



# SIXO

## 1818 6TH AVE

601 Stewart St, 1818 6th Ave  
Project #: 3038764-EG

### EARLY DESIGN GUIDANCE MEETING

DOWNTOWN DESIGN REVIEW BOARD

January 4, 2022

[THIS PAGE LEFT INTENTIONALLY BLANK]

# DATA

## NEW COMBINED ADDRESS:

1818 6th Avenue, Seattle, WA 98101

## PREVIOUS ADDRESSES:

601 Stewart Street, Seattle, WA 98101

1818 6th Avenue, Seattle, WA 98101

600 Olive Way, Seattle, WA 98101

## SDCI PROJECT NO:

#3038764-EG

## LEGAL DESCRIPTION:

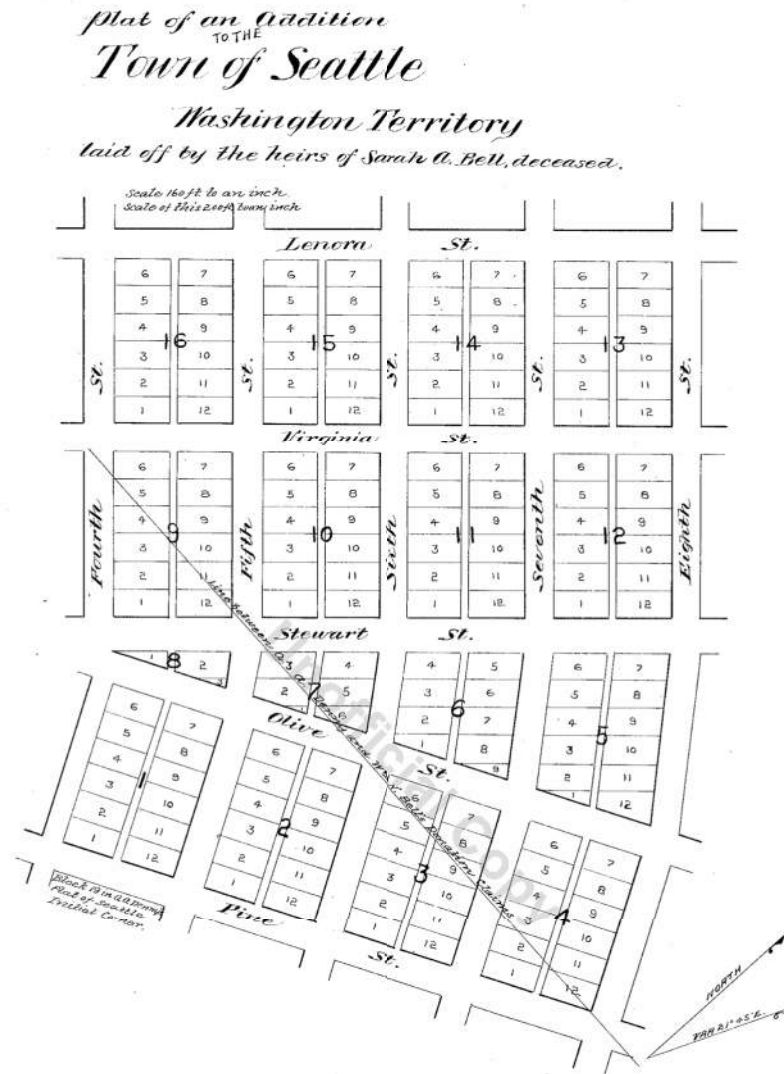
The site is within the original land claim of W.N. Bell, which was incorporated into the Town of Seattle as the Heirs of Sarah A. Bell Addition, according to the plat thereof recorded in volume 13 of plats, page 24 in King County, Washington. The parcels are located in Plat Block 6. This project will combine Plat Lot 4 (065900-0350, 601 Stewart Street), Plat Lot 3 (065900-0340, 1818 6th Avenue), and already combined Plat Lots 2 and 1 (065900-0330, 600 Olive Way).

## PARCEL NO:

065900-0350 (601 Stewart Street)

065900-0340 (1818 6th Avenue)

065900-0330 (600 Olive Way)



SEE SURVEY VOL. 13 PAGE 24

*This Addition is a portion of W. N. Bell's & A. A. Denny's Donation Claims, in Town 25 N. of Range 4 East of Will Mer. Lots not fractional, 60 by 120 ft. Streets 16 ft. alleys 16 ft. Streets south of Olive Street correspond in their courses with those on A. A. Denny's Addition to Seattle, Streets north of Stewart Street correspond with A. A. Denny's and W. N. Bell's Addition. Streets between Stewart and Olive Sts. run to connect the two courses together, all lying south of Division line between A. A. Denny and W. N. Bell's belongs to A. A. Denny and include Blocks 1, 2 & 8 with parts of blocks 3, 4, 7 & 9. W. N. Bell guardian of Austin A. Bell, W. N. Bell Attorney in fact of Laura K. Coffman, Olive J. Stewart & Mary Y. Bell heirs of Sarah A. Bell deceased, A. A. Denny.*

Territory of Washington }  
County of King }  
On this Eighth day of April A. D. 1872 before me the undersigned a Notary Public in and for Washington Territory personally came William N. Bell and Arthur A. Denny to me known to be the individuals who executed the within Town Plat, the first named as guardian for the minor heirs of Sarah A. Bell, deceased, and as Attorney for the other heirs, the second named in his individual capacity and acknowledged the execution of the within plat, as their act and deed.  
Witness my hand and Seal the day and year above written.  
George F. Whitworth  
Notary Public W. T.

Recd for Record Apl 9<sup>th</sup> 1872 at 3 O'Clock P.M.  
G. Kellogg Auditor.

Recorded in the Records of King Co. W. T. in Vol. 6 of Deeds on pages 196 & 197 Re-Recorded per order County Com's at Feb'y Term 1875 in this book S. C. Harris Draughtsman April 30 1875.

# PROJECT TEAM

## OWNER:

Kilroy Realty Corporation  
601 108th Avenue NE, Suite 1560  
Bellevue, WA 98004  
CONTACT: John Gillespie

## ARCHITECT:

Miller Hull Partnership, LLP  
71 Columbia Street, 6th floor  
Seattle, WA 98104  
CONTACT: Whitney Pearce

## LANDSCAPE ARCHITECT:

Site Workshop  
3800 Woodland Park Ave. N.  
Suite 200  
Seattle, WA 98103  
CONTACT: Mark Brands

# KILROY REALTY CORPORATION

Kilroy Realty Corporation (KRC), a publicly traded real estate investment trust and member of the S&P MidCap 400 Index, is one of the West Coast's premier landlords. The Company has over 70 years of experience developing, acquiring and managing office and mixed-use real estate assets. The Company provides physical work environments that foster creativity and productivity and serves a broad roster of dynamic, innovation-driven tenants, including technology, entertainment, digital media and health care companies.

Kilroy Realty is seeking entitlements to develop a state-of-the-art mixed-use project consisting of approximately 900,000 square feet of office, including the full restoration of the Lloyd Building, and approximately 25,000 square feet of street-level food and beverage retail as well as underground parking. In-place zoning on the 621 Stewart St. parcel allows for approximately 575,000 square feet of residential development, for which the company is evaluating various options. The proposed project will target the highest levels of sustainability.

Kilroy Realty has past experience with historic preservation, including the CBS Columbia Square building in Hollywood, California which earned a Conservancy Preservation Award in 2017.

The company's commitment and leadership position in sustainability has been recognized by various industry groups across the world. In 2020, the company was recognized by GRESB as the sustainability leader in the Americas across all asset classes for the seventh time. Other sustainability accolades include NAREIT's Leader in the Light award for the past seven years and the EPA's highest honor of ENERGY STAR Partner of the Year Sustained Excellence award for the past six years. The company is listed in the Dow Jones Sustainability World Index. At the end of the fourth quarter of 2020, the company's stabilized portfolio was 68% LEED certified and 67% of eligible properties were ENERGY STAR certified. More information is available at <http://www.kilroyrealty.com>



333 Dexter, Seattle, design by Miller Hull (Completed 2021)



350 Mission, San Francisco



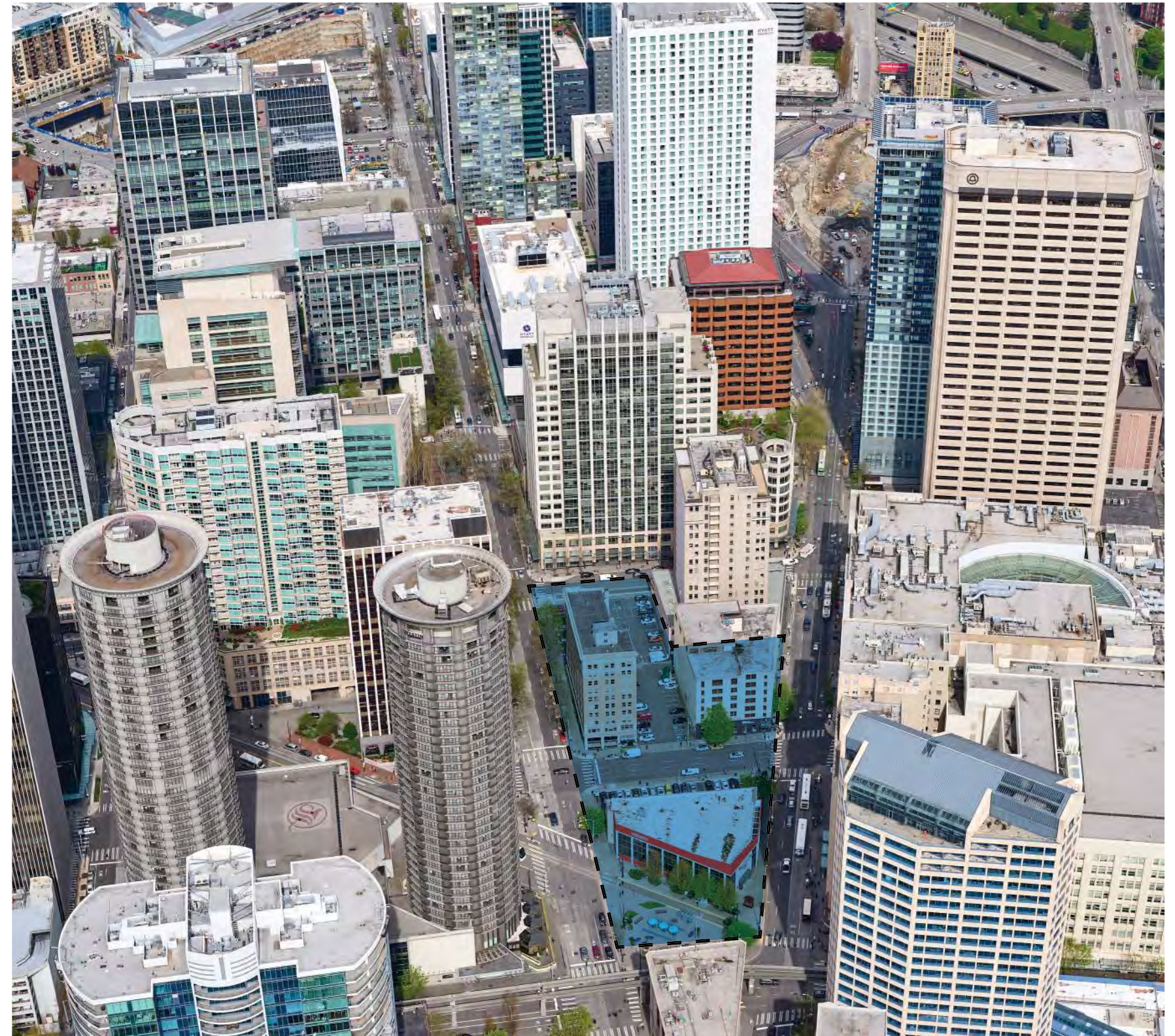
9455 Towne Centre Drive, San Diego (2020)

# PLANNED COMMUNITY DEVELOPMENT

## Objectives

This Planned Community Development consists of 4 parcels in the Downtown Office Core 2 zone: a new 420' mixed-use commercial office building with ground-floor retail and underground parking at 1815 6th Ave, a new 240' mixed-use commercial office building with ground-floor retail and underground parking and the rehabilitation of an existing ten-story historic structure with commercial office use and ground-floor retail at 1818 6th Ave, and a new 550' residential tower with underground parking at 621 Stewart St.

The purpose of the PCD is to allow for a more appropriate distribution of FAR across the site. By shifting FAR away from the historic structure, the Lloyd building will remain a prominent feature building within the development. The proposed FAR will not exceed the aggregate maximum FAR of the individual parcels. The project will be phased, with the 621 Stewart St residential tower happening in a future phase.



# PLANNED COMMUNITY DEVELOPMENT

## PCD Scope Summary

- TOTAL PCD SITE AREA: 103,778 SF
- TOTAL FAR COMMERCIAL OFFICE AREA WITHIN PCD: 891,735 SF
- TOTAL PROPOSED RETAIL WITHIN PCD: 10,800 SF
- TOTAL PROPOSED RESIDENTIAL AREA WITHIN PCD: 614,305
- ROW IMPROVEMENTS WITHIN THE PCD BOUNDARY AREA BOUND BY STEWART ST, 7TH AVE, OLIVE WAY, AND 5TH AVE.
- HISTORIC REHABILITATION OF LLOYD BUILDING

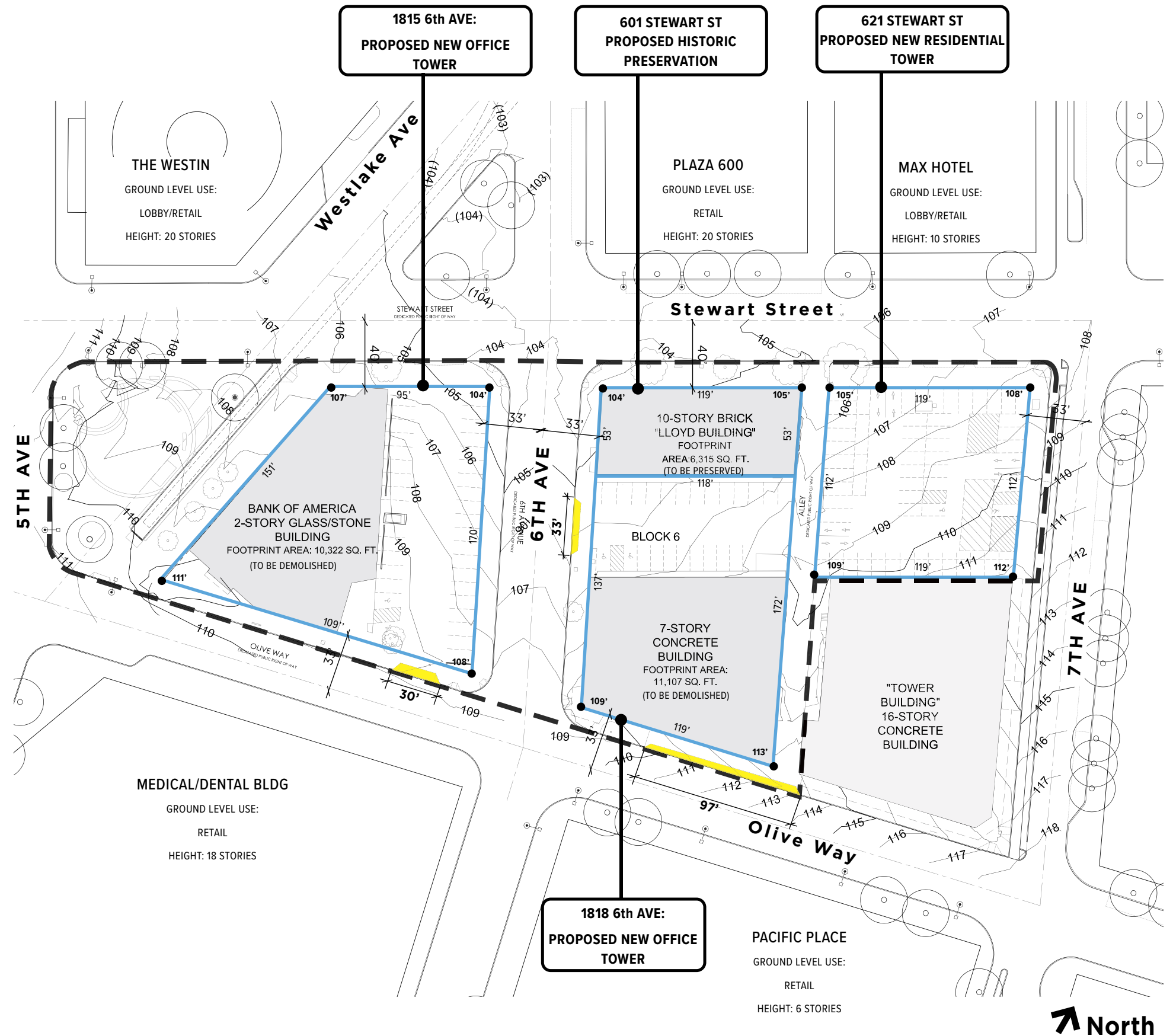
## PCD Public Benefits Summary

SDCI has published the following Public Benefit Priorities for this PCD development. The project team has been, and will continue to coordinate with SDOT, OPCD, Landmarks and SDCI on the proposed public benefit scope and will submit that scope for review within a separate PCD MUP.

- LOW INCOME HOUSING
- HISTORIC PRESERVATION
- PUBLIC OPEN SPACE
- IMPROVEMENTS TO PEDESTRIAN CIRCULATION
- IMPROVEMENTS TO URBAN FORM
- IMPROVEMENTS TO TRANSIT FACILITIES

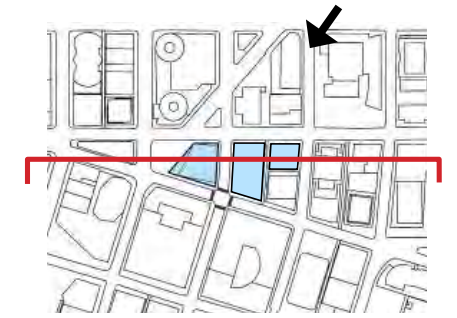
- EXISTING BUILDINGS
- PROPERTY LINES
- EXISTING CURB CUTS, 160' LF
- PCD BOUNDARY

TOTAL PCD AREA: 103,778 SF

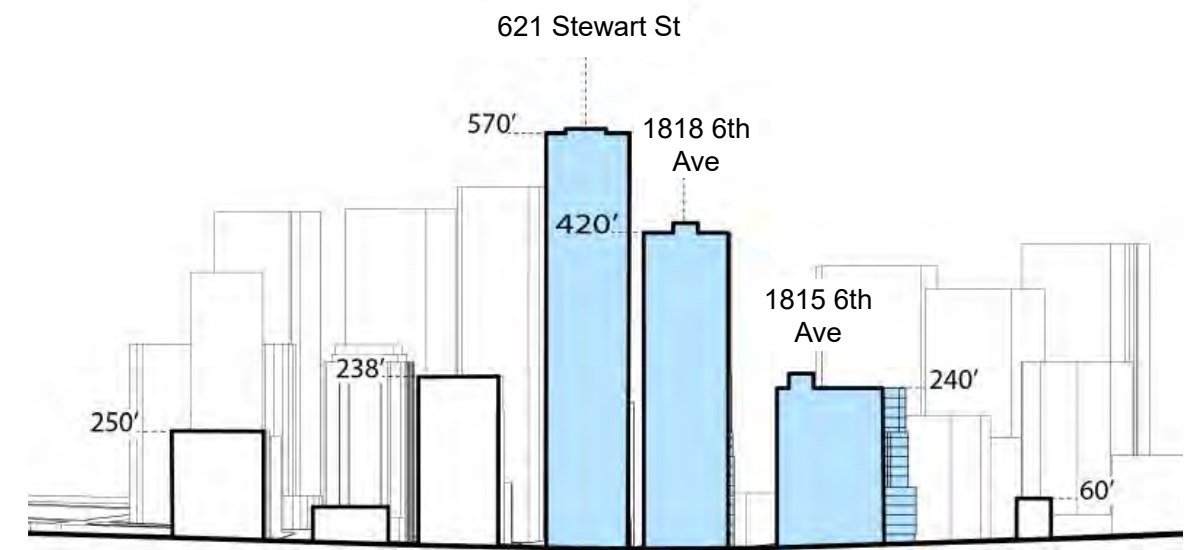
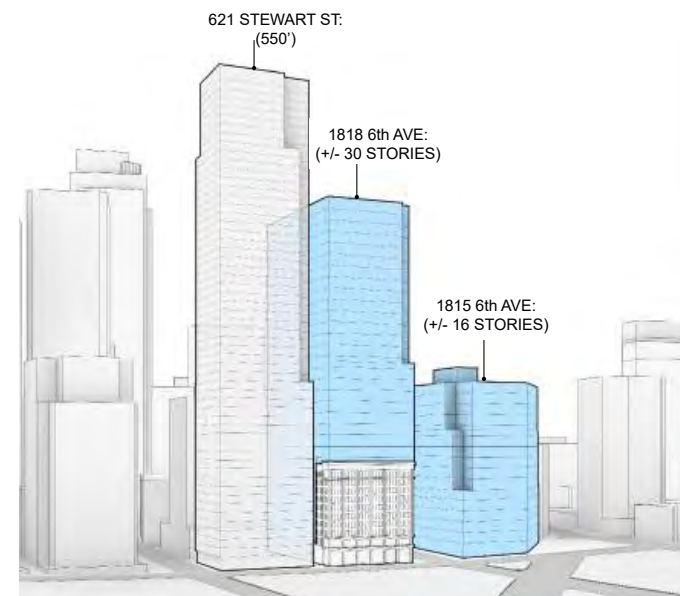


