians.

RELEVANT DESIGN GUIDELINES:

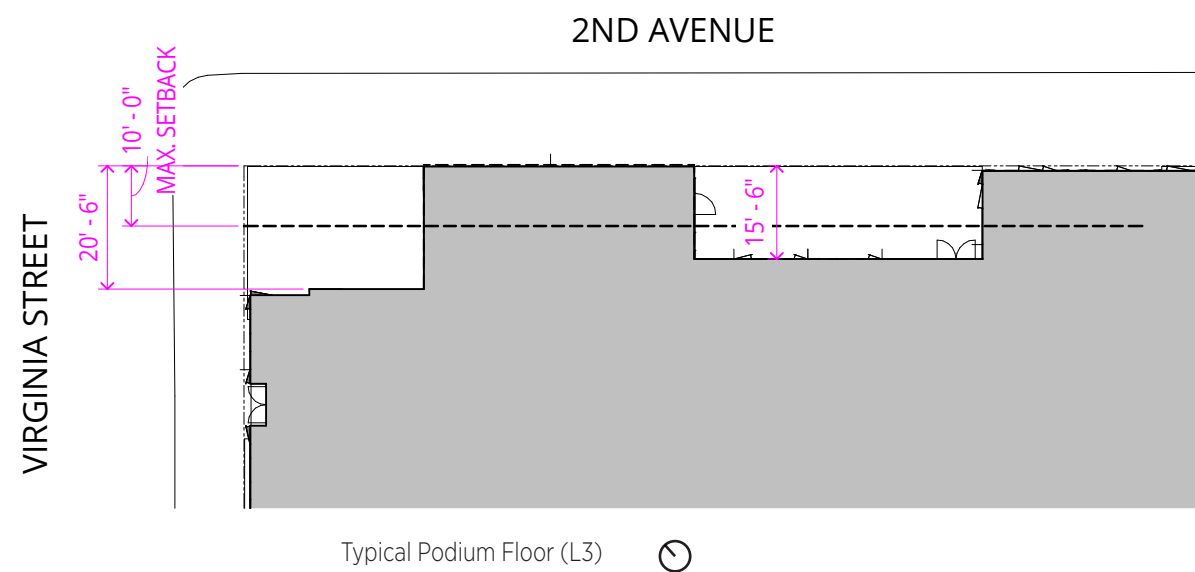
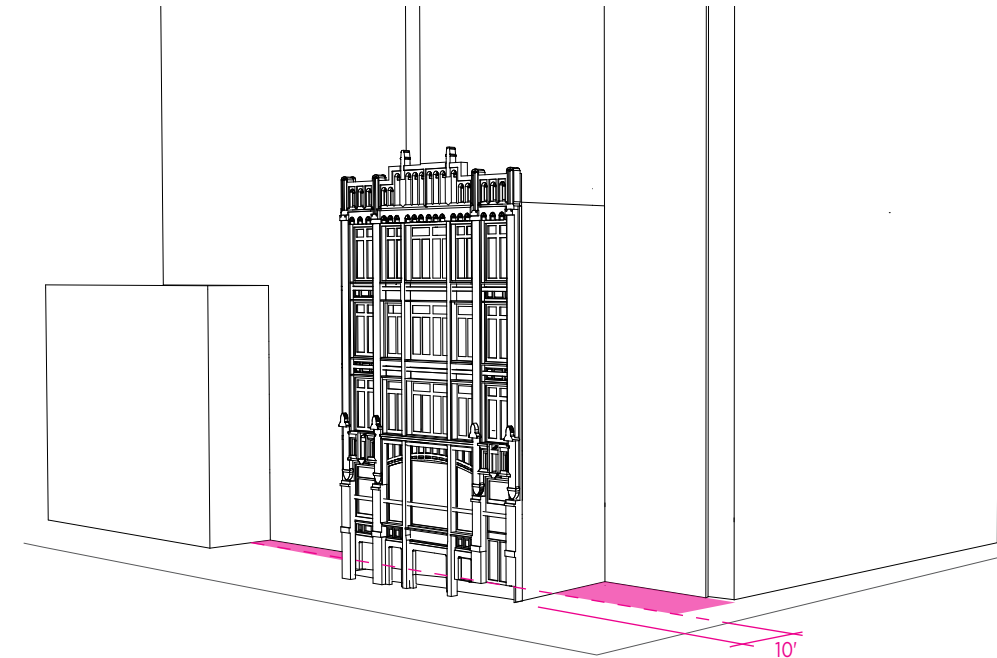
- B-2 Create a Transition in Bulk and Scale
- B-3 Reinforce Positive Urban Form & Arch. Attributes
- D-1 Provide Inviting and Usable Open Space