# PLANNED COMMUNITY DEVELOPMENT

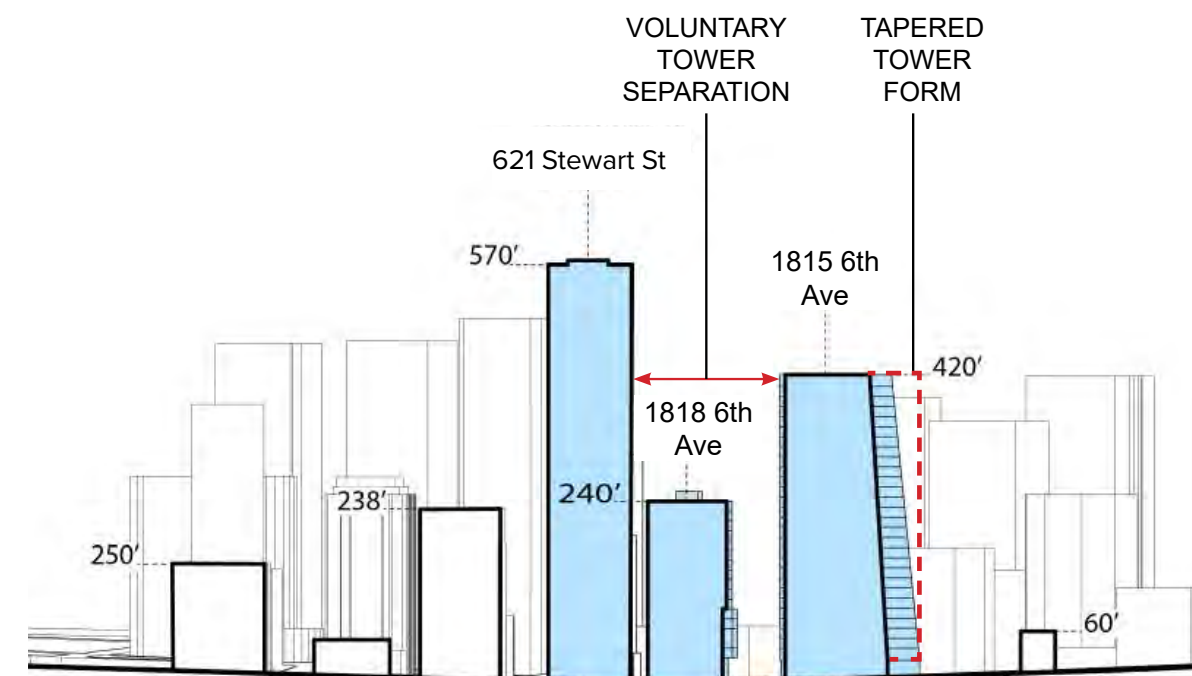
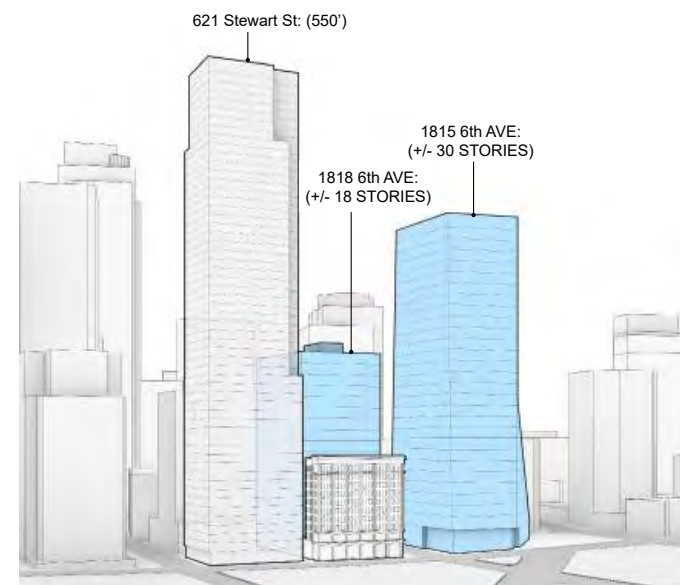
## Urban Form Analysis



**1 PRESCRIPTIVE MASSING - NO PCD**



**2 PROPOSED MASSING - WITH PCD**



[THIS PAGE LEFT INTENTIONALLY BLANK]

# INDEX

---

## CONTENT

01	DEVELOPMENT OBJECTIVES	11
02	SITE	17
03	URBAN DESIGN ANALYSIS	23
04	ZONING DATA	39
05	DESIGN GUIDELINES	43
06	ARCHITECTURAL MASSING CONCEPTS	41
07	DEPARTURES	79

[THIS PAGE LEFT INTENTIONALLY BLANK]

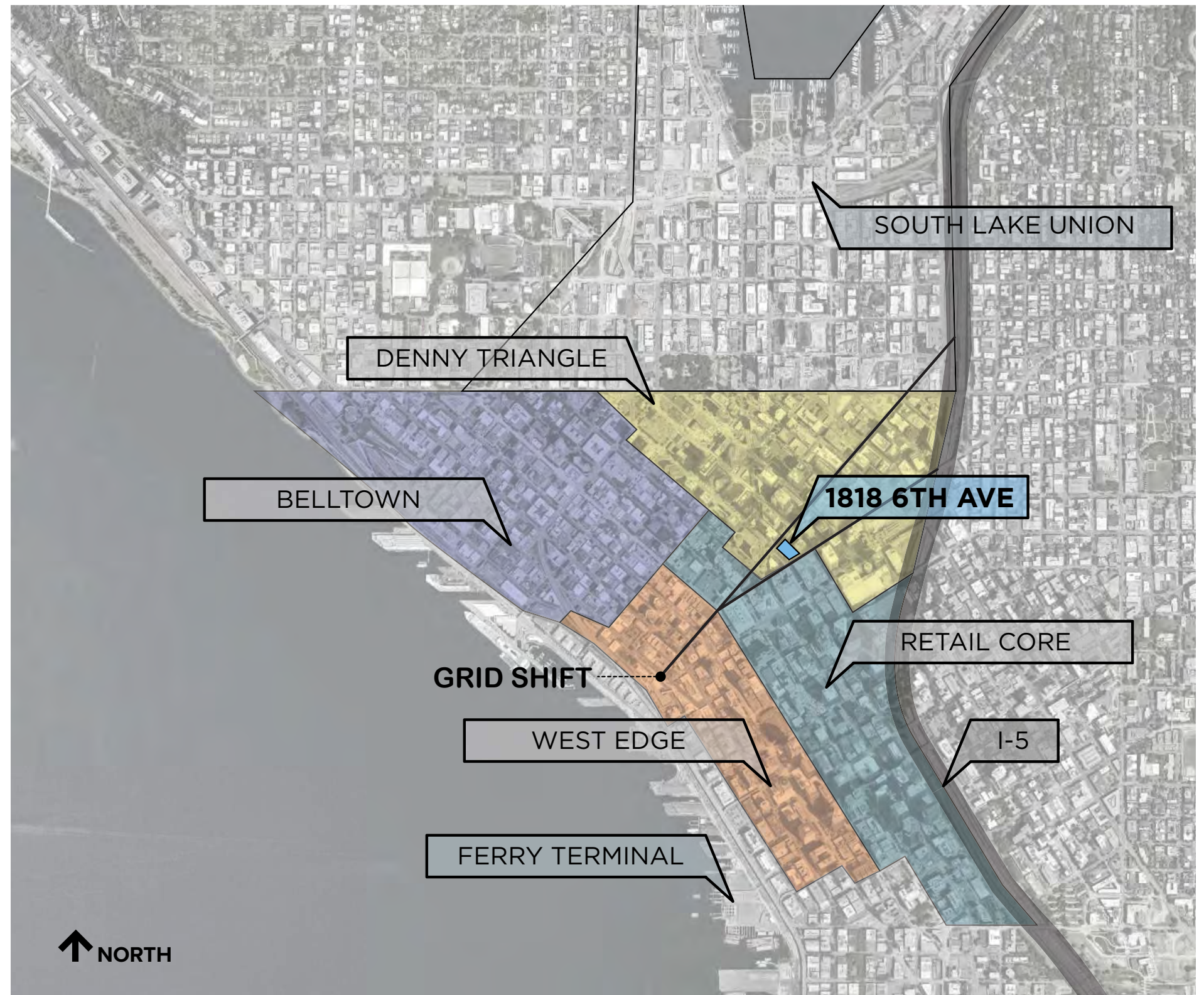
**01**  
**DEVELOPMENT OBJECTIVES**

# OI\_DEVELOPMENT OBJECTIVES

## 1818 6th AVE

This proposal combines three parcels to create a new office development in the Central Business District (within the Downtown Office Core 2 Zone). The project will renovate the landmark Lloyd Building (ca. 1926) and integrate it with the construction of a new mixed-use commercial office building. The proposed scope includes:

- Total area of combined parcels: 24,589 SF
- Renovation of the landmark Lloyd Building, a ten-story historic commercial office structure with ground floor retail along Stewart Street.
- Construction of a new 240' mixed-use commercial office building.
- Approximately 346,500 GFA of commercial office space.
- Four levels of below grade parking with approximately 157 parking stalls.
- Approximately 2,000–4,000 SF of ground floor retail space.



# 01\_SUMMARY OF PUBLIC OUTREACH



## JOIN US

Join Us for a Community Meeting to Provide Input on Projects Located at **500 Olive Way, 600 Olive Way, 601 Stewart St and 1825 7th Ave.**

These new projects are adjacent to each other downtown and are as follows: a new 500' mixed-use commercial office building with ground-floor retail and underground parking at 500 Olive Way, a new 270' mixed-use commercial office building with ground-floor retail and underground parking at 600 Olive Way, renovation of an existing ten-story commercial office structure with ground-floor retail at 601 Stewart St, and a new 550' residential tower with underground parking at 1825 7th Ave. The existing structures at 500 & 600 Olive Way will be demolished. These sites will be jointly developed. The project sites are zoned downtown.

**What:** Let us know what you think! Join the project team and their architects to discuss the vision and approach for this new project in the neighborhood. Coffee and cookies will be provided. All are welcome. No RSVP needed.

**Date:** Thursday, March 5, 2020  
**Time:** Event begins promptly at 6pm and will end around 7pm  
**Where:** 603 Stewart St, Suite 420, Seattle, WA 98101

**Date:** Friday, March 6, 2020  
**Time:** Lunchtime pop-up from 12pm to 1pm  
**Where:** Tower Building Surface Parking Lot, 7th & Stewart, Seattle, WA 98101

[www.RequiredOutreach-KilroyProjects.com](http://www.RequiredOutreach-KilroyProjects.com)

**THU  
MAR 5  
&  
FRI  
MAR 6**

**Project Address:**  
 500 Olive Way,  
 600 Olive Way,  
 601 Stewart St and  
 1825 7th Ave,  
 Seattle, WA, 98101  
**Contact:** Natalie Quick  
**Applicant:** KR Manager, LLC  
**Additional Project Information on Seattle Services Portal via the Project Address:**  
 500 Olive Way, 600 Olive Way,  
 601 Stewart St and 1825 7th Ave  
**Project Email & Website:**  
 KRmanager@earlyDROutreach.com  
 www.RequiredOutreach-KilroyProjects.com  
Note: Emails are returned within 1-2 business days. Emails are subject to City of Seattle public disclosure laws.



### PRINTED OUTREACH

- Choice: POSTERS, HIGH IMPACT
- Requirement: Posters hung in a minimum of 10 local businesses, community centers, or other publicly-accessible venues, located a half-mile from the proposed site. At least half must be visible from the sidewalk.
- What we did: Posters were hung in 13 locations according to and exceeding requirements. Poster, spreadsheet with locations, and photos included in Appendix A.
- Date completed: February 20, 2020

### ELECTRONIC/DIGITAL OUTREACH

- Choice: PROJECT WEBSITE, HIGH IMPACT
- Requirement: Interactive project website (with public commenting function)
- What we did: Project website established. Publicized website via poster. Checked daily for comments from website. Website included in Appendix A.
- Additional Equity Requirement: An intercept survey was also included online.
- Date completed: February 20, 2020

### IN-PERSON OUTREACH

- Choice: COMMUNITY MEETING, HIGH IMPACT
- Requirement: Host or co-host a community meeting (at least one hour of presentation/discussion of project).
- What we did: Held a Community Meeting event, open to the public, publicized through posters and DON calendar. Event photos, agenda, sign-in sheets, and comments included in Appendix A.
- Additional Equity Requirement: We also held a pop-up event on the street near the project site.
- Dates completed: March 5-6, 2020

# OI\_SUMMARY OF AGENCY OUTREACH

## SDOT

**March 27, 2020**

### McGraw Square Improvements

- SDOT presented plans for McGraw Sq Lighting Improvements through One Center City plan.
- SDOT goal for McGraw Sq: to create a distinct identity separate from other public open spaces in the City.
- Since timeline for the streetcar re-alignment decision is unknown, SDOT agreed that the Vance project should plan for the existing streetcar condition (assuming it is not moved or changed in time for Vance construction)
- Opportunities for Vance project to contribute:
  - Activating building edge along McGraw Sq w/ retail & active, transparent program
  - Respond to the existing circle concept with project improvements.
  - Improvements between building edge and existing streetcar stop
  - Opportunity for removing or replacing existing trees.

**March 30, 2020**

### Bike Infrastructure Improvements

- SDOT gave direction that no curb bulb is desired on Stewart on either 600 Olive Way or 7, this lane should be preserved as a travel lane in the near term with the flexibility to become a PBL in the future.
- Pedestrian realm should be prioritized over bikes on 6th Ave. No separate bike lane is needed on 6th
- No opportunities for bike infrastructure improvements as part of this project's scope

## SDOT - STREETCAR

**February 4, 2020**

### Meeting

- Project Introduction to SDOT & Streetcar
- Discussed Streetcar Expansion Plan to abandon stop in McGraw Sq.
- Desire to locate substation box below grade (out of Westlake Sq)
- All noted that there is great opportunity to work together on ROW improvements and a commitment to do so by all parties

**March 5, 2020**

### Project Introduction

- SDOT presented current plan for Streetcar, including plans for new streetcar stop in Westlake Sq.
- SDOT requested time to meet internally to discuss potential public benefits related to streetcar for project's PCD public benefits package

**March 17, 2020**

### Streetcar Plans

- Timing of streetcar realignment project unknown
- Alternate options being considered by SDOT for streetcar stop location, including potential for stop to remain in McGraw Sq
- Opportunities for project to enhance McGraw Sq as mobility hub
- Alternate location for streetcar substation desired to improve pedestrian realm at Westlake Sq

## LANDMARKS

**October 1, 2019**

### Informal Introduction

- Prior to Acquisition an introduction to Kilroy Realty, plus fact finding inquiry to connect with City of Seattle - Historic Preservation.

**February 4, 2020**

### Informal Meeting with Director & SDCI

- Introduction Meeting to discuss the projects being planned and review the process steps associated with the Planned Community Development, the historic improvements at the Lloyd Building and the multiple MUP's.

**April 29, 2020**

### 600 Olive Way Landmark's Introduction Package Overview Summary

- Lloyd Building, Landmarks Preservation Board, Information Packet

**May 14, 2021**

### Historic Lloyd Building Landmark's Introduction Meeting

- Presentation to the Landmarks Presentation Board of the Lloyd Building Introduction Meeting Packet

**Nov 12, 2021**

### Historic Lloyd Building Landmark's ARC Meeting

- Presentation to the Landmarks Presentation Board of the Lloyd Building & New Tower Massing Options

## DESIGN RESPONSE TO INPUT RECEIVED:

### ROW Improvements

- No new bicycle lanes are currently planned as part of the project's ROW improvements
- Sidewalks will be widened to improve pedestrian realm
- Curb bulbs are under study along 6th Avenue to improve pedestrian crossings

### Landmarks

- Enthusiasm over the dynamic connection between the new office tower floor plates and historic Lloyd building has influenced the third, preferred massing scheme.

# 01\_SUMMARY OF LANDMARKS ARC FEEDBACK

## Introduction Meeting 01

May 14, 2021

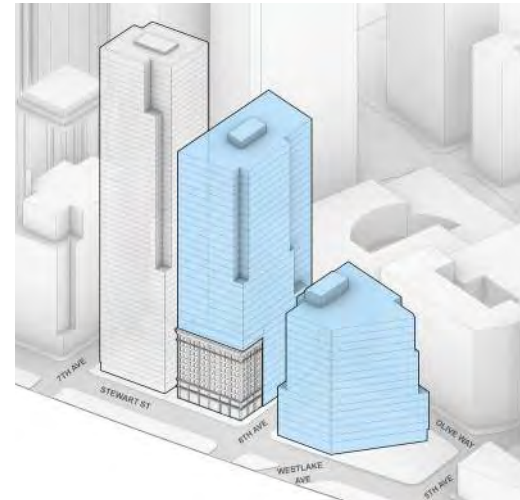
### LANDMARKS ARC FEEDBACK:

- "10 out of 10!"
- PCD Massing and transfer of FAR is preferred
- "Gasket" separation between historic building and new tower is appreciated
- Very supportive of preserving Lloyd superstructure
- Support of a new shared lobby between the historic Lloyd building and the new office tower
- Interest and support over the approach to connecting different floor heights between old and new
- Willing to endorse preferred massing to SDCI

## MASSING STUDIES, 600 OLIVE

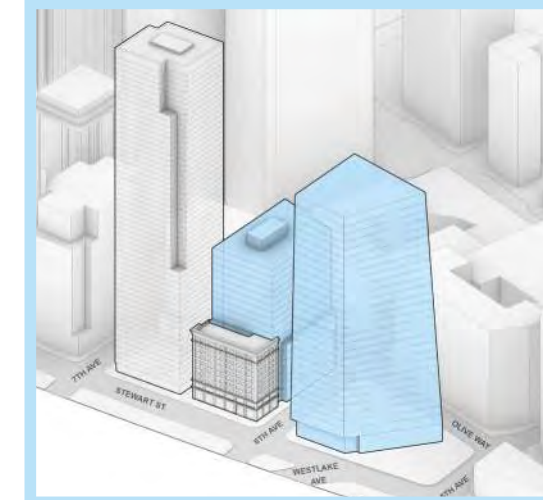
FAR Distribution

### ALLOWABLE MASSING, NO PCD



PRESCRIPTIVE MASSING, NO PCD

### FAR TRANSFER, WITH PCD



PREFERRED MASSING, PCD

## Meeting 02

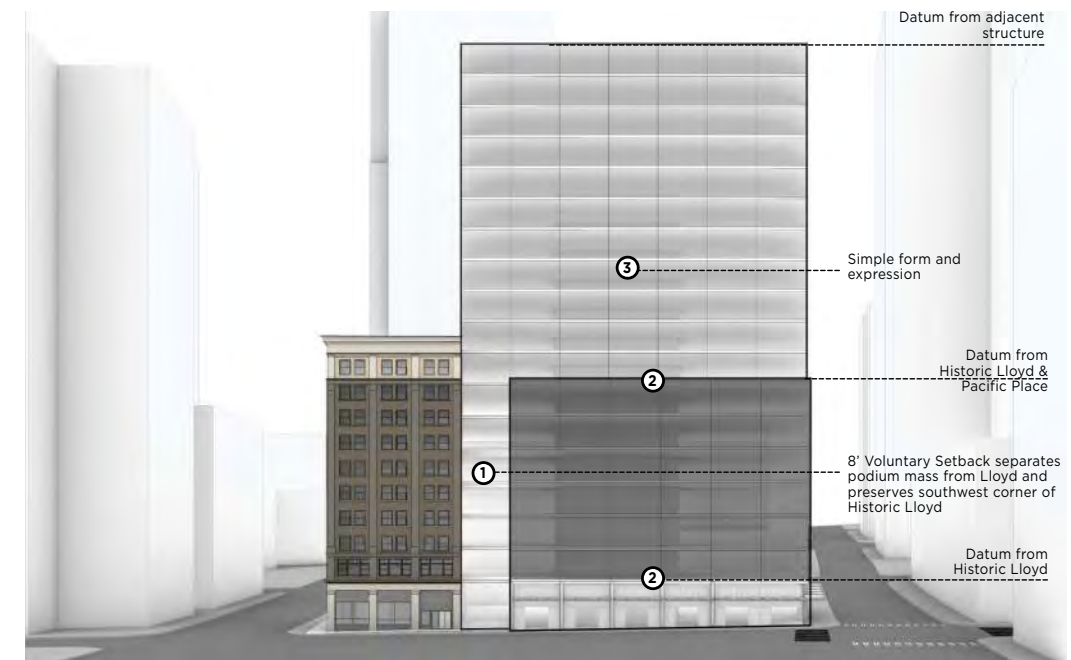
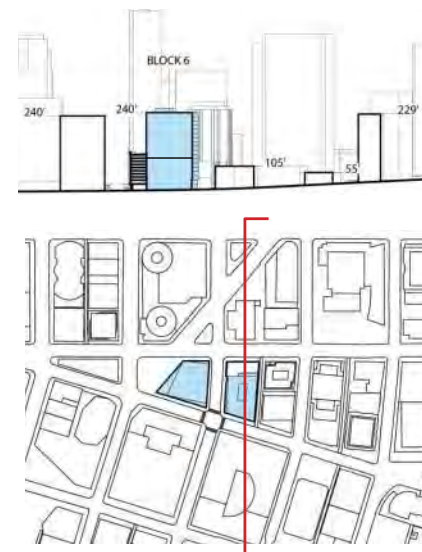
Nov 12, 2021

### LANDMARKS ARC FEEDBACK:

- "The gasket is a great move, nice separation between new and historic"
- Appreciates that the plan for loading dock is not disrupting the Lloyd building structure or frontage
- Would like to see further development of the ground plane to ensure there is variation across the street frontage
- Composition and general scale of the proposed new office tower is respectful to Lloyd
- Appreciates the thoughtful facade analysis of the Lloyd Building and is looking forward to seeing how that informs the new office tower expression

In Preparation for Upcoming Early Design Guidance

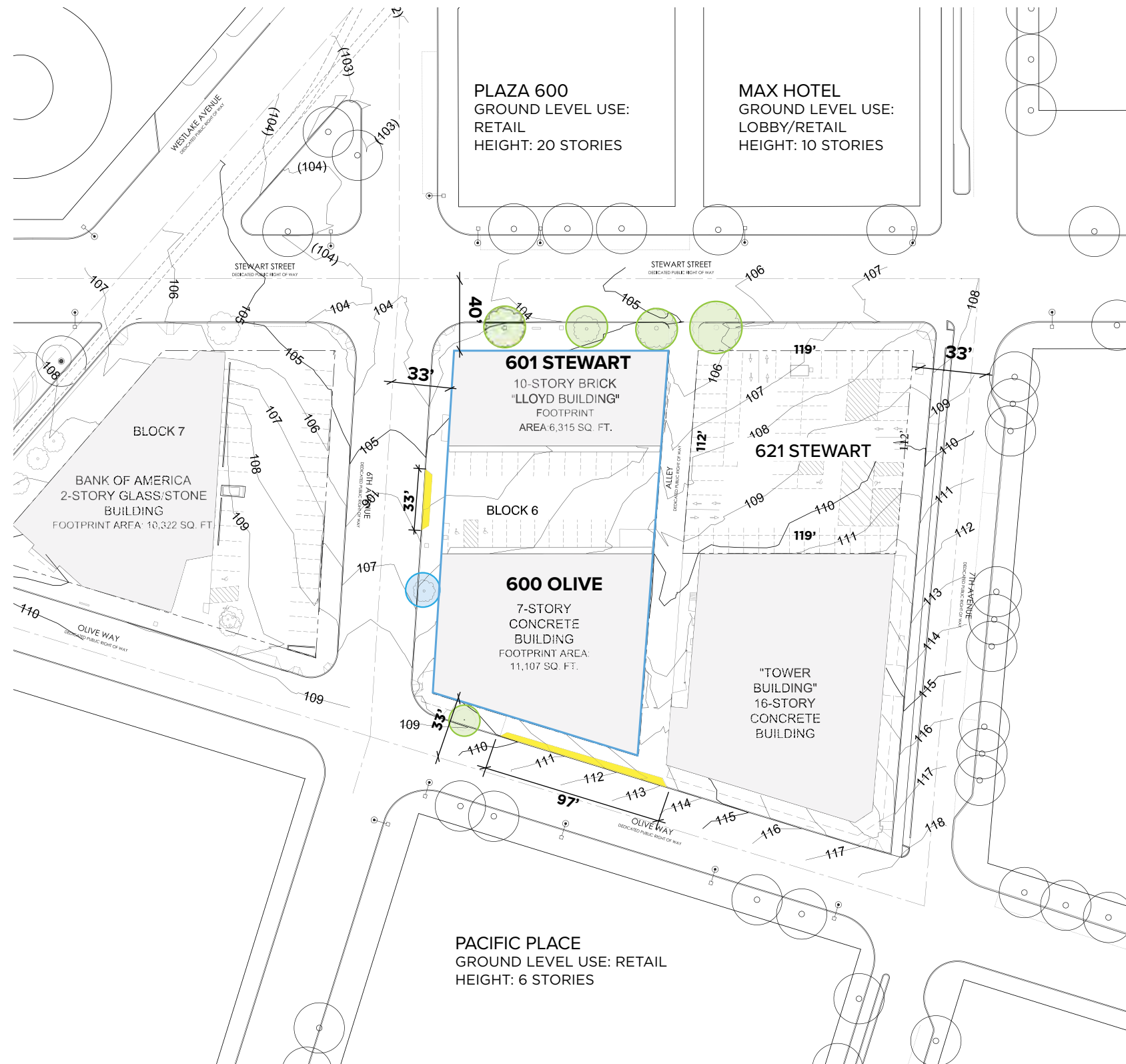
Page 26



[THIS PAGE LEFT INTENTIONALLY BLANK]

**02**  
**SITE**

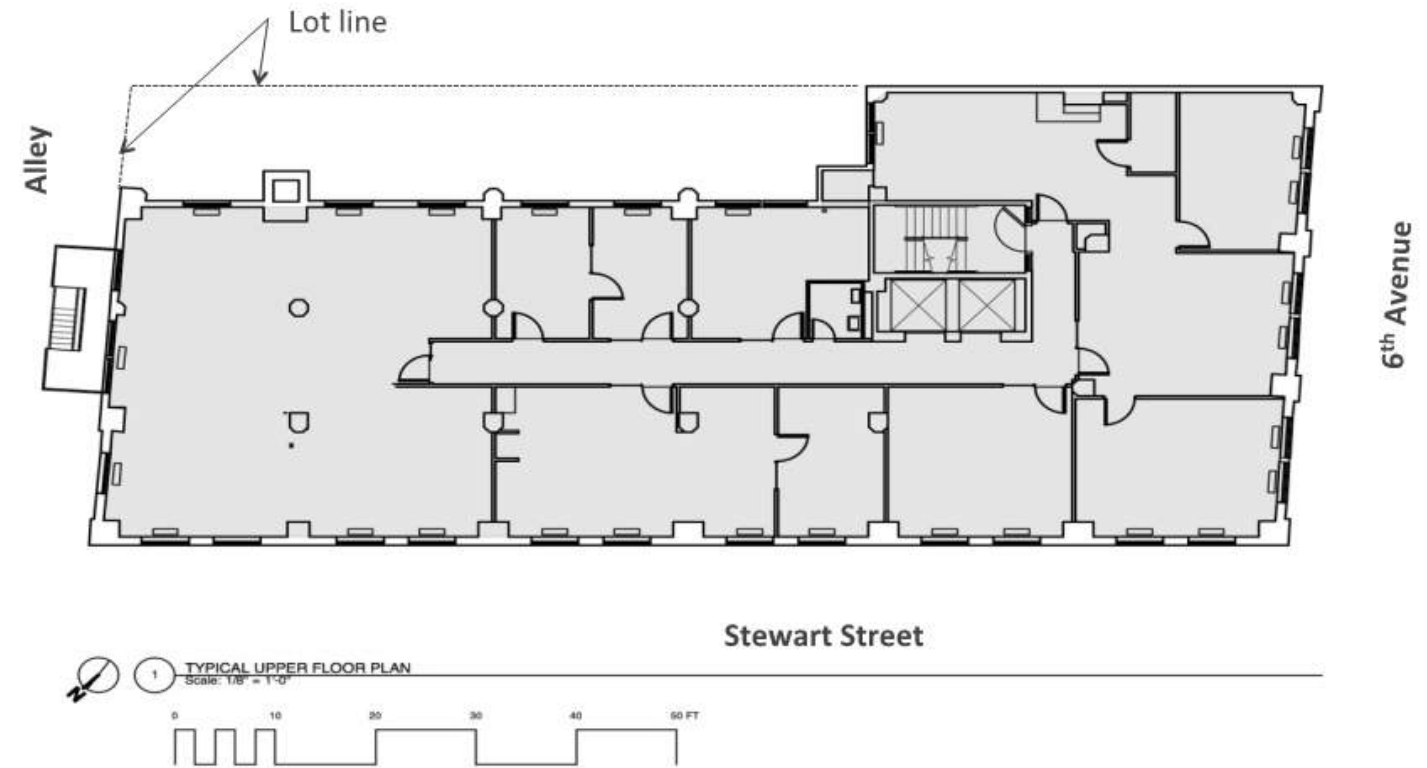
# O2\_EXISTING SITE PLAN



- Existing Buildings
- Property Line
- Existing Curb Cuts, 130 LF
- Existing trees to remain
- Existing trees to be removed

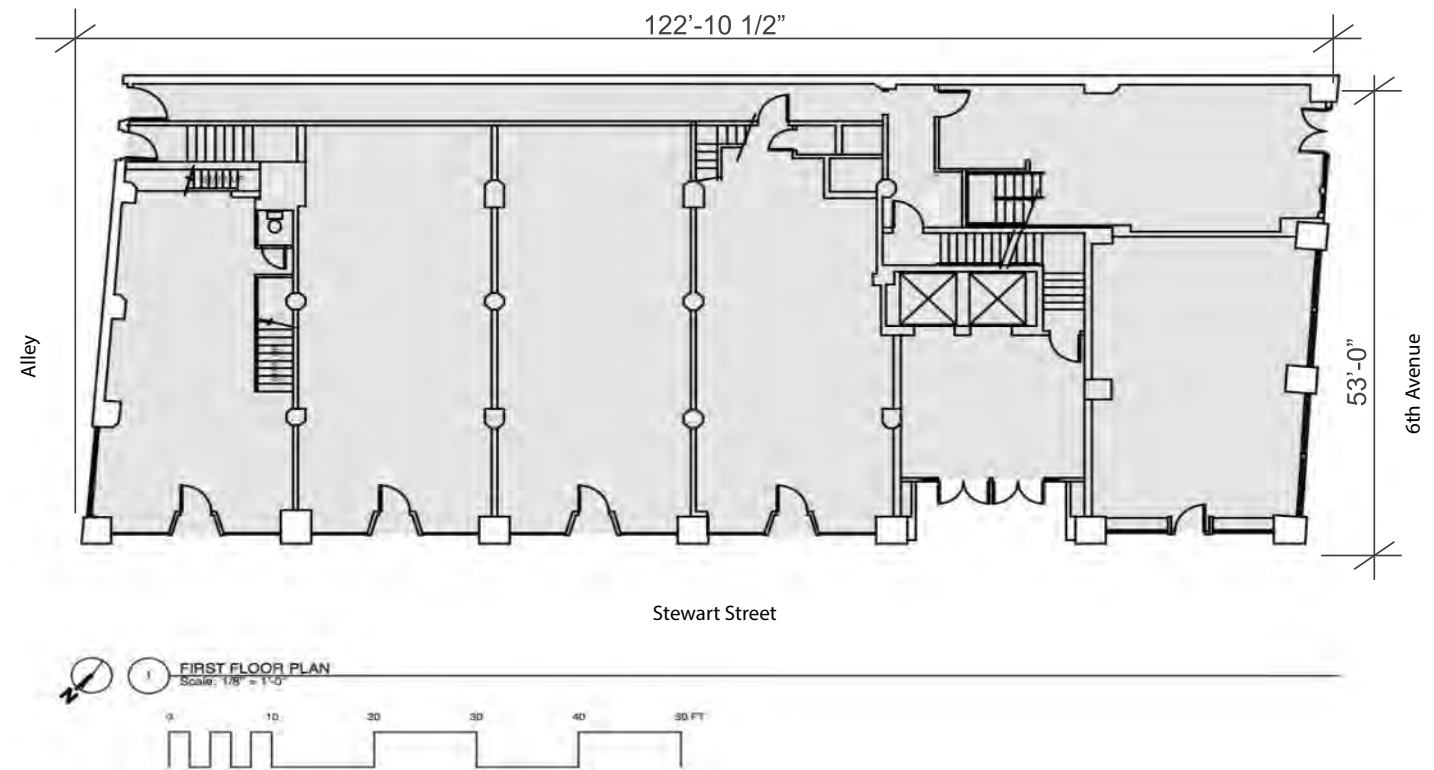
## 02\_601 STEWART (LLOYD) EXISTING CONDITIONS

- Existing floor to floor height of 10' is well below market standard of 13'.
- Lloyd structure is seismically deficient and requires a substantial alteration to bring it up to current structural code, requiring a complete structural core replacement and additional lateral support system
- Elevators do not meet current industry standards for service speed and load capacity and will need to be replaced
- Life and Safety - existing fire alarm system is outdated and requires a complete replacement to meet current code.
- Fire protection - the Lloyd building does not have a sprinkler system, a complete new fire water system and fire water storage system will need to be added.
- The restrooms and drinking fountains are not accessible and will need be updated to comply with current codes
- Restroom count is not sufficient to meet current codes, restrooms will be added
- Mechanical, electrical and lighting are outdated and will require complete replacement to meet current codes.
- The existing windows and envelope do not meet current energy code.



Existing Typical Upper Floor Plan

NORTH



Existing Ground Floor Plan

NORTH

# 02\_601 STEWART (LLOYD) EXISTING CONDITIONS

## SUMMARY OF MODIFICATIONS TO DATE:

- 52% of wood windows replaced with vinyl and e-glass windows
- Black spandrel replaced with black plexiglass
- Wood transom beam replaced with aluminum

## NOTE:

KRC is working with Legacy Renovation Products & Services, Inc. to perform a Lloyd Building - Exterior Window Assessment to identify the recommended corrective work to perform at the historically significant facades. These recommendations will be presented to and reviewed by the ARC Landmarks Committee prior to execution.



Original Design

NORTH ELEVATION



Original Design

EAST



WEST

Highlighted yellow zones indicate areas in which the historic facade has been altered through past building interventions and updates



Existing Configuration

NORTH ELEVATION



Existing Configuration

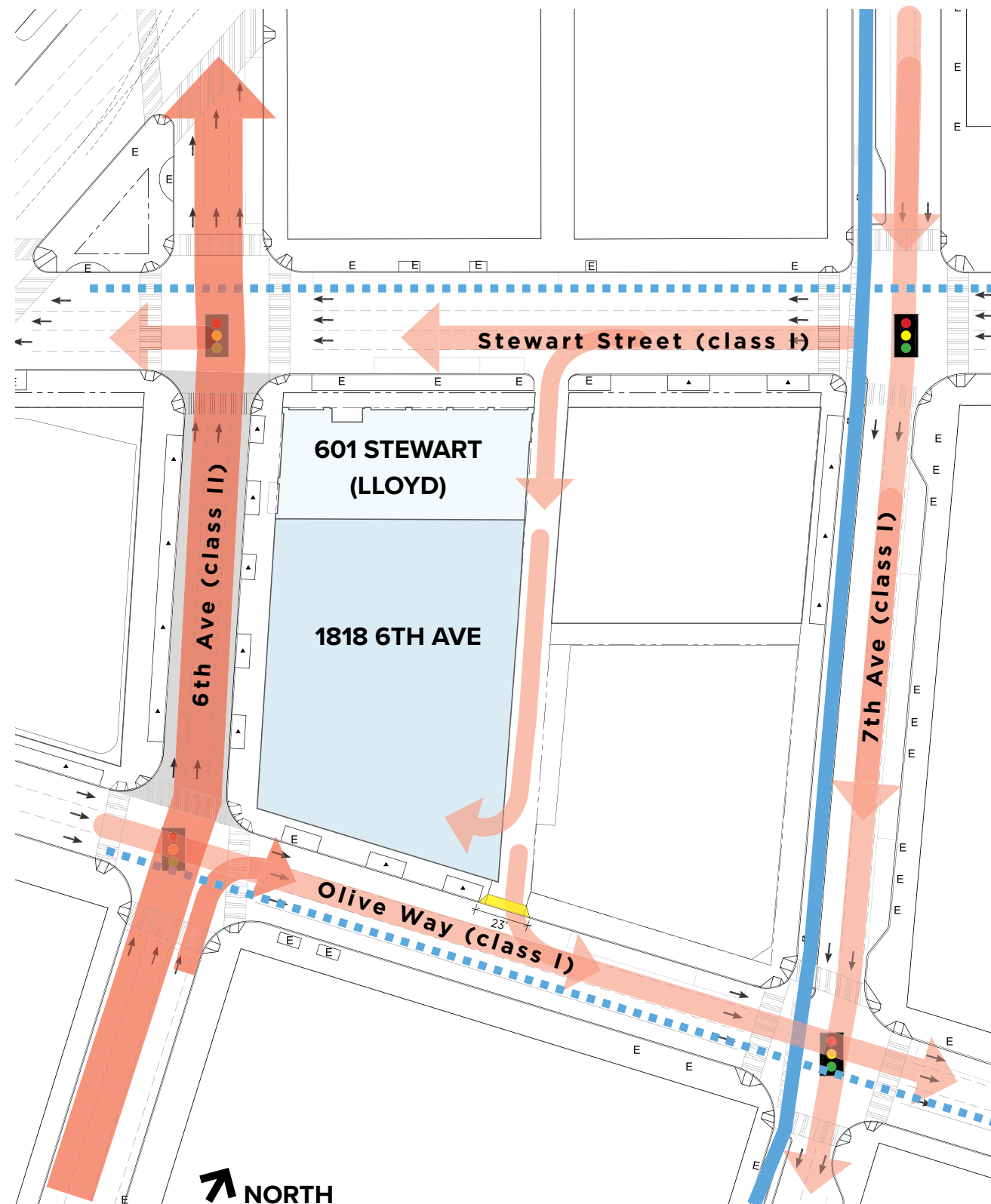
EAST



WEST

**BLACK SPANDREL**  
508 SF 74% Black C  
175 SF 26% Black F

# O2\_PROPOSED SITE ACCESS



## STEWART STREET - 66' ROW

- Downtown, Principal Arterial Street
- Primary entry to downtown - one-way west
- Future streetcar extension to 1st Ave
- Future bike facilities connecting to 4th and 2nd Ave

## 6TH AVENUE - 80' ROW

- Downtown, Principal Arterial Street
- Primary exit from downtown - one-way north
- No bus routes
- Potential future ST3 light rail station (Westlake)

## OLIVE WAY - 74' ROW

- Downtown, Principal Arterial Street
- Primary exit from downtown - one-way east
- Dominated by major bus routes
- Minimal parking

- PRINCIPAL ARTERIALS
- MINOR ARTERIALS
- PROPOSED CURB CUTS
- PROTECTED BIKE LANE
- - - SHARROW
- PROPOSED CHANNELIZATION

[THIS PAGE LEFT INTENTIONALLY BLANK]