LANDMARK PRESERVATION BOARD PRIORITIES:

1. Set tower back from Landmark
2. Express volume of Landmark through the building
4. Retain existing entry door of Landmark
6. Tower expression should draw inspiration from TSA
7. Landmark to complete corner

DRB DIRECTION FROM EDG REPORT

The Board indicated preliminary support for the requested departure as the proposed setbacks provide additional relief for the existing landmark structure, as well as creating opportunity for enhanced entry experiences. (B-2 Create a Transition in Bulk and Scale, B-3 Reinforce Positive Urban Form & Arch. Attributes, D-1 Provide Inviting and Usable Open Space)



DEPARTURE 2

Street Facade, Landscaping, and Street Setback Requirements

SUPPORTED AT EDG

SMC 23.49.056

"Along 2nd Avenue, facades between 15 and 35 feet above sidewalk grade shall be located within 2-feet of the street lot line, except as permitted below:

- No setback deeper than deeper than 2-feet shall be wider than 20-feet"

REQUEST:

A departure is requested to increase width of setback to relate to the TSA on the south, while opening the corner to Landmark on the North. We are requesting 43' on the south and 30' on the north.

JUSTIFICATION:

South of the TSA: The width of the setback on the south of TSA is directly related to the width of the TSA itself. The TSA is approximately 45' wide and the width of the setback is approximately 48'. This width also is directly related to the proportions of the tower's telescoping form concept.

North of the TSA: To create a corner open space and for the Landmark to 'front' the corner, a greater setback is required. This corner open area reveals the lower edge of the tower as it descends from above, hovering just above ground at the main entry.

RELEVANT DESIGN GUIDELINES:

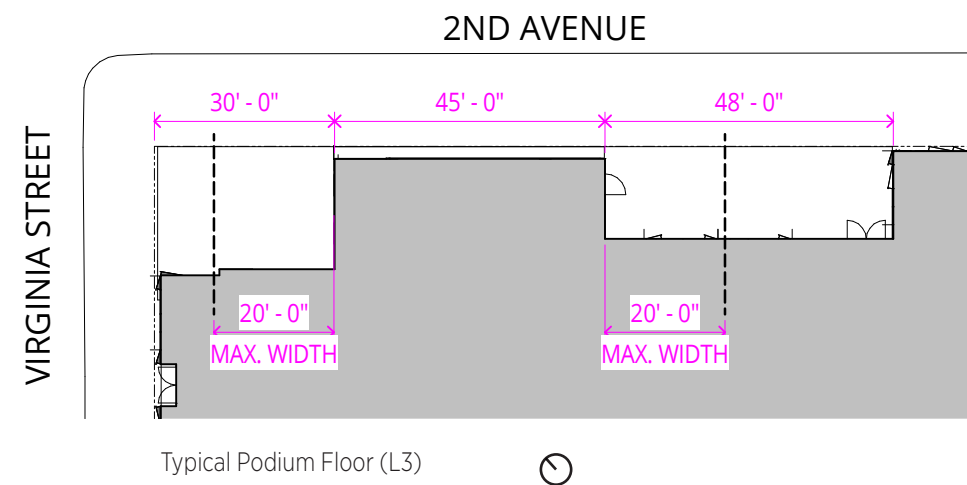
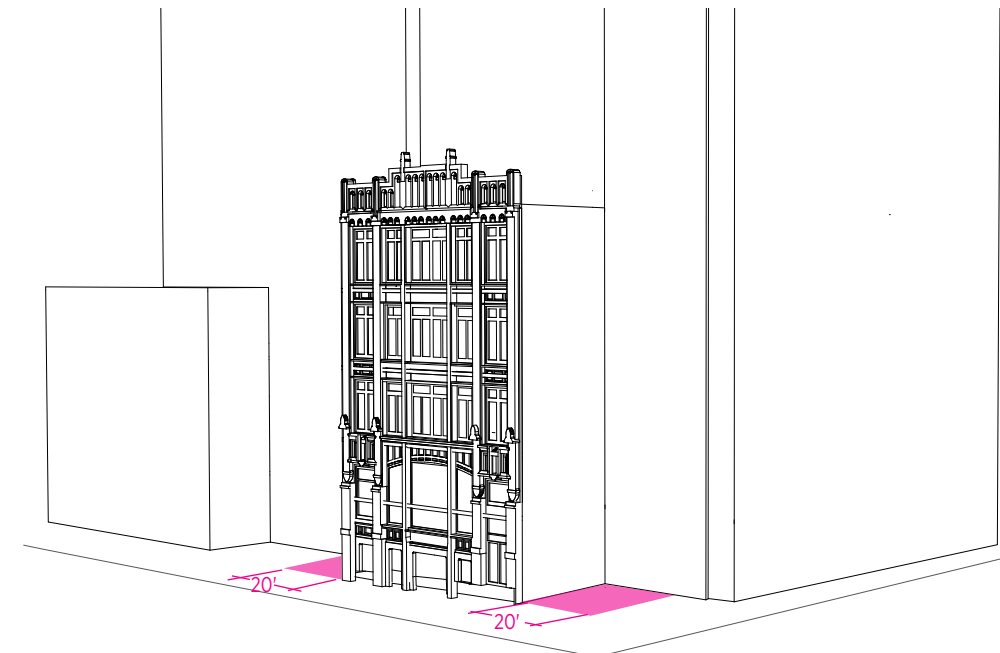
- A-1 Respond to the physical environment
- B-2 Create a transition in bulk and scale
- D-1 Provide inviting and usable open space

LANDMARK PRESERVATION BOARD PRIORITIES:

1. Set tower back from Landmark
2. Express volume of Landmark through the building
4. Retain existing entry door of Landmark
6. Tower expression should draw inspiration from TSA
7. Landmark to complete corner

DRB DIRECTION FROM EDG REPORT

The Board indicated preliminary support for the requested departure as the proposed setbacks provide additional relief for the existing landmark structure, as well as creating opportunity for enhanced entry experiences. (B-2 Create a Transition in Bulk and Scale, B-3 Reinforce Positive Urban Form & Arch. Attributes, D-1 Provide Inviting and Usable Open Space)



DEPARTURES

DEPARTURE 3

Maximum Tower Width

SUPPORTED AT EDG 2

SMC 23.49.058C.2

"In DMC zones, the maximum facade width for portions of a building above 85 feet along the general north/south axis of a site (parallel to the Avenues) shall be 120 feet or 80 percent of the lot measured on the Avenue, whichever is less"

REQUEST:

A departure is requested to extend the residential portion of the tower by 12' at the widest section.

JUSTIFICATION:

The current maximum facade width is not ideal for the historic landmark on site. When the tower sets back from the landmark, it immediately creates a more rectangular tower which does not allow an average tower plate of 10,700sf without increasing the overall width. By setting the entire tower back from the landmark by 18', it essentially widens the tower to create an average residential footprint of 10,500sf.

At EDG 1, the project team had requested 19' of additional width at Levels 14-27, and additional 9' at Levels 28-41. At EDG 2, the project team has significantly reduced the tower width to better meet the tower width requirement. The project now meets the tower width requirement at the top portion of the tower, and a departure is only requested at the middle section of the tower. The Architectural Review Committee is supportive of this departure.