**03**  
**URBAN DESIGN ANALYSIS**

# 03\_ZONING & NEIGHBORHOOD MAP

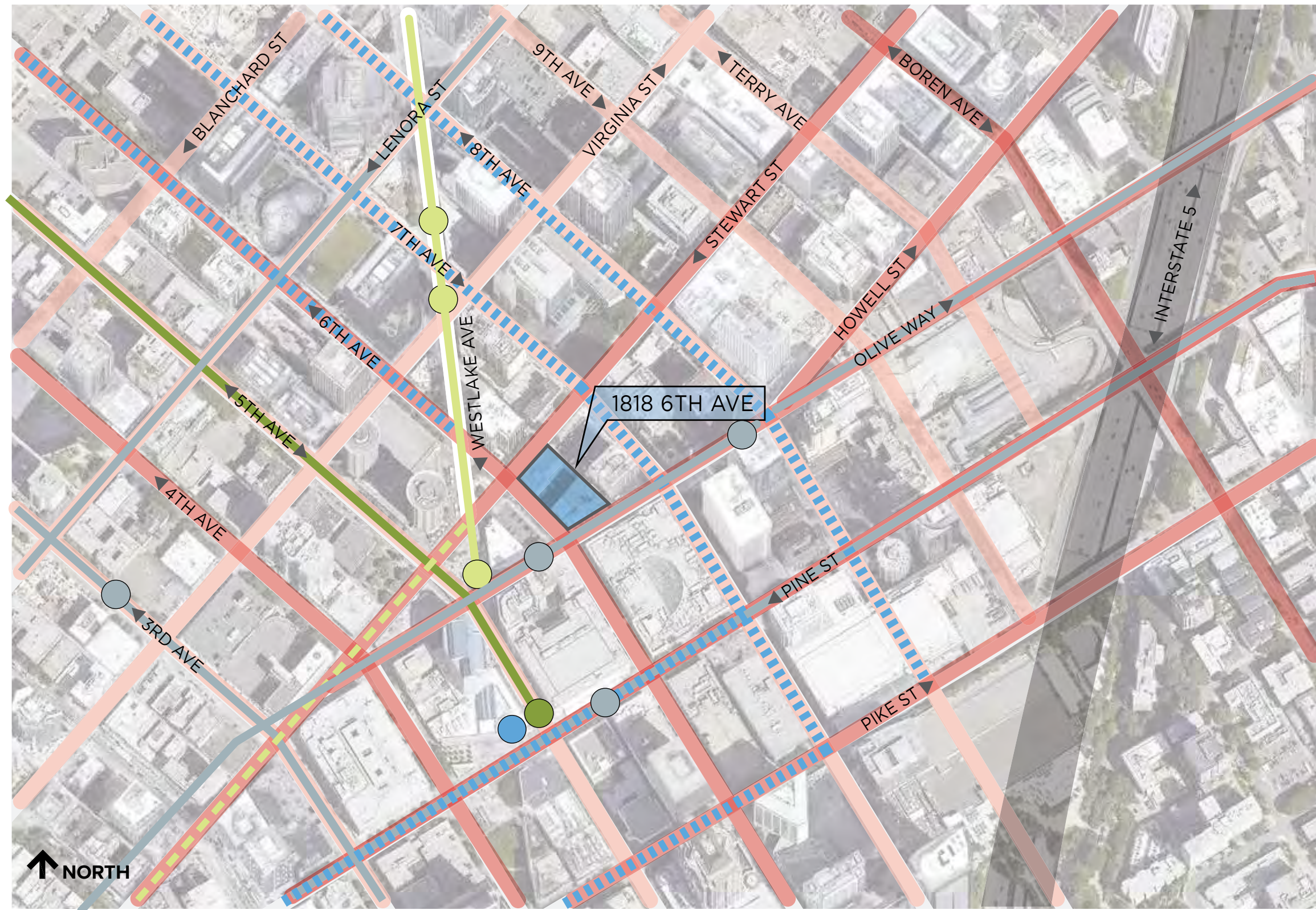


# 03\_STREET CLASSIFICATION & OPEN SPACE MAP



- GREEN STREET
- CLASS I PEDESTRIAN
- CLASS II PEDESTRIAN
- COMMUNITY OPEN SPACE
- PRIVATE OPEN SPACE

# 03\_TRAFFIC & TRANSPORTATION MAP



- PRINCIPAL ARTERIALS
- MINOR ARTERIALS
- SLU STREET CAR
- BUS ROUTES
- MONORAIL
- LIGHT RAIL STATION
- PROTECTED BIKE LANE
- FUTURE STREET CAR EXPANSION

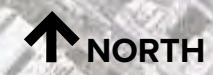


# 03\_VICINITY MAP: USES



- MULTI-FAMILY/MIXED USE RESI
- COMMERCIAL/RETAIL/OFFICE
- INSTITUTION/EDUCATION
- CIVIC/RELIGION
- RECREATION/OPEN SPACE
- MOTEL/HOTEL
- FUTURE DEVELOPMENTS
- 9 BLOCK AREA SURROUNDING SITE

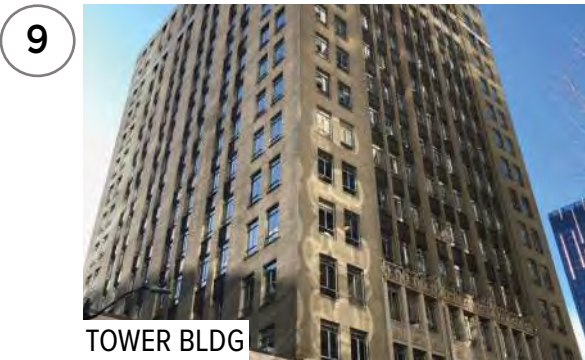
- 1 Plaza 600 Building
- 2 Amazon Towers
- 3 1903 5th Ave - Future 54-story Hotel/Resi
- 4 The Westin Seattle
- 5 Max Hotel
- 6 US District Court
- 7 Times Bldg
- 8 John Harte McGraw Statue
- 9 Tower Bldg
- 10 8th+Olive Tower
- 11 Hyatt Regency Seattle
- 12 Medical Dental Bldg
- 13 Pacific Place Mall
- 14 Hyatt @ Olive 8
- 15 802 Pine St - Future 55-story Apt Bldg
- 16 Future Convention Center
- 17 Westlake Park
- 18 1520 5th Ave - Future 17-story Apt Bldg
- 19 Hotel Theodore Hotel
- 20 Washington State Convention Center



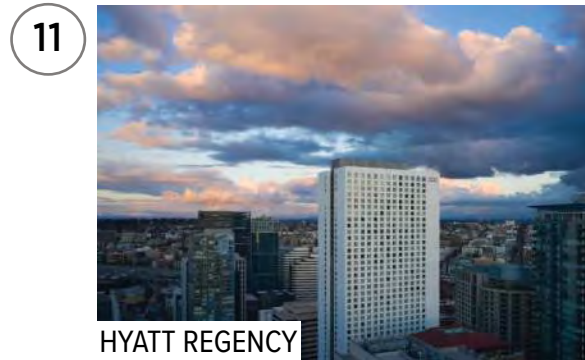
EXISTING BUILDINGS



EXISTING BUILDINGS



NEWER BUILDINGS



FUTURE BUILDINGS



# 03\_VICINITY MAP: LANDMARKS



1. Vance Hotel [Hotel Max]



2. McGraw Square



3. Times Square Building



4. Medical Dental Building



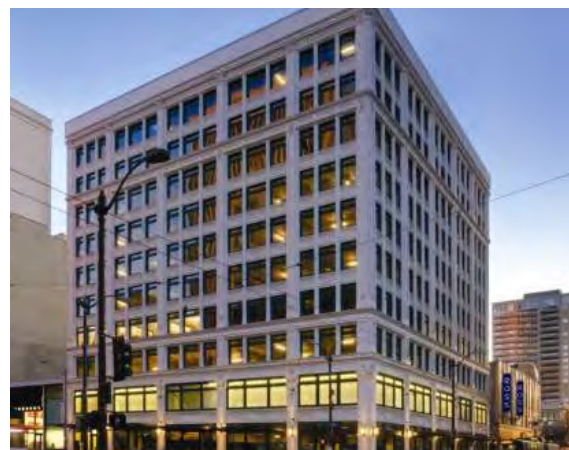
5. Fredrick and Nelson Building [Nordstrom Building]



6. Bon Marche [Macy's Building]



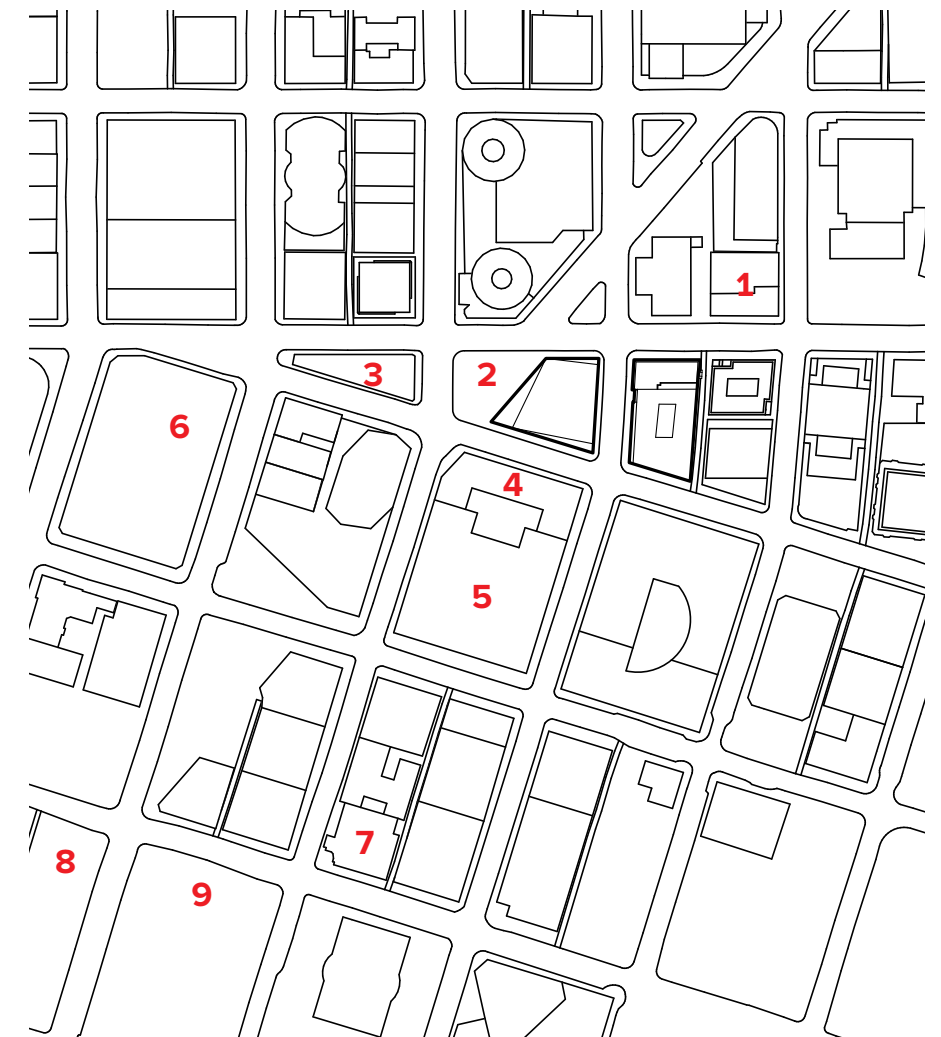
7. Coliseum Theater Building



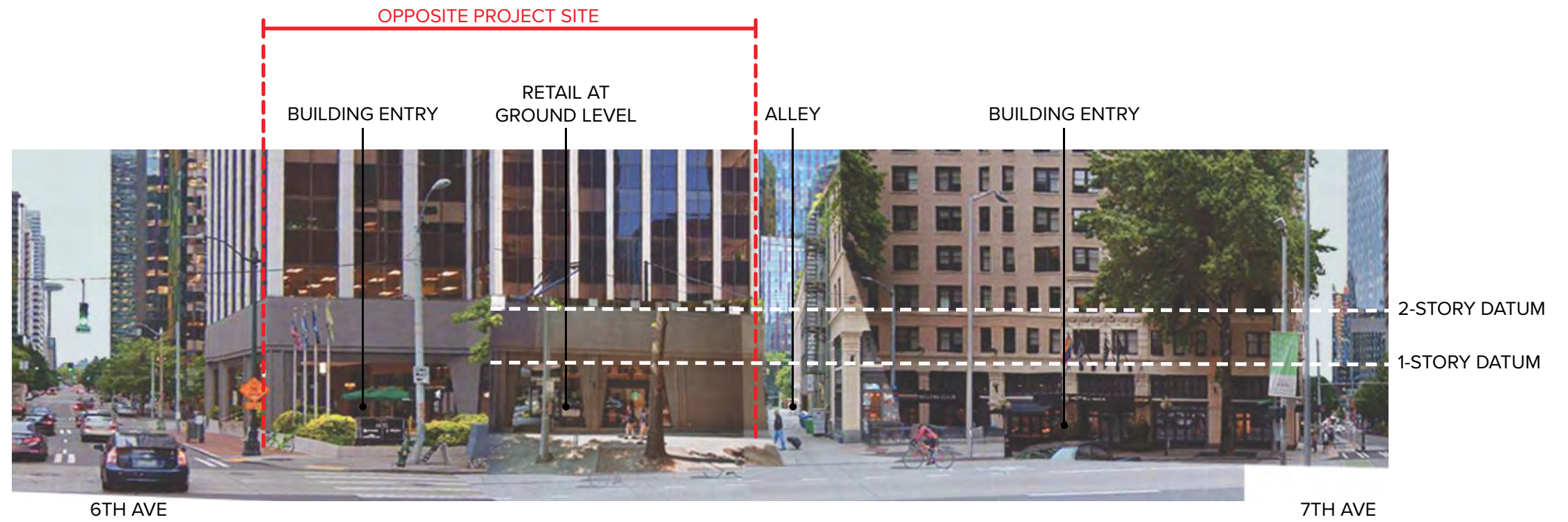
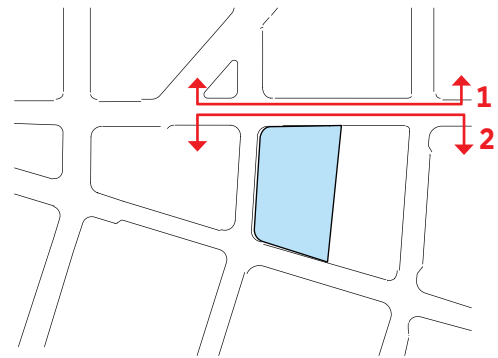
8. Joshua Green Building



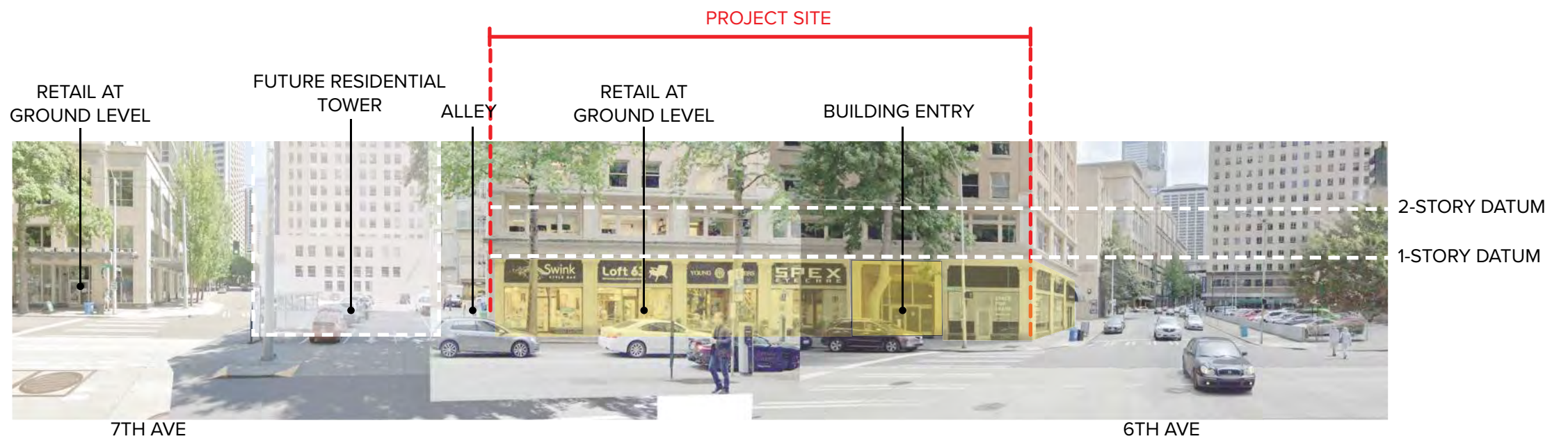
9. Liggett Building [4th & Pike Building]



# 03\_STREETSCAPES

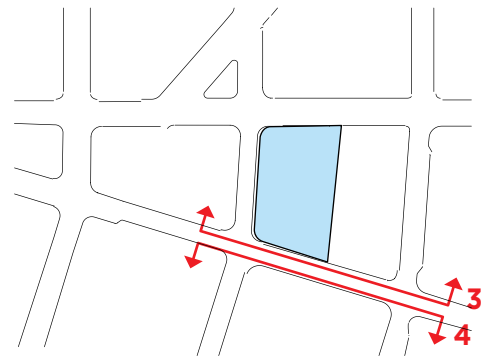


1. LOOKING NORTH ALONG STEWART ST

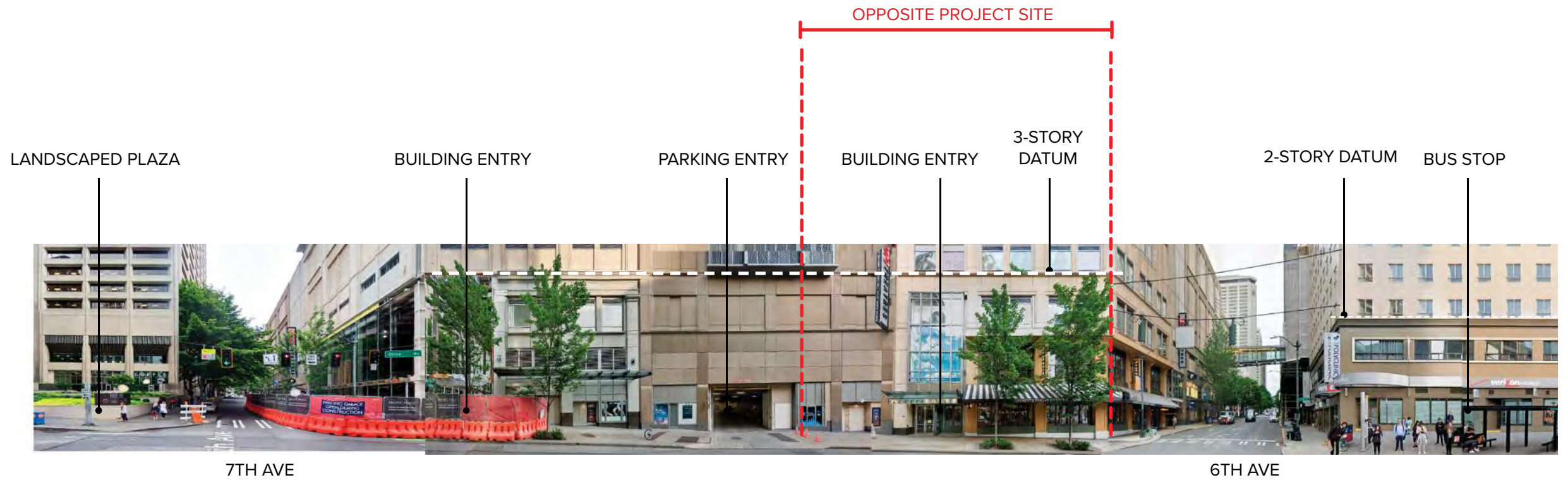


2. LOOKING SOUTH ALONG STEWART ST

# 03\_STREETSCAPES

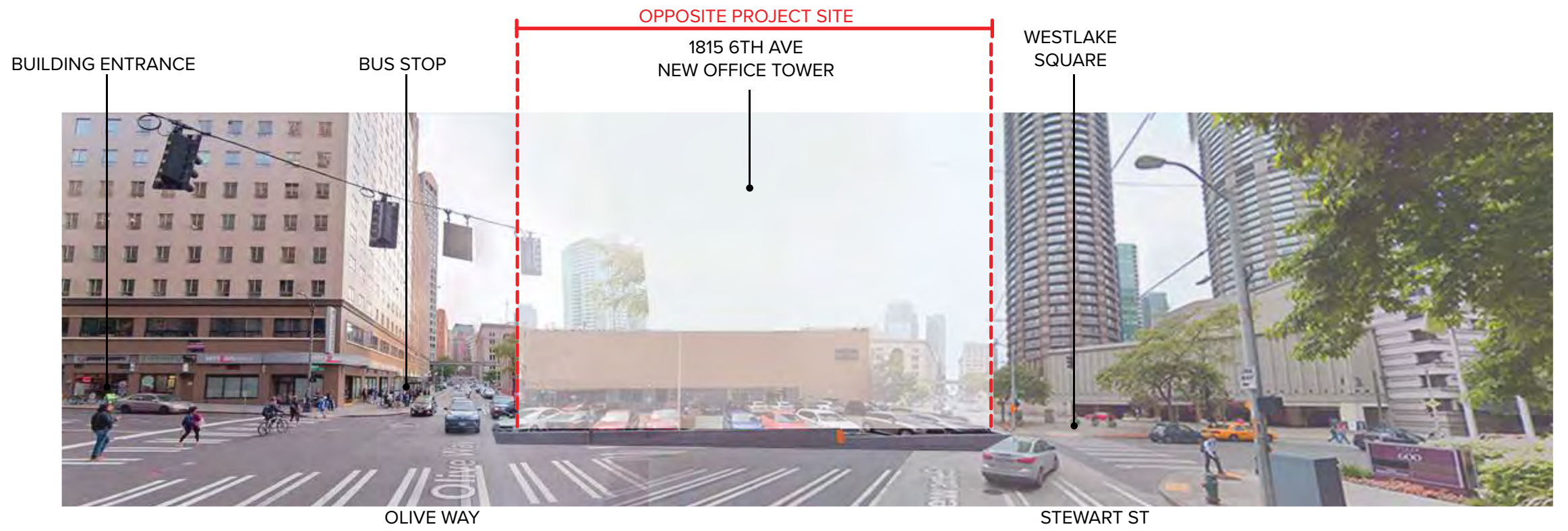
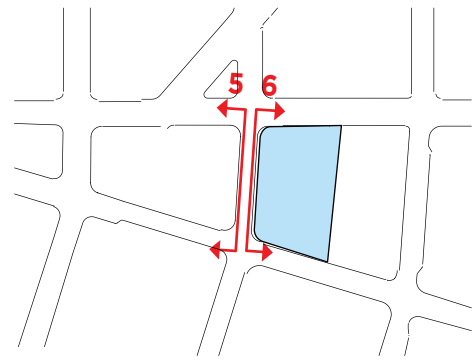


3. LOOKING NORTH ALONG OLIVE WAY

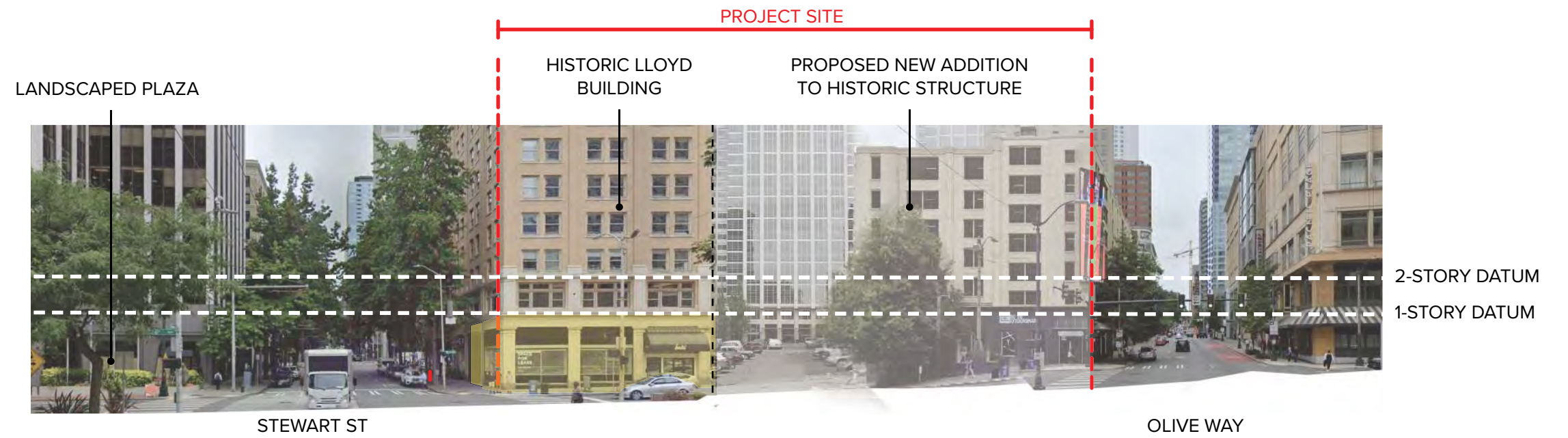


4. LOOKING SOUTH ALONG OLIVE WAY

# 03\_STREETSCAPES

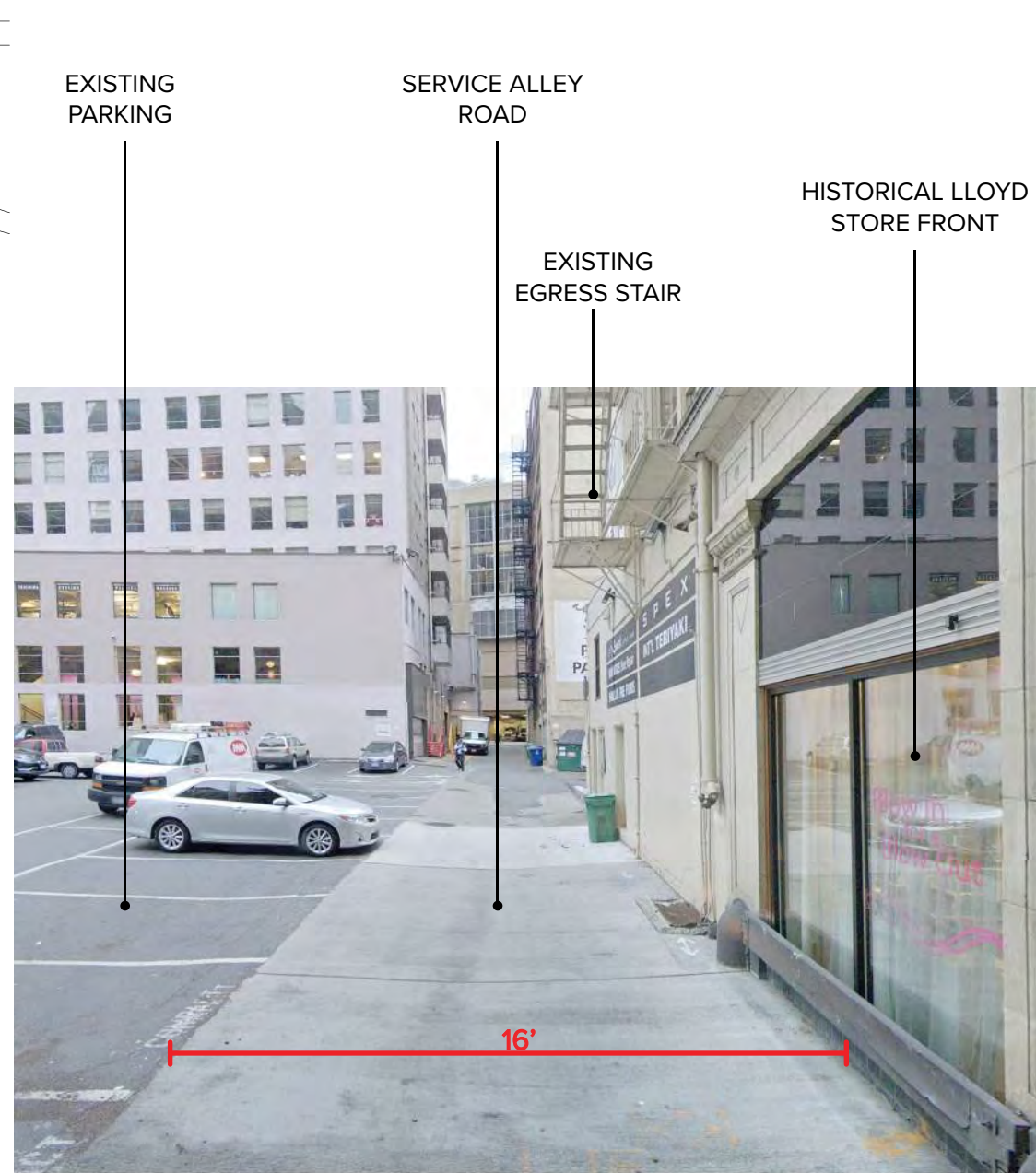
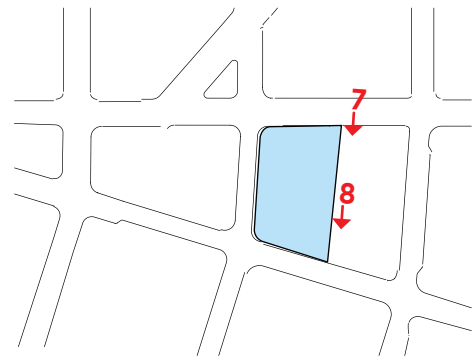


5. LOOKING WEST ALONG 6TH AVENUE

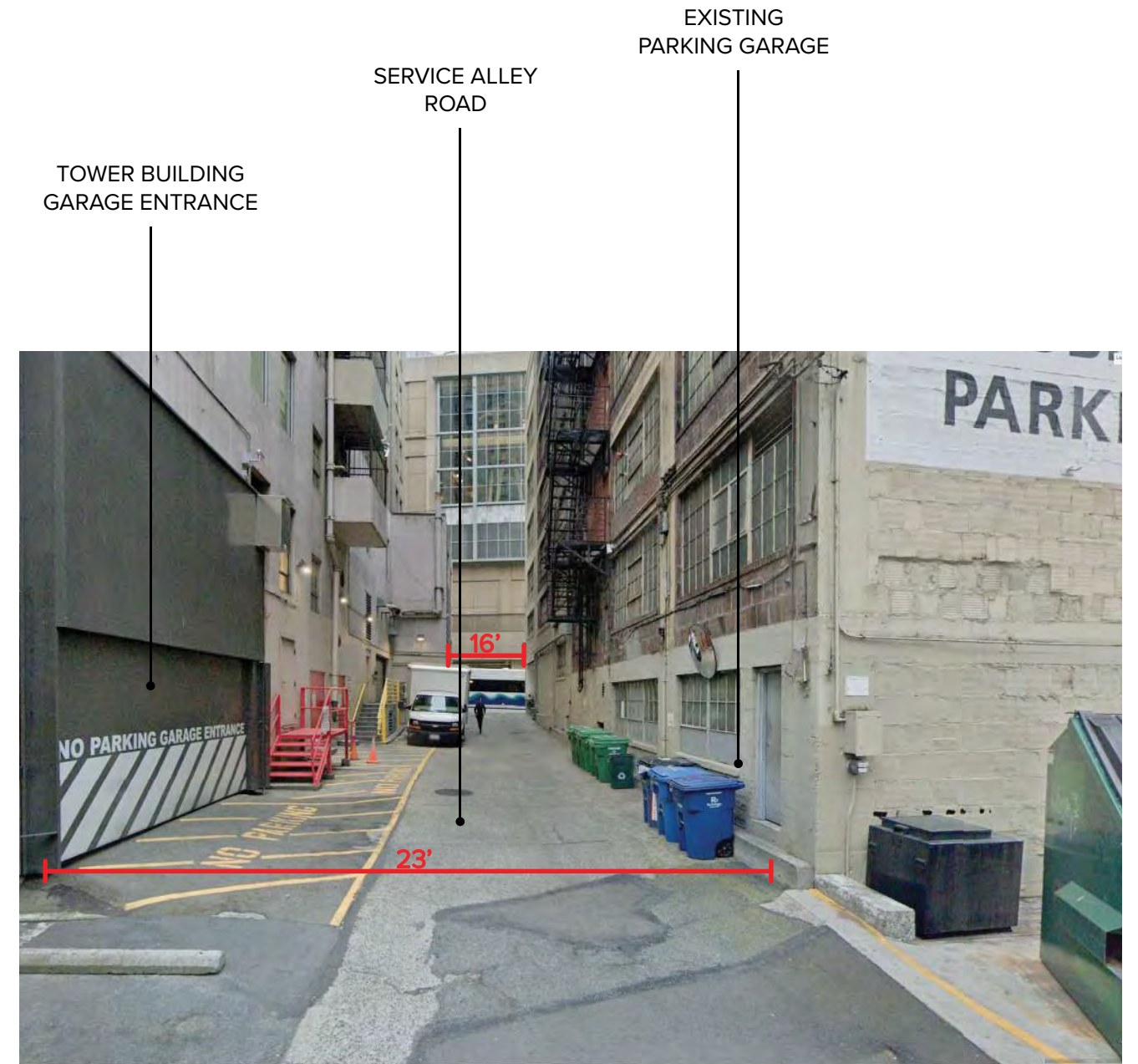


6. LOOKING EAST ALONG 6TH AVENUE

# 03\_ALLEY CONDITIONS



7. LOOKING SOUTH ON ALLEY FROM STEWART ST.

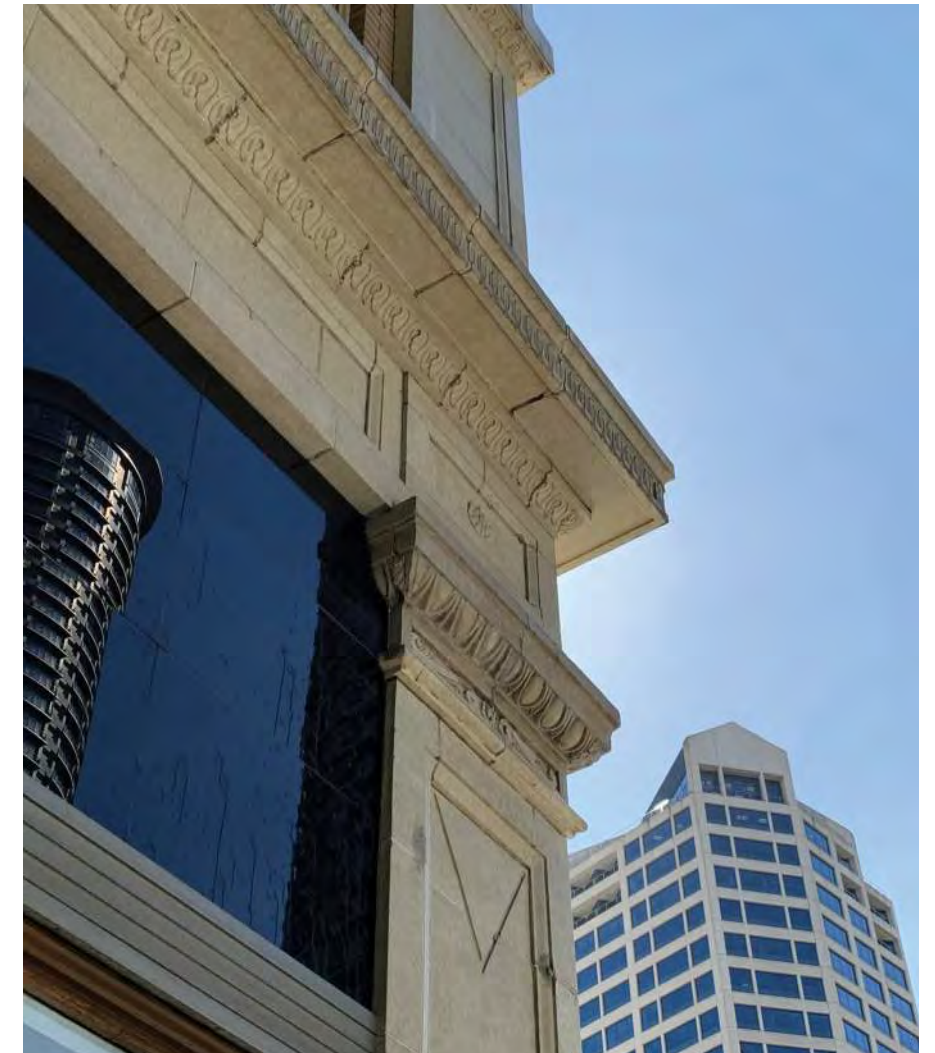


8. LOOKING SOUTH ON ALLEY FROM PARKING LOT.

# 03\_LLOYD BUILDING CONTEXTUAL ANALYSIS

## HISTORIC FACADE ANALYSIS

The rhythm and order of the historic Lloyd Building provides a distinctive character to the block and serves as a source of inspiration for the development of the podium in the preferred design option.



FACADE RELIEF ANALYSIS

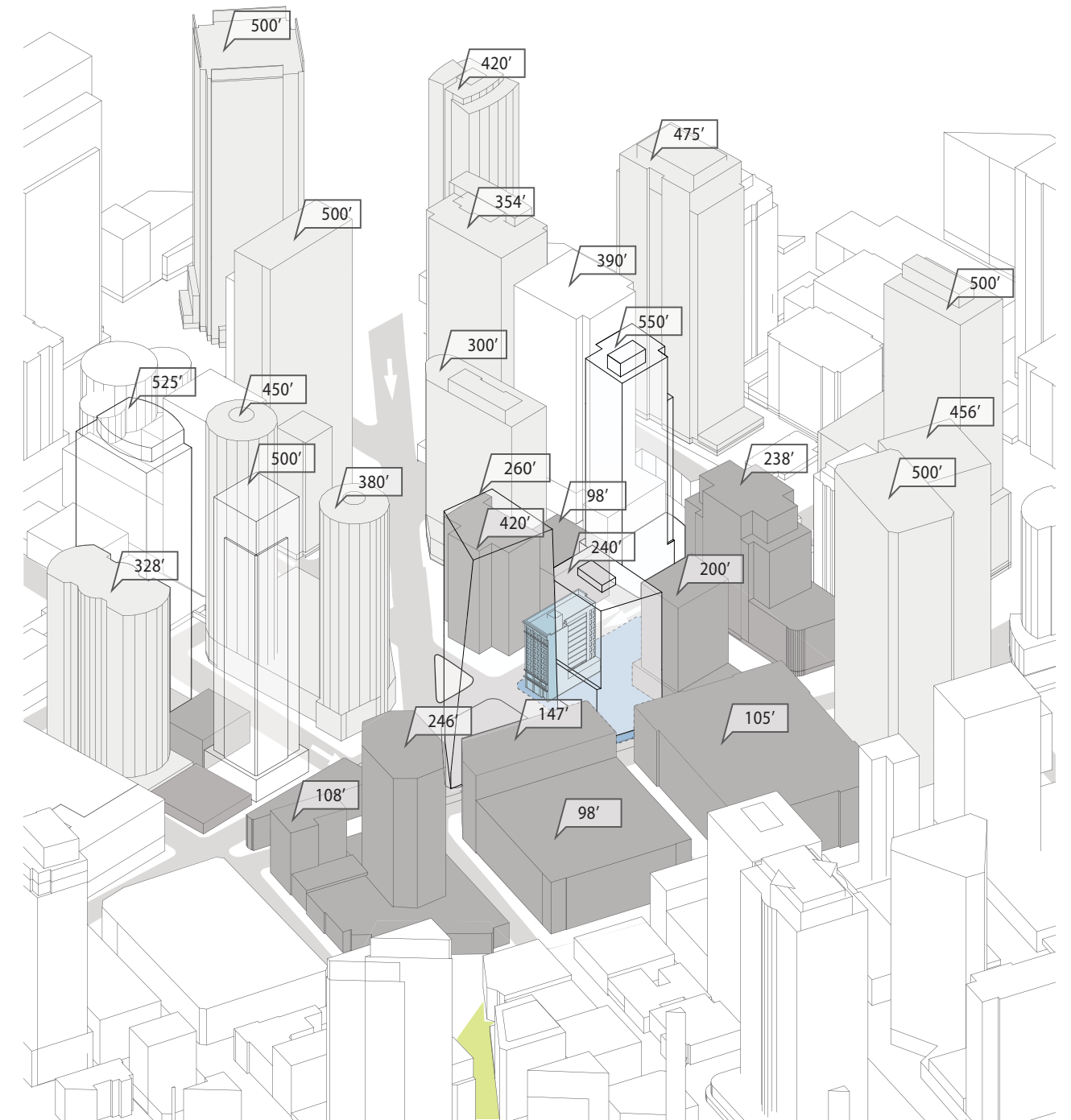
# 03\_CONTEXT BUILDING USE & HEIGHTS

- MULTI-FAMILY/MIXED USE RESI
- COMMERCIAL/RETAIL/OFFICE
- CIVIC/RELIGION
- RECREATION/OPEN SPACE
- MOTEL/HOTEL
- SITE
- FUTURE DEVELOPMENT

- 300' - 550'
- 100' - 260'
- SITE



CONTEXT AERIAL MAP - USES



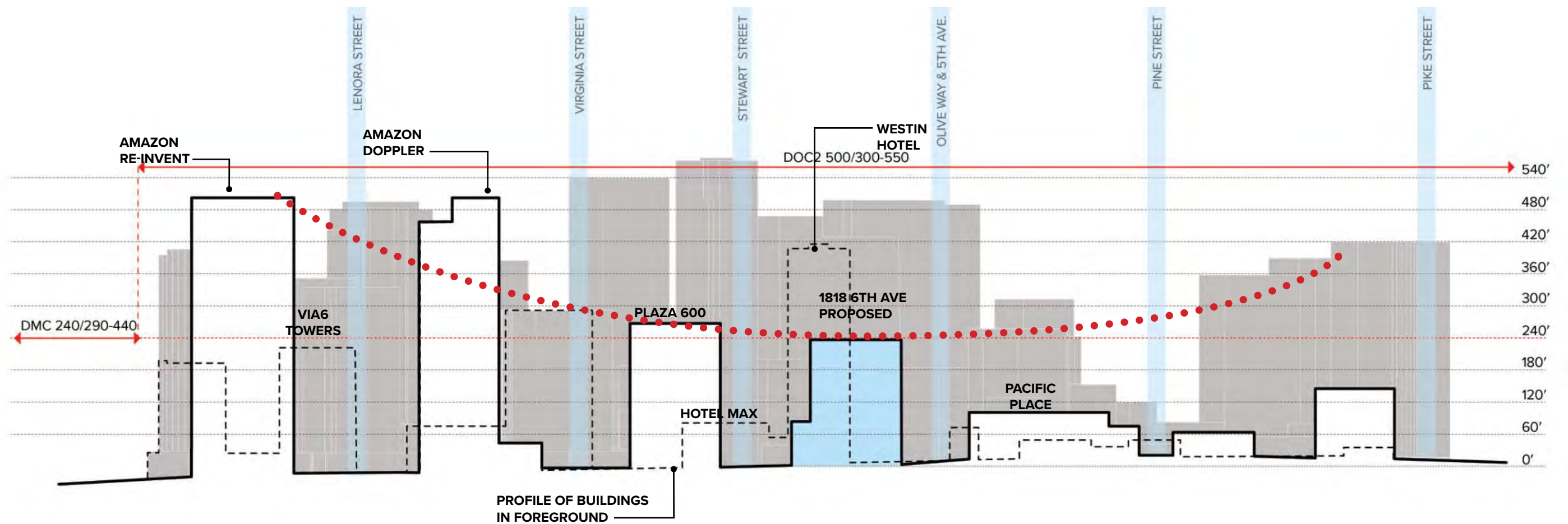
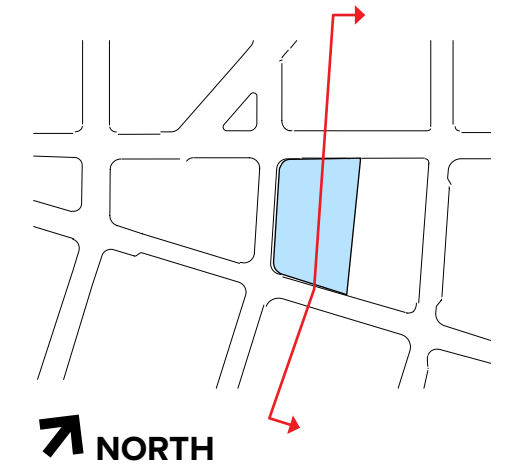
CONTEXT AERIAL MAP - HEIGHTS



# 03\_CONTEXT BUILDING HEIGHT ANALYSIS

## CONSIDERATIONS

- The project site sits at a “hinge” condition at the transition of the grid shift and the transition between the lower building heights of the shopping core and the taller building heights of the Denny Triangle tech hub
- The proposed new tower height responds to the immediate adjacent context, with a direct relationship to the Plaza 600 tower just to the north as well as a number of towers just to the southeast of this site. The intent is to provide both a complimentary massing to the historic Lloyd building and an intermediary scaled tower between the lower building heights of the shopping core and the taller building heights of the Denny Triangle tech hub
- The proposed new tower height is a significant reduction from the code allowable height on this parcel, made possible by the PCD transfer of FAR to the neighboring 1815 6th Ave Office Tower. Project Height is established under the Planned Community Development (PCD) Type II Decision.