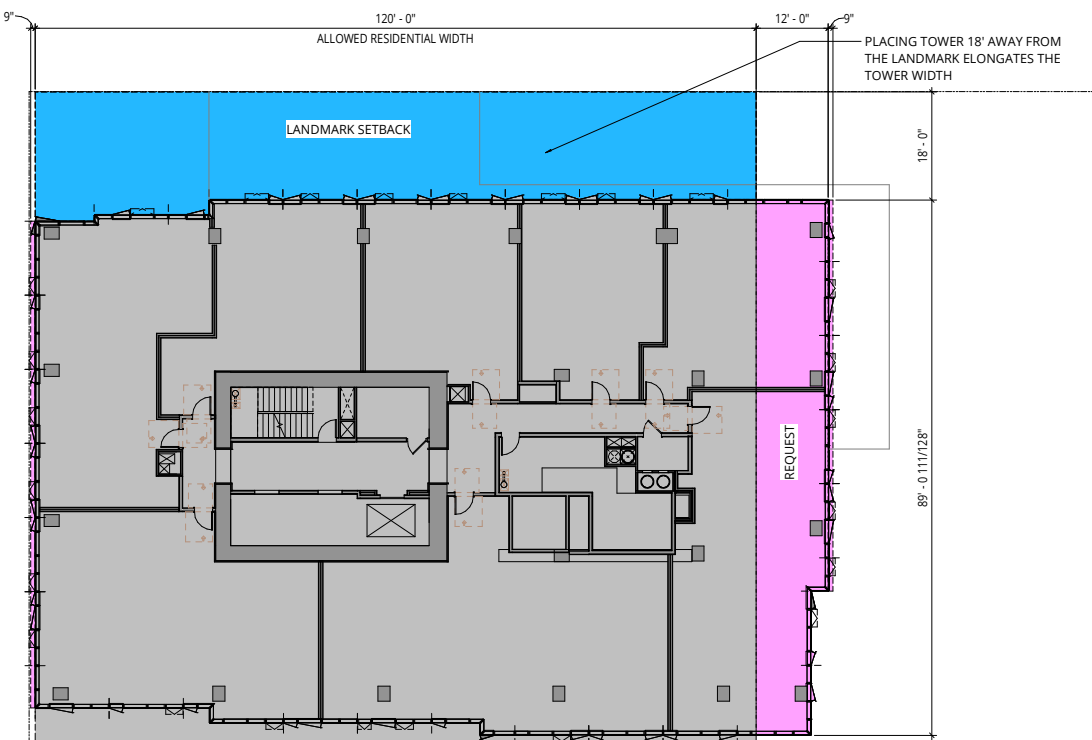
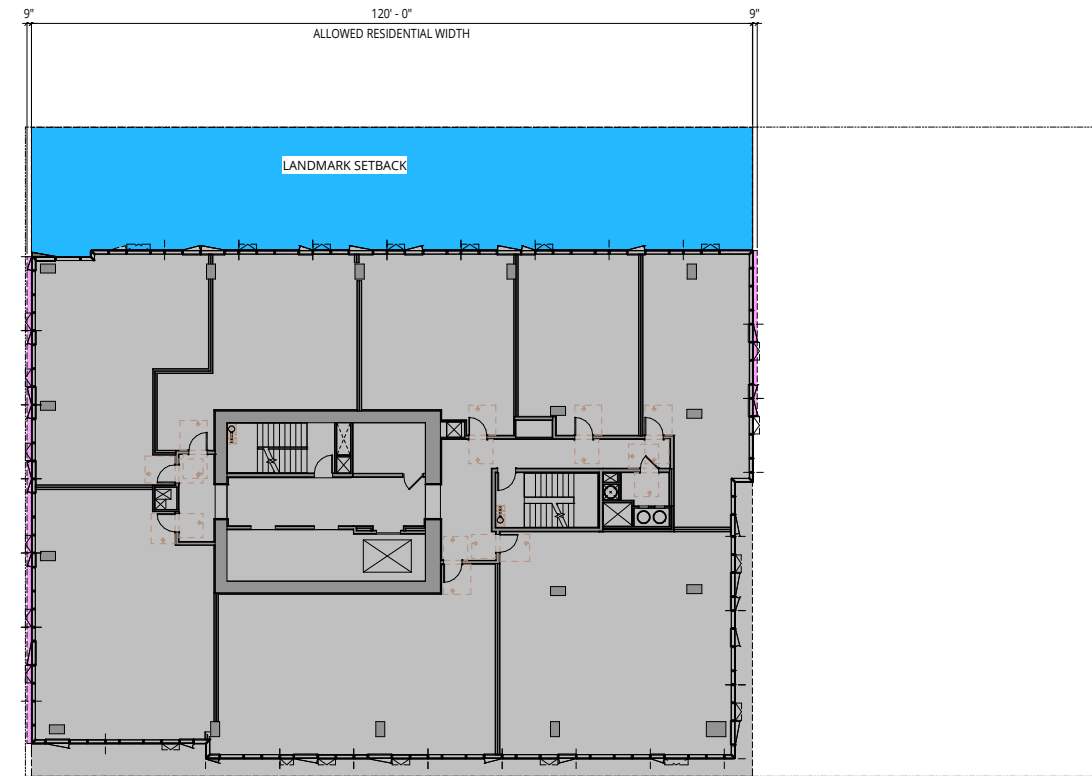
RELEVANT DESIGN GUIDELINES:

- B-1 Respond to the Neighborhood Context
- B-2 Create a Transition in Bulk and Scale
- B-3 Reinforce Positive Urban Form & Arch. Attributes

DRB DIRECTION FROM EDG REPORT

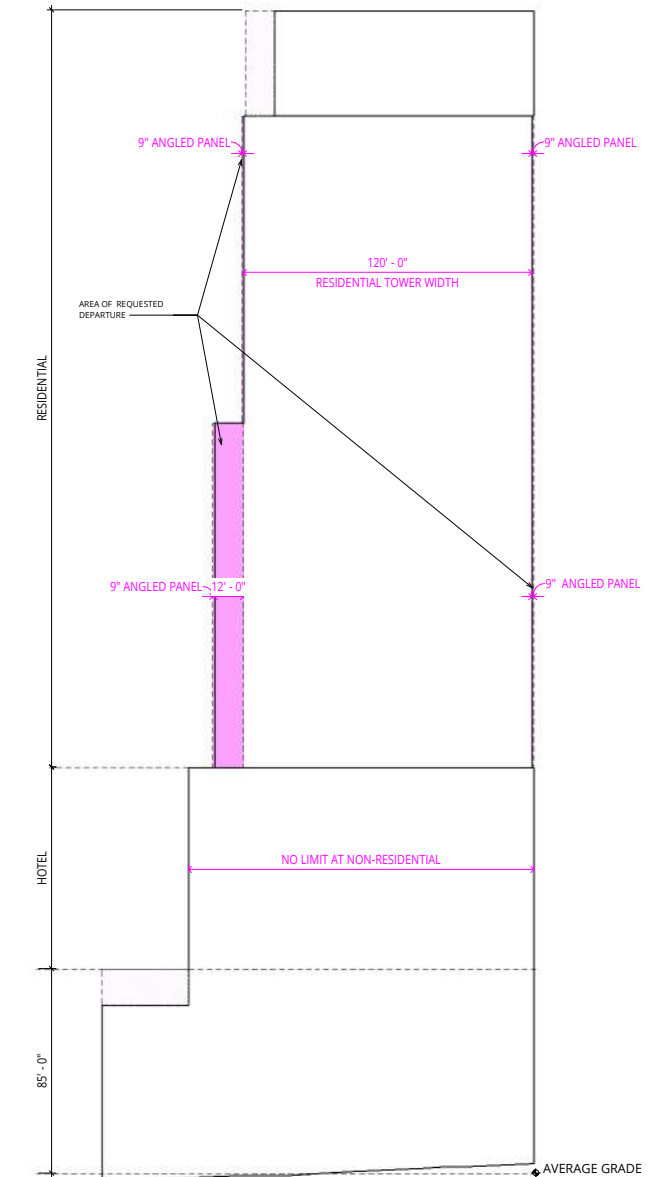
The Board indicated they preliminary support for the requested tower width departure, as the increased width reinforces the design of the tower stepping and enhances the scaling moves in the upper tower levels. The Board also noted they appreciated the reduction in the departure request, which should be maintained moving forward. (A2.1. Desired Architectural Treatments, B1 Respond to the neighborhood context, B-2 Create a Transition in Bulk and Scale, B-3 Reinforce Positive Urban Form & Arch. Attributes)

- The overall tower width has been reduced to 120'-0" at Level 30
- The total average floor plate is 10,500 sf, 200 sf under the maximum floor plate allowed.