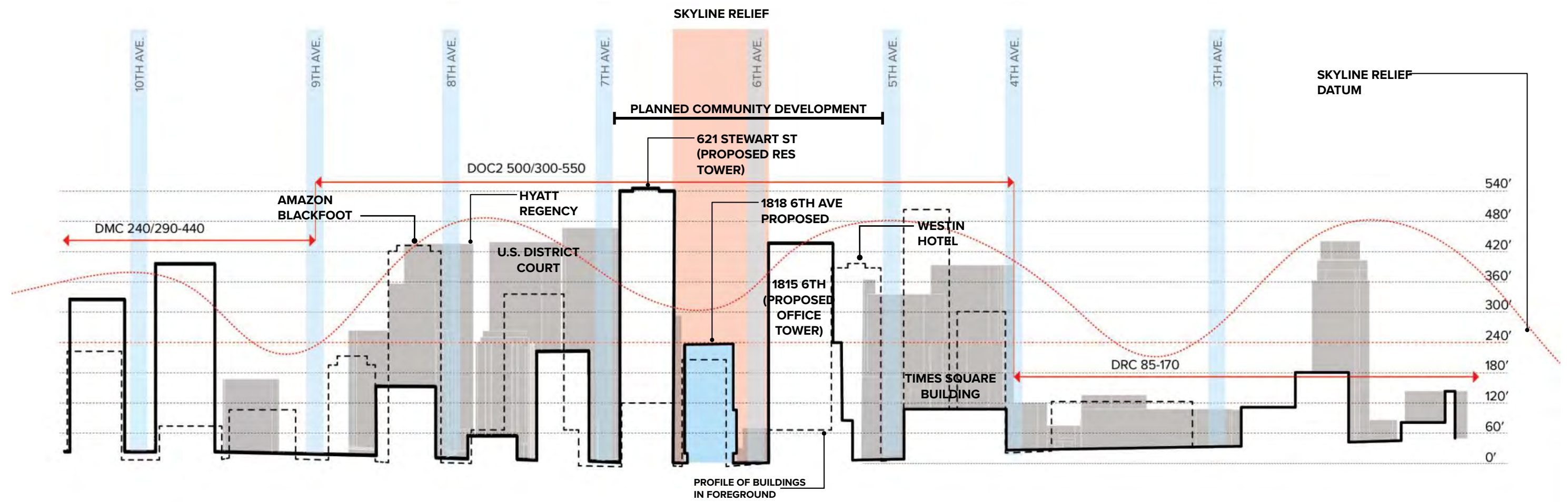
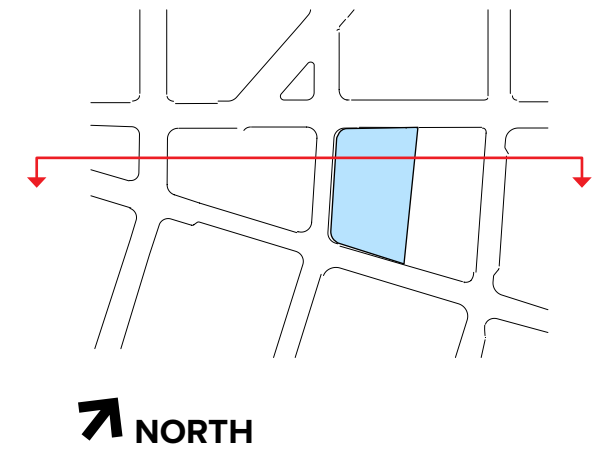


CONTEXTUAL HEIGHT STUDY - N/W AXIS

# 03\_CONTEXT BUILDING HEIGHT ANALYSIS

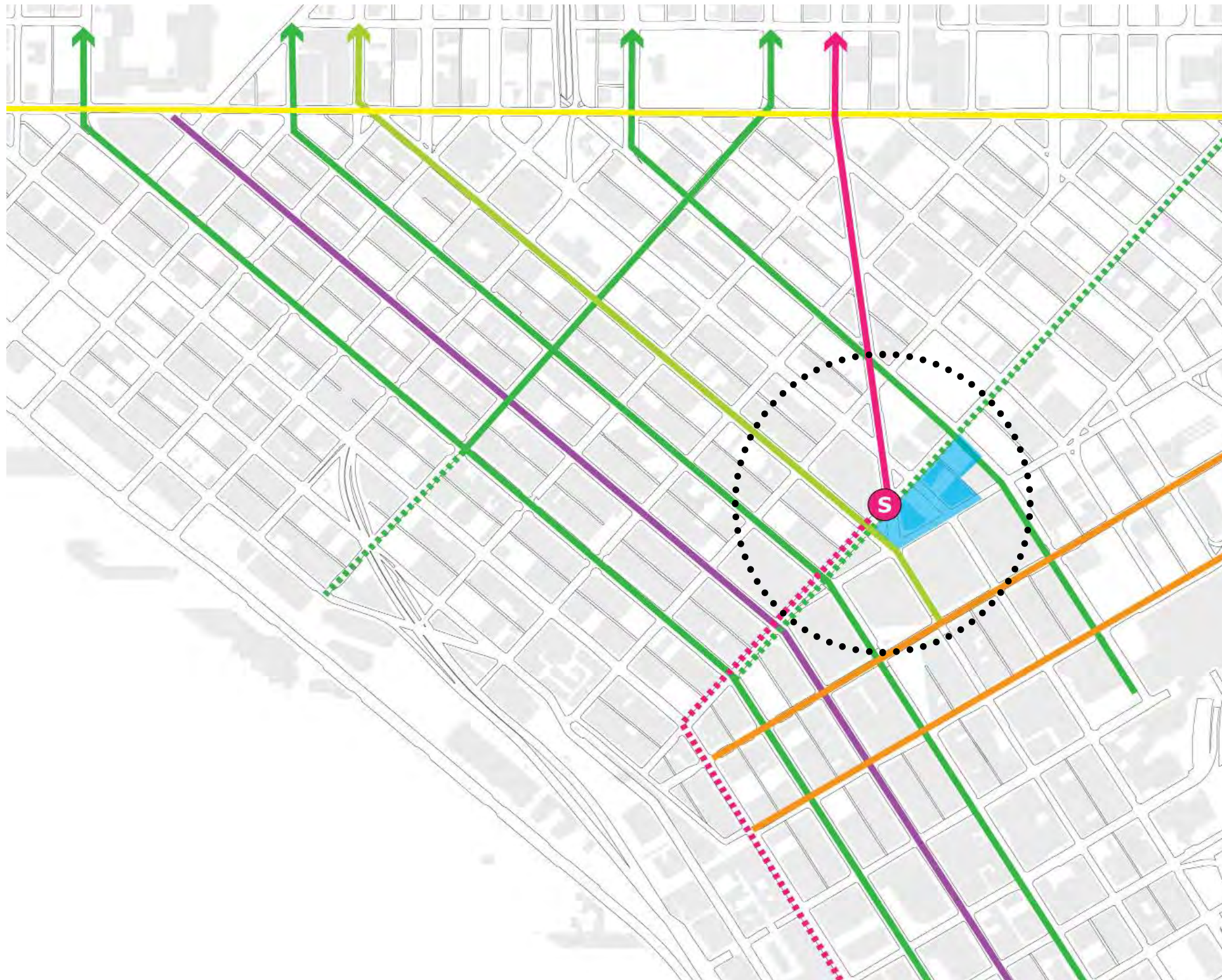
## CONSIDERATIONS

- Typical tower separation provides relief to the skyline and better access to light and views
- There is no tower separation requirement in this zone. Code allows both a 550' Residential tower and a 500' Commercial Office Tower on this block.
- The proposed new tower height is a significant reduction from the code allowable height on this parcel, made possible by the PCD transfer of FAR to the neighboring 1815 6th Ave Office Tower. Project Height is established under the Planned Community Development (PCD) Type II Decision.
- The proposed 1818 6th Ave new office tower height provides visual separation and relief between the 621 Stewart St Residential Tower and the 1815 6th Ave new office tower. It also maximizes views to the West for the proposed 621 Stewart St Residential tower.



SKYLINE RELIEF STUDY - E/W AXIS

## 03\_CITY INITIATIVES



### Current & Relevant Planning Studies:

- Bike Master Plan (2019): Allow for future protected bike lane on Stewart Ave (SDOT).
- City Center Connector (2019): consider future streetcar realignment and impact to McGraw Square (SDOT).
- ST3 Future Light Rail Station Location (2022): Potential future impact of Westlake Station planning (OPCD).
- Pike-Pine Corridor (2021): Consider design relevant design elements for continuity.
- » Seamless Seattle Pedestrian Wayfinding Program (2019): Plan for Area Sign and other signage at McGraw Square.
- » Westlake & 7th Ave Street Concept Plans (2013): Consider recommendations at Westlake and McGraw Squares focused on pedestrian circulation and open space.

### Other Less Relevant Planning Studies:

- Denny Way Street Concept Plan (2013)
- 5th Ave Corridor / Emerald Mile (2015)
- 3rd Ave Vision Plan (2019)
- » Denny Triangle Urban Design Framework Study (2015)
- » Westlake Hub Urban Strategy (2009)
- » South Lake Union Mobility Plan

**04**  
**ZONING DATA**

# 04\_ZONING DATA

ZONING CODE SUMMARY	
ADDRESS	1818 6TH AVE + 601 STEWART ST (TO BE COMBINED INTO ONE PARCEL AT 1818 6TH AVE)
APPLICABLE CODE	SEATTLE MUNICIPAL CODE, TITLE 23 LAND USE CODE
ZONING:	DOC2 500/300-550 DF
ZONING MAP 109	
NEIGHBORHOOD OVERLAY	DENNY TRIANGLE URBAN CENTER VILLAGE
23.49.056 MAP A	
FLOOR AREA RATIO	BASE 5
23.49.009.A, 23.49.011	MAX 15
KING COUNTY PARCEL NO.	065900-0330, 065900-0340, 065900-0350
PARCEL AREA (SF)	24,589
LEGAL DESCRIPTION	PARCEL A: LOTS 1, 2, 3, AND 4, BLOCK 6, ADDITION TO THE TOWN OF SEATTLE AS LAID OFF BY THE HEIRS OF SARAH A. BELL (COMMONLY KNOWN AS HEIRS OF SARAH A. BELL'S ADDITION TO THE CITY OF SEATTLE), ACCORDING TO THE PLAT THEREOF, RECORDED IN VOLUME 1 OF PLATS, PAGE 103, IN KING COUNTY, WASHINGTON
GROSS FLOOR AREA (GFA)	GROSS FLOOR AREA (GFA) MEANS THE NUMBER OF SQUARE FEET OF TOTAL FLOOR AREA BOUNDED BY THE INSIDE SURFACE OF THE EXTERIOR WALL OF THE STRUCTURE AS MEASURED AT THE FLOOR LINE, AND ANY ADDITIONAL AREAS IDENTIFIED AS GROSS FLOOR AREA WITHIN A ZONE.
23.84A.014	
PROPOSED SF & USE	OFFICE SF (RANGE): 346,000 - 370,000 SF RETAIL (RANGE): 3,000 - 4,500SF PARKING (RANGE): 150 - 185 STALLS RESIDENTIAL: 0 UNITS
MANDATORY HOUSING AFFORDABILITY	THE PROVISIONS OF CHAPTERS 23.58 B AND 23.58C APPLY IN ALL DOWNTOWN ZONES
23.49.007	
STREET CLASSIFICATIONS	WESTLAKE AVE AND 6TH AVE: PRINCIPAL ARTERIAL; 7TH AVE: MINOR ARTERIAL 15'; OLIVE ST AND STEWART ST: PRINCIPAL TRANSIT STREET
23.49 MAP 1B	
SIDEWALK WIDTH REQUIREMENTS	WESTLAKE AVE, 6TH AVE AND 7TH AVE: 15'; OLIVE ST AND STEWART ST: 18' (WHEN ON A ONE-WAY STREET, ONLY THE SIDE WITH TRANSIT STOPS SHALL BE 18'; THE OTHER SIDE SHALL BE 15')
23.49 MAP 1C	
VIEW CORRIDORS	N/A
23.49 MAP 1D	
PEDESTRIAN STREET CLASSIFICATIONS	6TH AVE: CLASS II; WESTLAKE AVE, 7TH AVE, OLIVE ST AND STEWART ST: CLASS I
23.49 MAP 1F	
STREET LEVEL USES REQUIRED	ALL
23.49 MAP 1G	
PROPERTY LINE FACADES	6TH AVE, OLIVE; SEE MAP.
23.49 MAP 1H	
PUBLIC AMENITY AND OTHER FEATURES	FAR EXEMPTION AREA USES LISTED IN 23.49.009.A, RETAIL STORE AND SHOPPING ATRIUM
23.49 MAP 1J	

STREET-LEVEL STANDARDS	
REQUIRED STREET LEVEL USES	WESTLAKE AVE, 6TH AVE, 7TH AVE, OLIVE ST AND STEWART ST
MAP 1G	<i>(Departure Requested for Active Use percentage requirement along 6th Ave.)</i>
OVERHEAD WEATHER PROTECTION AND LIGHTING	CONTINUOUS OVERHEAD PROTECTION AT 10 - 15 FT ABOVE SIDEWALK EXCLUDING THOSE PORTIONS OF THE STRUCTURE FAÇADE THAT ARE LOCATED FARTHER THAN FIVE FEET FROM THE STREET PROPERTY LINE OR WIDENED SIDEWALK, OR ARE SEPARATED FROM THE STREET PROPERTY LINE BY A LANDSCAPED AREA AT LEAST TWO FEET IN WIDTH. ADEQUATE LIGHTING FOR PEDESTRIANS SHALL BE PROVIDED.
23.49.018	
MINIMUM FAÇADE HEIGHT REQUIREMENTS	6TH AVE 25' (CLASS 2 PEDESTRIAN ST) WESTLAKE AVE, 7TH AVE, OLIVE ST AND STEWART ST 35' (CLASS 1 PEDESTRIAN ST)
23.49.056.A	
MAXIMUM SETBACKS	OLIVE ST AND 6TH AVE, BLOCK 6 ONLY 15'<X<35' ABOVE SIDEWALK: FAÇADE SHALL BE LOCATED WITHIN 2' OF STREET LOT LINE SEE 23.49.056.B FOR EXEMPTIONS.
MAP 1H, 23.49.056.B	
FAÇADE TRANSPARENCY	AREA BETWEEN 2' AND 8' ABOVE THE SIDEWALK CLASS 1 PEDESTRIAN STREETS: MINIMUM 60% SHALL BE TRANSPARENT CLASS 2 PEDESTRIAN STREETS: MINIMUM 30% SHALL BE TRANSPARENT
23.49.056.C	
BLANK FAÇADE	AREA BETWEEN 2' AND 8' ABOVE THE SIDEWALK CLASS 1 PEDESTRIAN STREETS: NO MORE THAN 15' WIDE EXCEPT AT GARAGE DOORS, TOTAL BLANK FAÇADE SEGMENTS WILL NOT EXCEED 40%. CLASS 2 PEDESTRIAN STREETS: NO MORE THAN 30' WIDE EXCEPT AT GARAGE DOORS, TOTAL BLANK FAÇADE SEGMENTS WILL NOT EXCEED 70%
23.49.056.D	
SETBACKS AND LANDSCAPING REQUIREMENTS	REQUIREMENTS FOR LOTS LOCATED WITHIN THE DENNY TRIANGLE AREA: LANDSCAPED AREA TO BE 1.5X LENGTH OF THE STREET LOT LINE. LANDSCAPED AREA TO BE AT LEAST 18" WIDE LANDSCAPED AREAS TO BE AT LEAST 20% OF AREAS ABUTTING THE STREET LOT LINE.
23.49.056.F	

UPPER LEVEL DEVELOPMENT STANDARDS	
FAÇADE MODULATION AND UPPER-LEVEL WIDTH LIMIT	FAÇADE MODULATION IS REQUIRED ABOVE 85 FEET AS PRESCRIBED BY TABLE A ON LOTS WHERE THE WIDTH AND DEPTH EXCEEDS 200 FEET, THE MAX. FAÇADE WIDTH ABOVE 280 FT. SHALL BE 145 FEET ALONG THE NORTH/SOUTH AXIS.
23.49.058.2	
23.49.058.3	<i>(Departure requested for maximum facade length above 160' in height along 6th Ave)</i>

AREA AND HEIGHT LIMITS			
MAXIMUM HEIGHT	NONRESIDENTIAL USE: 500'-0"		
23.49.008	RESIDENTIAL USES: 300'-0" BASE, 550'-0" MAX		
HEIGHT EXCEPTIONS	STRUCTURES IN DOC2 500/300-550 ZONES MAY EXCEED THE MAXIMUM HEIGHT LIMIT FOR RESIDENTIAL USE BY 10% IN HEIGHT INCREASE IF THE FAÇADES OF THE PORTION OF THE STRUCTURE ABOVE THE LIMIT DO NOT ENCLOSE AN AREA GREATER THAN 9,000 SQUARE FEET, AND THE ENCLOSED SPACE IS OCCUPIED ONLY BY THOSE USES OR FEATURES OTHERWISE PERMITTED IN THIS SECTION 23.49.008 AS AN EXCEPTION ABOVE THE HEIGHT LIMIT. THE EXCEPTION IN THIS SUBSECTION 23.49.008.B SHALL NOT BE COMBINED WITH ANY OTHER HEIGHT EXCEPTION FOR SCREENING OR ROOFTOP FEATURES TO GAIN ADDITIONAL HEIGHT.		
23.49.008.B			
ROOFTOP FEATURES	UP TO 4': OPEN RAILINGS, PLANTERS, CLERESTORIES, SKYLIGHTS, PLAY EQUIPMENT, PARAPETS, AND FIREWALLS UP TO 2': INSULATION MATERIAL, ROOFTOP DECKS AND OTHER SIMILAR FEATURES, OR SOIL FOR LANDSCAPING UP TO 7': SOLAR COLLECTORS UP TO 15': SOLAR COLLECTORS, STAIR PENTHOUSES, PLAY EQUIPMENT AND OPEN-MESH FENCING, COVERED OR ENCLOSED COMMON RECREATION AREA OR EATING AND DRINKING ESTABLISHMENT, MECHANICAL EQUIPMENT, WIND TURBINES, GREENHOUSES UP TO 23': ELEVATOR PENTHOUSES UP TO 50': RELIGIOUS SYMBOLS FOR RELIGIOUS INSTITUTIONS, SMOKESTACKS, AND FLAGPOLES		
23.49.008.D			
FAR	BASE	5	
23.49.011, TABLE A	MAX	15	
FAR CALCULATION (PCD)	PARCEL	LOT AREA (SF)	MAXIMUM CHARGEABLE FAR (SF)
	1818 6TH	24,589	368,835
	621 STEWART	13,399	200,985
	1815 6TH	21,461	321,915
	<b>TOTAL</b>	<b>59,449</b>	<b>891,735</b>
FAR EXEMPTIONS	STREET-LEVEL USES MEETING THE REQUIREMENTS OF SECTION 23.49.009 RESIDENTIAL USE FLOOR AREA BELOW GRADE SHOWER FACILITIES FOR BIKE COMMUTERS PUBLIC BENEFIT FEATURE PUBLIC RESTROOMS 3.5% DEDUCTION FOR MECHANICAL EQUIPMENT ALLOWANCE		
23.49.011B			
BASE FAR	1ST INCREMENT OF ADDITIONAL FAR: REGIONAL DEVELOPMENT CREDITS: .75 FAR		
23.49.012.B.1	LOW INCOME HOUSING: 15.6% PER GSF OF BONUS FLOOR AREA; OR \$24.85 PER GSF OF BONUS FLOOR AREA		
23.49.013	PUBLIC OPEN SPACE AMENITIES: URBAN PLAZAS, PARCEL PARKS, PUBLIC ATRIA, GREEN STREET IMPROVEMENTS AND GREEN STREET SETBACKS ON DESIGNATED GREEN STREETS. MAXIMUM AREAS FOR BONUS FLOOR AREA PER TABLE A FOR 23.49.013.		
23.49.015	BONUS RESIDENTIAL FLOOR AREA IN DOC2 ZONES FOR VOLUNTARY AGREEMENTS FOR LOW-INCOME HOUSING AND MODERATE-INCOME HOUSING		

# 04\_ZONING DATA

PARKING & ACCESS	
<u>REQUIRED PARKING</u> 23.49.019	NO PARKING, EITHER LONG-TERM OR SHORT-TERM, IS REQUIRED FOR USES ON LOTS IN DOWNTOWN ZONES.
<u>PARKING PROVIDED</u>	STALL COUNT (RANGE): 150 - 185
<u>ACCESS</u> 23.54.030	FOR LOTS ON PRINCIPAL ARTERIALS AS DESIGNATED BY THE SEATTLE DEPARTMENT OF TRANSPORTATION, THE MAXIMUM CURB CUT WIDTH IS 23 FEET. THE MINIMUM DISTANCE BETWEEN ANY TWO CURB CUTS LOCATED ON A LOT IS 30 FEET
TABLE B	CURB CUTS FOR PRINCIPAL ARTERIAL STREET FRONTAGE: 160 FEET OR LESS - 1 GREATER THAN 160 FEET UP TO 320 FEET - 2 GREATER THAN 320 FEET UP TO 480 FEET - 3
23.54.030.F.2.A.4	IN DOWNTOWN ZONES, A MAXIMUM OF TWO CURB CUTS FOR ONE-WAY TRAFFIC AT LEAST 40 FEET APART, OR ONE CURB CUT FOR TWO-WAY TRAFFIC, ARE PERMITTED ON EACH STREET FRONT WHERE ACCESS IS PERMITTED BY SUBSECTION 23.49.019.H. NO CURB CUT SHALL BE LOCATED WITHIN 40 FEET OF AN INTERSECTION. THESE STANDARDS MAY BE MODIFIED BY THE DIRECTOR AS A TYPE I DECISION ON LOTS WITH STEEP SLOPES OR OTHER SPECIAL CONDITIONS, TO THE MINIMUM EXTENT NECESSARY TO PROVIDE VEHICULAR AND PEDESTRIAN SAFETY AND FACILITATE A SMOOTH FLOW OF TRAFFIC.
<u>RIDESHARING AND TRANSIT INCENTIVE</u> 23.49.019.D	THE REQUIREMENTS IN THIS SECTION APPLY TO ALL NEW STRUCTURES CONTAINING MORE THAN 10,000 SF OF NEW NON-RESIDENTIAL USE, AND TO STRUCTURES WHERE MORE THAN 10,000 SF OF NON-RESIDENTIAL USE IS PROPOSED TO BE ADDED.  1. BUILDING OWNER SHALL ESTABLISH AND MAINTAIN A TRANSPORTATION COORDINATOR POSITION.  2. BUILDING OWNER SHALL ALLOW A DESIGNATED SEATTLE DEPARTMENT OF TRANSPORTATION OR RIDESHARE REPRESENTATIVE TO REVIEW OPERATION OF THE RIDESHARING PROGRAM.  3. BUILDING OWNER SHALL PROVIDE AND MAINTAIN A TRANSPORTATION INFORMATION CENTER.
<u>ACCESS TO LOADING</u> 23.66.170	ACCESS TO PARKING AND LOADING FROM ALLEYS, AND FROM STREETS THAT GENERALLY RUN EAST/WEST IS PREFERRED TO ACCESS FROM AVENUES.
<u>ALLEY IMPROVEMENTS</u> 23.53.030	PER TABLE C, REQUIRED MINIMUM RIGHT-OF-WAY WIDTH FOR EXISTING ALLEYS IS 20 FEET.
<u>LOADING BERTH REQUIREMENTS</u> 23.54.035	MIN. NUMBER REQUIRED (LOW DEMAND): 4 PROPOSED NUMBER: 2 EACH LOADING BERTH SHALL NOT BE LESS THAN 10' IN WIDTH AND SHALL PROVIDE NOT LESS THAN 14' FEET VERTICAL CLEARANCE.  LOW-DEMAND USE: MINIMUM 25' IN LENGTH. <b>(TYPE I DECISION REQUESTED FOR A REDUCTION IN QUANTITY OF LOADING BERTHS)</b>
<u>BIKE PARKING</u> 23.54.015.k	MINIMUM NUMBER OF OFF-STREET PARKING SPACES FOR BICYCLES REQUIRED FOR SPECIFIC USES IS SET FORTH IN TABLE D FOR 23.54.015. LONG-TERM PARKING FOR BICYCLES SHALL BE FOR BICYCLES PARKED FOUR OR MORE HOURS.  1. ROUNDING. CALCULATION OF THE MINIMUM REQUIREMENT SHALL

PARKING & ACCESS CONTINUED	
	ROUND UP TO THE NEAREST WHOLE NUMBER. 2. PERFORMANCE STANDARDS. PROVIDE BICYCLE PARKING IN A HIGHLY VISIBLE, SAFE, AND CONVENIENT LOCATION, EMPHASIZING USER CONVENIENCE AND THEFT DETERRENCE  SALES AND SERVICES (RETAIL): LONG TERM: 1 PER 5,000 SF SHORT TERM: 1 PER 1,000 SF DWELLING: LONG TERM: 1 PER 1 UNIT SHORT TERM: 1 PER 20 UNITS OFFICE: LONG TERM: 1 PER 2,000 SF SHORT TERM: 1 PER 10,000 SF  SALES AND SERVICES: 3,000 - 4,500 SF DWELLING: 0 UNITS OFFICE: 346,000 - 370,000 SF ESTIMATED STALL CALCS (MAX RANGE) <b>186 LONG-TERM PARKING SPOTS REQ'D</b> <b>42 SHORT-TERM PARKING SPOTS REQ'D</b> <b>263 PROVIDED</b>  6. EXCEPT AS PROVIDED IN SUBSECTION 23.54.015.K.7, BICYCLE PARKING FACILITIES REQUIRED FOR NON-RESIDENTIAL USES SHALL BE LOCATED:  A. ON THE LOT; OR B. FOR A FUNCTIONALLY INTERRELATED CAMPUS CONTAINING MORE THAN ONE BUILDING, IN A SHARED BICYCLE PARKING FACILITY WITHIN 600 FEET OF THE LOT.  7. BOTH LONG-TERM AND SHORT-TERM BIYCLE PARKING FOR NON-RESIDENTIAL USES ON A FUNCTIONALLY INTERRELATED CAMPUS CONTAINING MORE THAN ONE BUILDING MAY BE LOCATED IN AN OFF-SITE LOCATION WITHIN 600 FEET OF THE LOT, AND SHORT-TERM PUBLIC BICYCLE PARKING MAY BE PROVIDED IN A PUBLIC PLACE, SUBJECT TO

LANDMARKS PRESERVATION	
<u>LANDMARKS PRESERVATION</u> 25.12.080	"CERTIFICATE OF APPROVAL" IS WRITTEN AUTHORIZATION WHICH MUST BE ISSUED BY THE BOARD OR CITY HISTORIC PRESERVATION OFFICER, AS APPLICABLE, BEFORE ANY ALTERATION OR SIGNIFICANT CHANGE MAY BE MADE TO THE CONTROLLED FEATURES OF A LANDMARK OR LANDMARK SITE, OR DURING THE PENDENCY OF DESIGNATION PROCEEDINGS, TO A SITE, IMPROVEMENT OR OBJECT AFTER ITS NOMINATION HAS BEEN APPROVED BY THE BOARD FOR FURTHER PROCEEDINGS.

OPEN SPACE REQUIREMENTS	
<u>QUANTITY OF OPEN SPACE FOR OFFICE USE</u> 23.49.016B	20 SF OF OPEN SPACE PER 1,000 SF OF OFFICE SPACE REQUIRED FOR PROJECTS THAT INCLUDE 85,000 SF OR MORE OF GROSS OFFICE FLOOR AREA IN DOC2, EXCEPT FLOOR AREA SATISFYING THE PROVISIONS OF 23.49.011.B.1.H
<u>STANDARDS FOR OPEN SPACE</u> 23.49.016c	PRIVATE OPEN SPACE ON-SITE PUBLIC OPEN SPACE OFF-SITE PUBLIC OPEN SPACE EASEMENT FOR OFF-SITE OPEN SPACE: THE OWNER OF ANY LOT ON WHICH OFF-SITE OPEN SPACE IS PROVIDED TO MEET THE REQUIREMENTS OF THIS SECTION SHALL EXECUTE AND RECORD AN EASEMENT IN A FORM ACCEPTABLE TO THE DIRECTOR ASSURING COMPLIANCE WITH THE REQUIREMENTS OF THIS SECTION, INCLUDING APPLICABLE CONDITIONS OF THE DOWNTOWN AMENITY STANDARDS. THE DIRECTOR IS AUTHORIZED TO ACCEPT SUCH AN EASEMENT, PROVIDED THAT THE TERMS DO NOT IMPOSE ANY COSTS OR OBLIGATIONS ON THE CITY. PAYMENT IN LIEU: IN LIEU OF PROVIDING OPEN SPACE UNDER THIS REQUIREMENT, AN OWNER MAY MAKE A PAYMENT TO THE CITY IF THE DIRECTOR DETERMINES THAT THE PAYMENT WILL CONTRIBUTE TO THE IMPROVEMENT OF A DESIGNATED GREEN STREET OR TO OTHER PUBLIC OPEN SPACE IMPROVEMENTS ABUTTING THE LOT OR IN THE VICINITY, IN AN AMOUNT SUFFICIENT TO DEVELOP IMPROVEMENTS THAT WILL MEET THE ADDITIONAL NEED FOR OPEN SPACE CAUSED BY THE PROJECT, AND THAT THE IMPROVEMENT WITHIN A REASONABLE TIME IS FEASIBLE.

VENTING/LIGHTING/SOLID WASTE	
<u>VENTING OF ODORS</u> 23.49.025.A	THE VENTING OF ODORS, FUMES, VAPORS, SMOKE, CINDERS, DUST, AND GAS SHALL BE AT LEAST 10 FEET ABOVE FINISHED SIDEWALK GRADE, AND DIRECTED AWAY FROM USES WITHIN 50 FEET OF THE VENT.
<u>LIGHTING AND GLARE</u> 23.49.025.C	EXTERIOR LIGHTING SHALL BE SHIELDED AND DIRECTED AWAY FROM ADJACENT USES.
<u>SOLID WASTE</u> 23.49.025.C/23.54.040	STORAGE SPACE FOR SOLID WASTE AND RECYCLABLE MATERIALS CONTAINERS SHALL BE PROVIDED AS SHOWN IN TABLE A FOR 23.54.040 FOR ALL NEW STRUCTURES, AND FOR EXISTING STRUCTURES TO WHICH TWO OR MORE DWELLING UNITS ARE ADDED.
23.54.040 - TABLE A	NON-RESIDENTIAL GSF 50,001 - 100,000 SF MINIMUM AREA FOR SHARED STORAGE SPACE: 225 SF PROPOSED TRASH ROOM IS GREATER THAN 225 SF.

[THIS PAGE LEFT INTENTIONALLY BLANK]

**05**  
**DESIGN GUIDELINES**

# 05 DESIGN GUIDELINES

## B-2 CREATE A TRANSITION IN BULK & SCALE

Compose the massing of the building to create a transition to the height, bulk, and scale of development in neighboring or nearby less intensive zones.

### RESPONSE:

This site is situated between the shopping core of downtown and the Denny Triangle tech hub. Although the adjacent blocks to this property are in the same DOC2 500/300-550 zone, the general fabric of the shopping core just to the south of this site is under 240'. Just two blocks north, the Amazon towers sit at 500'. The proposed building height and mass provides an important transition and stitching of this currently disconnected urban fabric.

## B-3 REINFORCE THE POSITIVE URBAN FORM AND ARCHITECTURAL ATTRIBUTES OF THE IMMEDIATE AREA

Consider the predominant attributes of the immediate neighborhood and reinforce desirable siting patterns, massing arrangements, and streetscape characteristics of nearby development.

### RESPONSE:

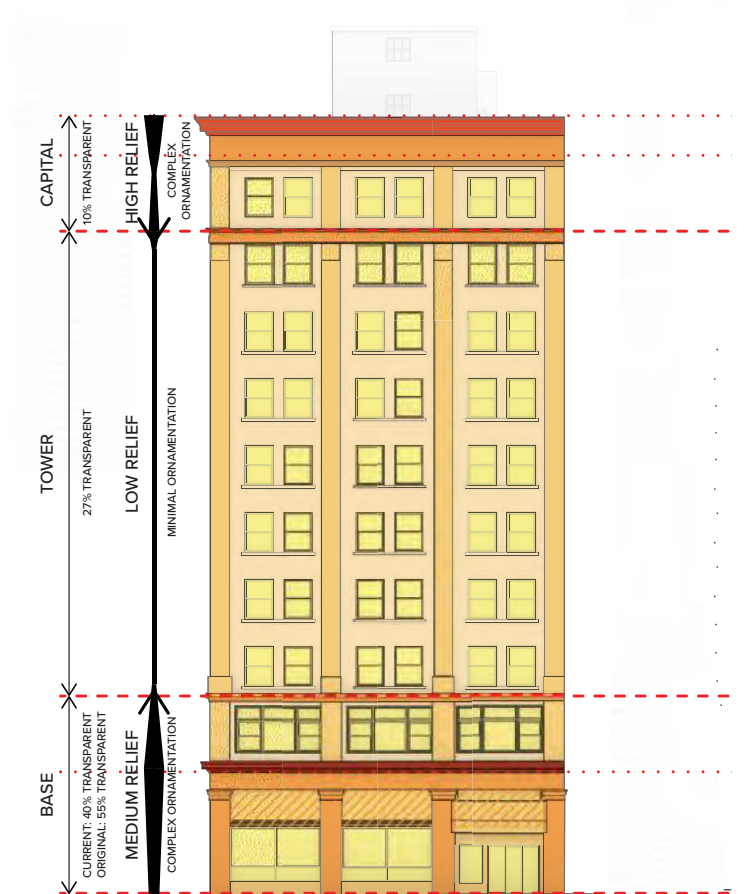
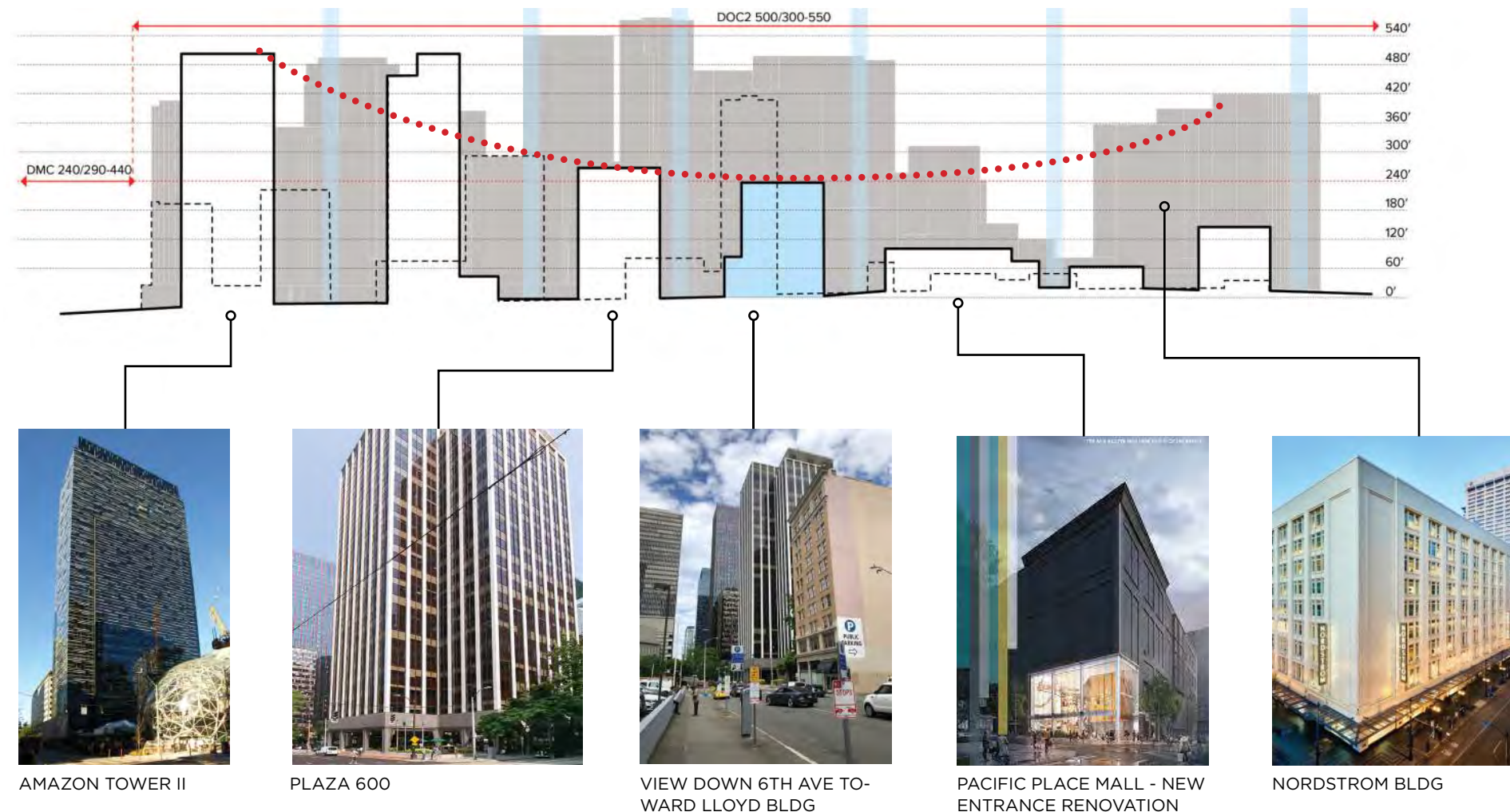
A key design strategy of this proposal is to harmonize with the adjacent historic Lloyd Building, as well as the other surrounding context. In order to achieve this, the proposed massing responds in both building height and massing articulation to key datums from the Lloyd Building and the other adjacent structures. A thorough analysis of the Lloyd Building's historic facade has been included in the proposal and has informed the proposed massing & facade concept.

## C-2 DESIGN FACADES OF MANY SCALES

Design architectural features, fenestration patterns, and materials compositions that refer to the scale of human activities contained within. Building facades should be composed of elements scaled to promote pedestrian comfort, safety, and orientation.

### RESPONSE:

Many facade concepts have been studied with the goal to develop an appropriate composition that balances the new, modern office tower with the historic Lloyd Building. The preferred scheme takes inspiration from the Lloyd Building's facade composition, which varies in both scale and depth of facade elements as you move from the pedestrian, ground level to the top level of the building.



# 05\_DESIGN GUIDELINES

## C-4 REINFORCE BUILDING ENTRIES

*To promote pedestrian comfort, safety, and orientation, reinforce the building's entry.*

### RESPONSE:

The massing concepts all aim to create pedestrian focal points at building entries. Each scheme treats the relationship between the historic Lloyd building entry and the new office tower building entry differently. The preferred and proposed scheme combines building lobbies between the historic Lloyd structure and the new office tower into one shared lobby, while maintaining clear and distinct separate entrances off of both Stewart Street and 6th Ave. The new primary building entry off of 6th Ave occurs within the 'spatial stitch' between the new office tower podium and the Lloyd Building, celebrating the dynamic relationship between new and historic.