	EDG 1		EDG 2	
	floor	gross	floor	gross
L 31-41	738 sf	8,118 sf	-	-
L 30	1,025 sf	1,025 sf	-	-
L 28-29	1,025 sf	2,050 sf	-	-
L 16-27	1,355 sf	16,260 sf	740 sf	1,480 sf
L 15	1,355 sf	1,355 sf	740 sf	8,880 sf
		28,808 sf		10,360 sf

64%



DEPARTURE 4

Loading Berth

SUPPORTED AT EDG 2

SMC 23.54.035C.1

"Width and Clearance. Each loading berth shall be not less than ten (10) feet in width and shall provide not less than fourteen (14) feet vertical clearance."

REQUEST:

Three full width and length (10' x 35') loading berths are provided. A departure is requested to reduce the height of one of the three required loading berths to 9'-0".

JUSTIFICATION:

Quantity and size can be reduced by Director's Rule, Type 1 Decision, however there is no provision for height. At EDG, a third full size loading berth was provided in the porte cochere area. In order to minimize curb cuts and the presence of service areas, the third loading berth has been moved to the first parking level allowing more active uses off of Virginia Street. This loading berth will be used by smaller delivery and moving vans.

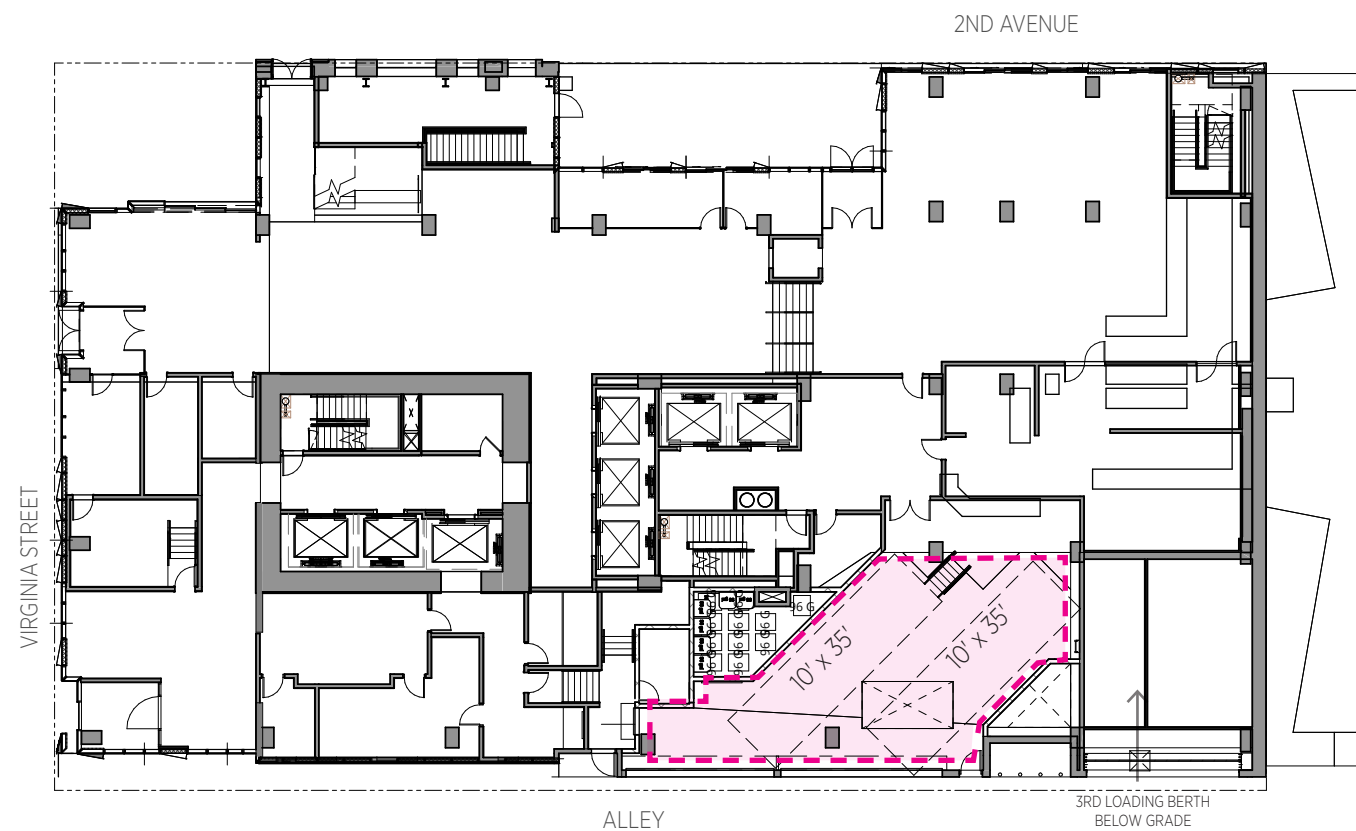
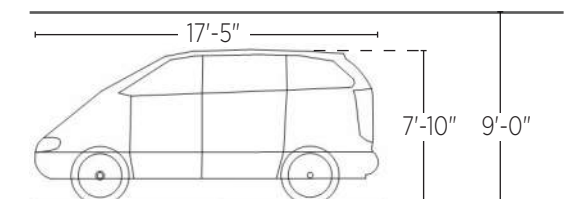
RELEVANT DESIGN GUIDELINES:

- C-1 Promote Pedestrian Interaction
- C-6 Develop the alley facade
- E-1 Develop the alley facade
- E-3 Minimize the presence of service areas

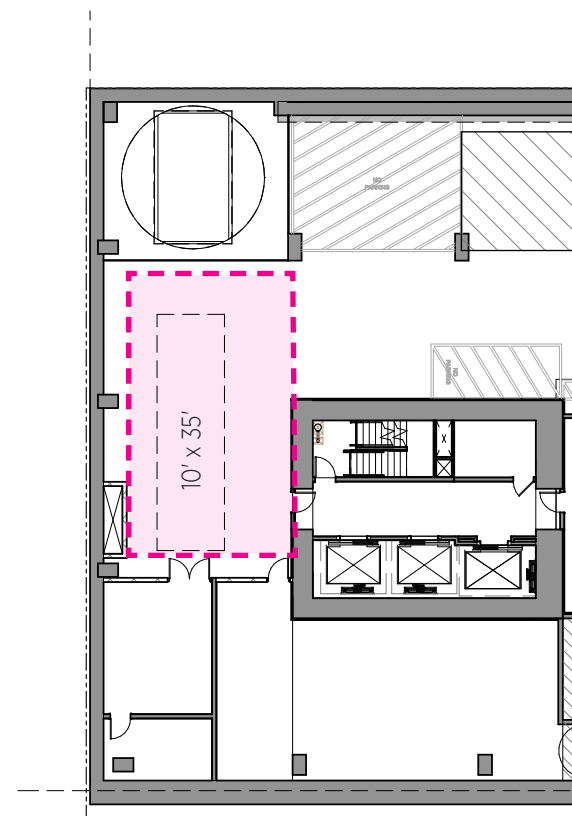
DRB DIRECTION FROM EDG REPORT

The Board indicated they preliminary support for the requested departure, as they supported an interior loading space completely off the alley. (C1 Promote Pedestrian Interaction, C6 Develop the Alley Façade, E1 Minimize Curb Cut Impacts, E3 Minimize the Presence of Service Areas)

Clearance for typical delivery van: 7'-10"



EDG 2 Ground Floor Plan



EDG2 Level P1 Plan - North



THANK YOU

2ND AND VIRGINIA