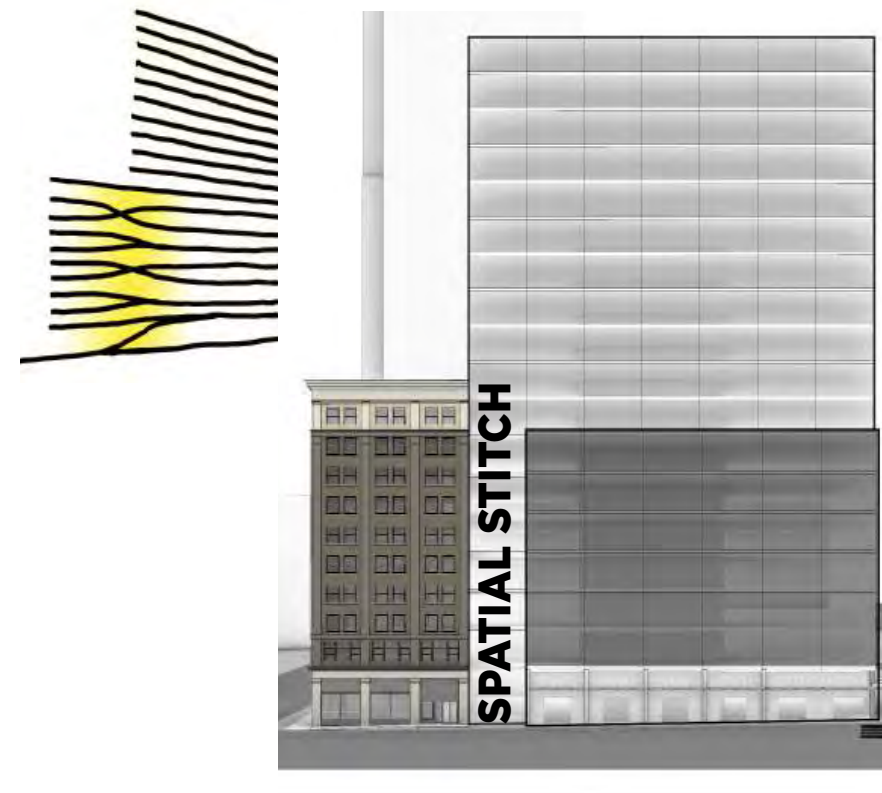
PERSPECTIVE VIEW OF THE PROPOSED BUILDING ENTRY

## D-3 PROVIDE ELEMENTS THAT DEFINE THE PLACE

*Provide special elements on the facades, within public open spaces, or on the sidewalk to create a distinct, attractive, and memorable "sense of place" associated with the building.*

### RESPONSE:

The "Spatial Stitch" in the preferred concept is a distinctive design element which defines the project's identity as an integration of the historic and contemporary. The connection of existing and new structural floors occurs at multiple levels and travels up the building. This connection is visible on the exterior and will be a unique and memorable element within the development.



CONCEPTUAL SKETCH OF THE 'SPATIAL STITCH'

## E-3 MINIMIZE THE PRESENCE OF SERVICE AREAS

*Locate service areas for trash dumpsters, loading docks, mechanical equipment, and the like away from the street front where possible. Screen from view those elements which for programmatic reasons cannot be located away from the street front.*

### RESPONSE:

The proposed massing concept combines the historic Lloyd structure with the new office tower structure, and proposes a singular mechanical and electrical system that will service both new and old floors. By consolidating MEP systems into the new office tower, it removes service equipment that may otherwise have been located along street frontages of the Lloyd building and locates the majority of these functions on either the roof, basement, or alley facade of the new office tower. Where service program and/or exiting occurs along street frontages, a combination of architectural treatments and display windows will activate the streetscape and create an engaging pedestrian experience.

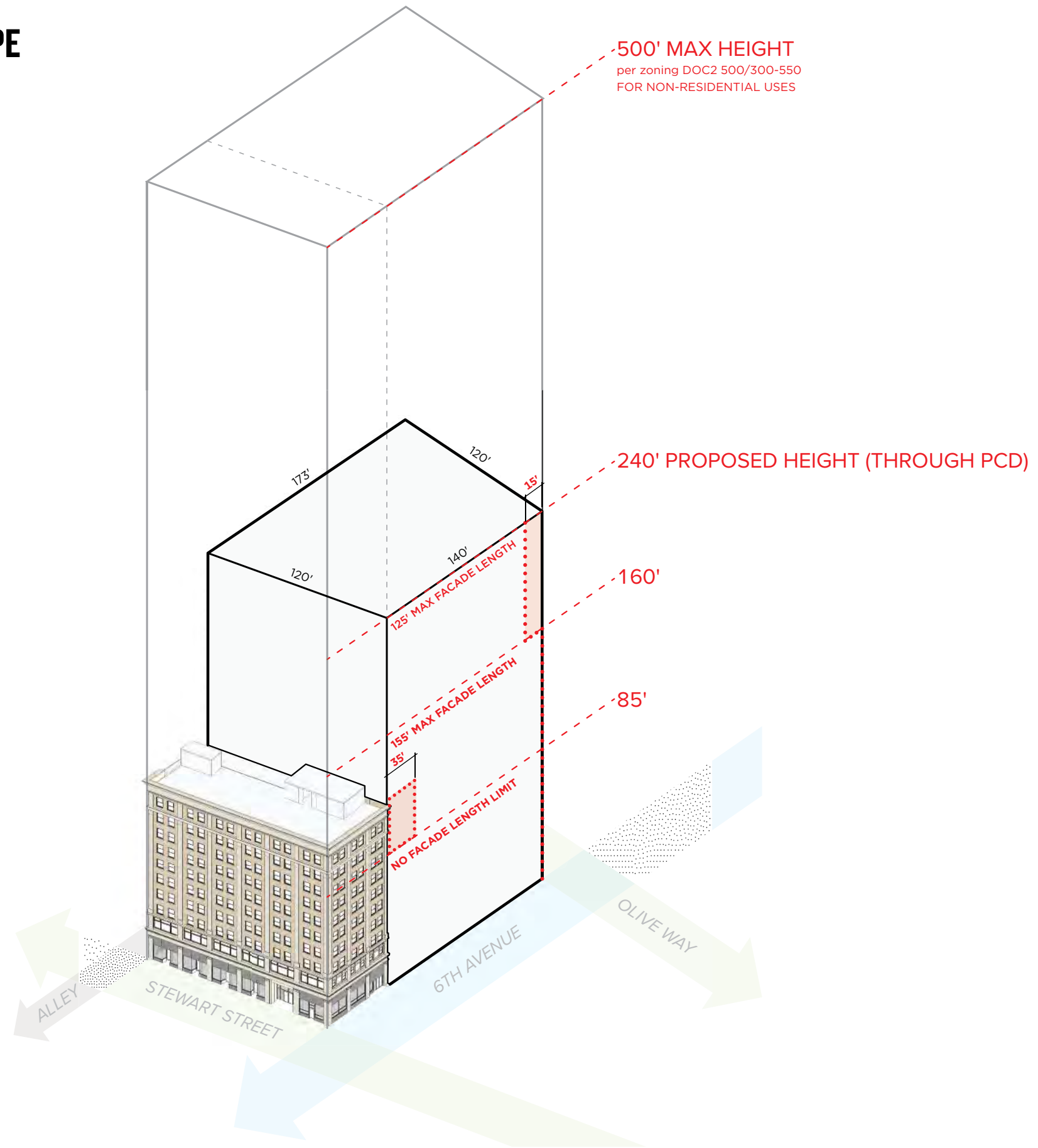


[THIS PAGE LEFT INTENTIONALLY BLANK]

**06**

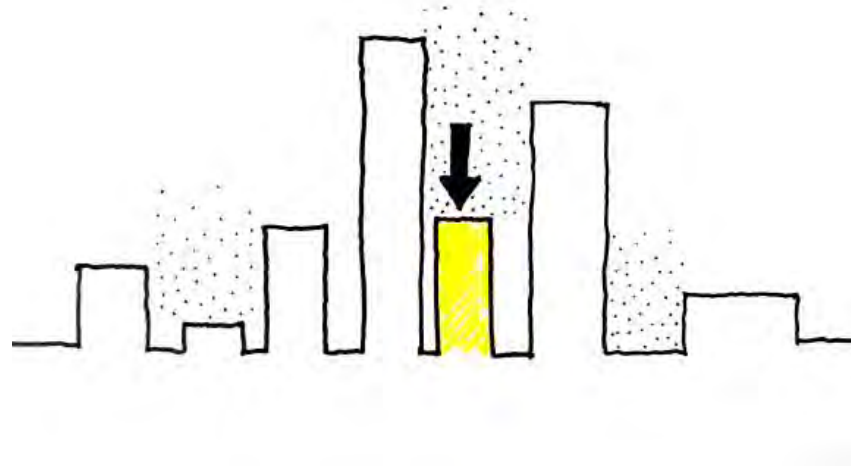
**ARCHITECTURAL MASSING CONCEPTS**

# 06\_PRESCRIPTIVE ZONING ENVELOPE



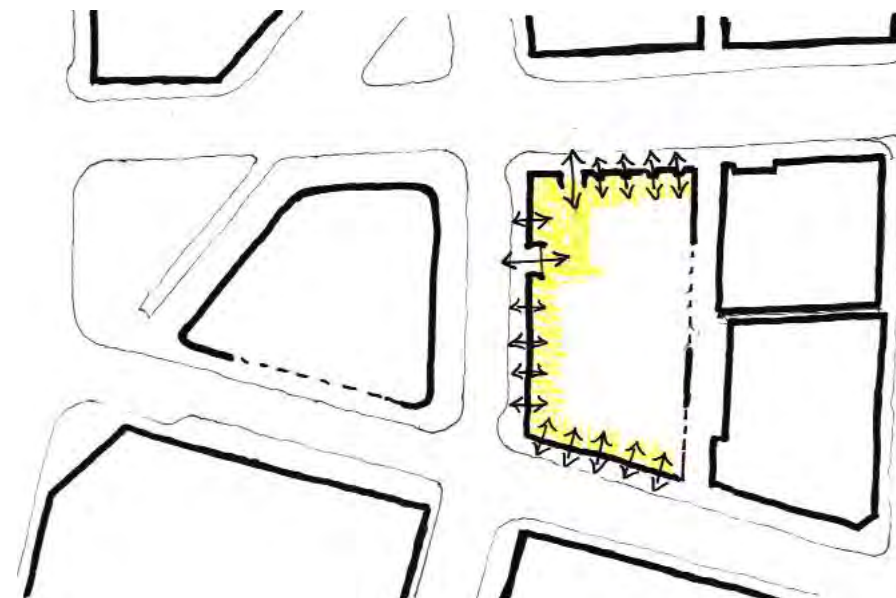
## 06\_GUIDING PRINCIPLES

The following principles are used to guide the development of all three massing schemes at the city scale, the street scale, and the building scale.



### PROVIDING SKYLINE RELIEF

At the city scale, the new 1818 6th Ave tower anticipates the future development of neighboring parcels within the PCD and maintains a shorter height of 240'. This allows for more generous separation between future towers and develops a more appropriate scale relationship with the historic Lloyd Building.



### ACTIVATING THE PUBLIC REALM

At the street scale, this project fills a missing tooth in the urban fabric and introduces new active frontages that will link the energy of the downtown core to the vibrancy of the Denny Triangle tech blocks. The project employs a mixture of retail, highly transparent lobbies and integrated art pieces to create a lively and inviting public realm.

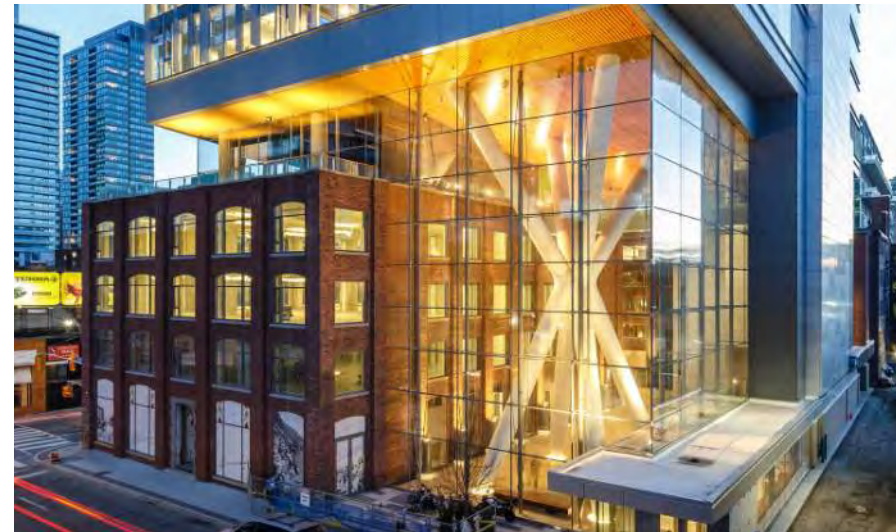


### CELEBRATING THE HISTORIC CONNECTION

At the building scale, the project embraces the historic structure as the greatest asset of the site and stitches the new tower into the existing Lloyd Building. The interface between historic and new is celebrated as a unique spatial opportunity and market differentiator.

## 06\_CONCEPT APPROACHES: MODERN ADDITIONS TO HISTORIC STRUCTURES

The following concept drivers describe three distinctly different ways of creating a new, modern addition to a historic landmark structure. These concept drivers help to inform the three architectural massing concepts on the following page. The following concept drivers describe three distinctly different ways of creating a new, modern addition to a historic landmark structure. These concept drivers help to inform the three architectural massing concepts on the following page.



### CONCEPT 1: CONTINUITY

Projects employing this approach extend the historic language by referencing the materiality, rhythm, scale and proportion in the new addition. New additions are often deferential to the historic fabric.

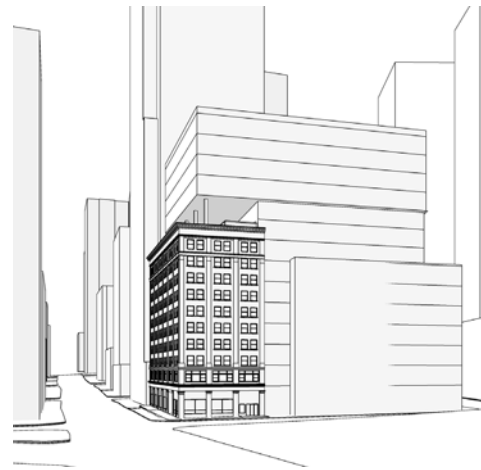
### CONCEPT 2: CONTRAST

Contrasting additions are clearly differentiated and often take an opposing position to the historic structure. Material choices, rhythm and scale are intentionally distinct as a means to amplify the differences between new and old.

### CONCEPT 3: HARMONY

Harmonious interventions employ thoughtful references to the historic structure without overt mimicry. They take a clear position on distinguishing new and old while taking clues from the historic materiality and rhythm. They stitch together new and old in a mutually beneficial manner.

# 06\_ARCHITECTURAL MASSING CONCEPTS [PREVIOUS STUDIES]



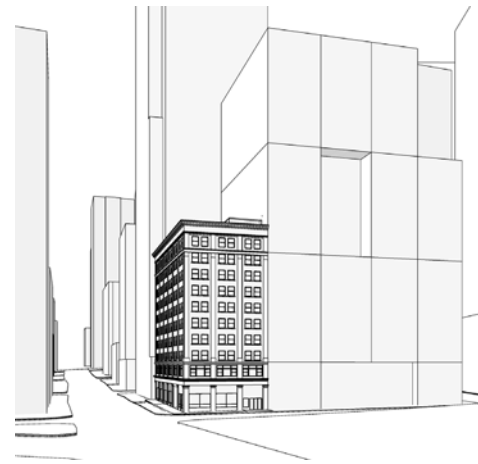
## 1. FLOATING VOLUME

### Pros:

- Creates covered roof terrace amenity area on Lloyd roof
- Lower overall tower height as a result of larger upper floor plates

### Cons:

- Unable to preserve Lloyd superstructure due to structural intervention



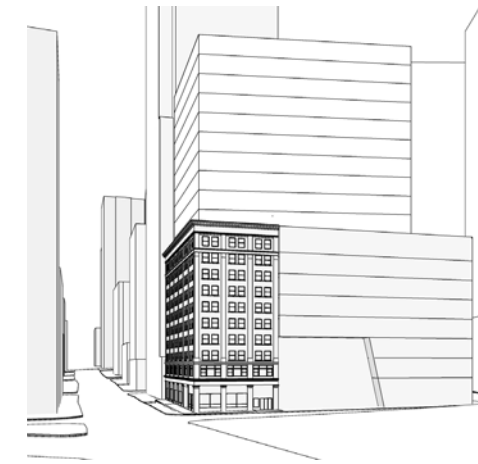
## 2. PUNCH

### Pros:

- Code Compliant facade modulation
- Sky terraces provide visual interest

### Cons:

- Scale does not speak to the pedestrian or to the Lloyd Bldg



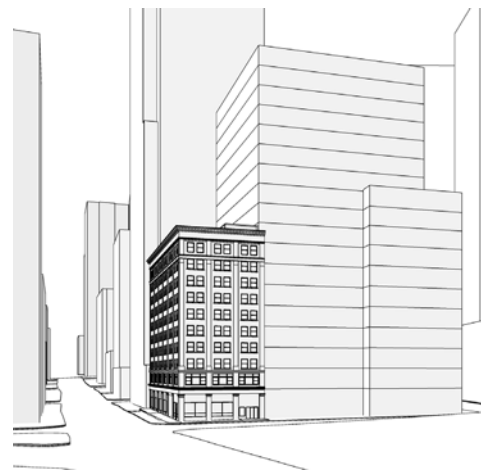
## 3. SHIFTED VOLUMES

### Pros:

- Large roof terrace with southern exposure

### Cons:

- Unable to preserve Lloyd superstructure due to structural intervention



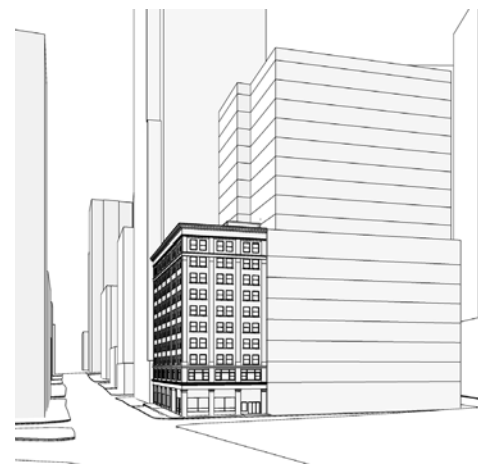
## 4. PROPORTIONAL TOWER

### Pros:

- Vertical Proportion is complimentary to Lloyd West facade proportion

### Cons:

- Scale does not speak to the pedestrian



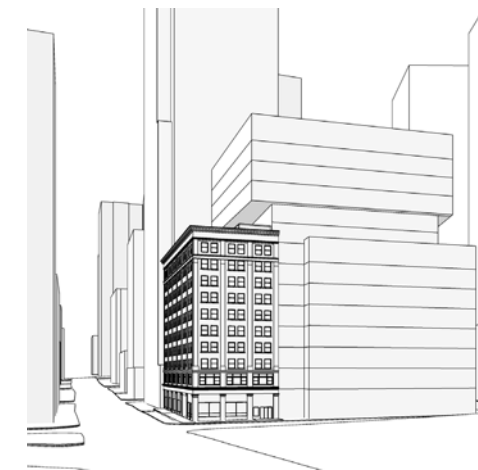
## 5. STEPPED TOWER

### Pros:

- Continuity of Lloyd height datum
- Podium expression speaks to the pedestrian scale

### Cons:

- No relief between historic Lloyd and new structure - challenging to clearly differentiate historic from new



## 6. CARVE

### Pros:

- Minimal cantilever is able to be accommodated without compromising Lloyd superstructure
- Reveal between historic and new helps to clearly differentiate

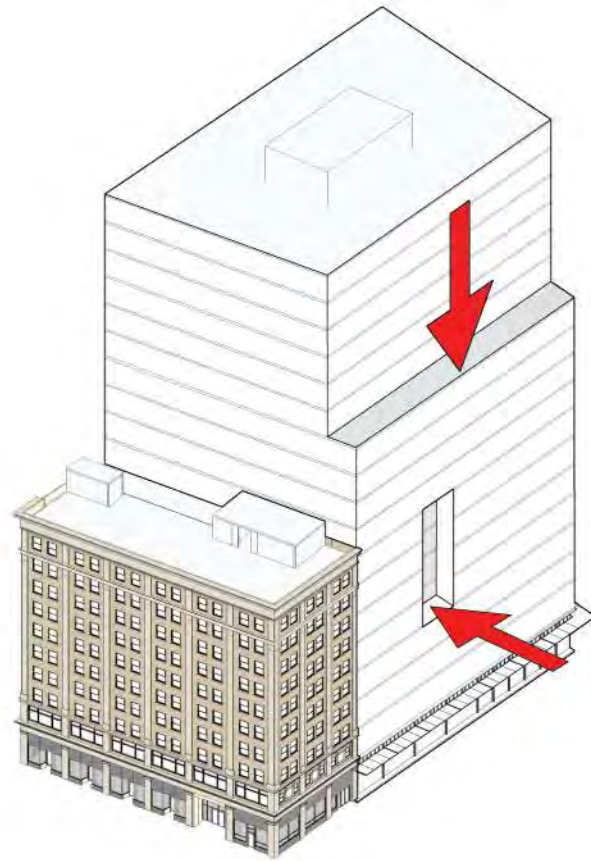
### Cons:

- Hovering mass visually distracts from prominence of Lloyd building

# 06\_ARCHITECTURAL MASSING CONCEPTS

CONCEPT 1 [Code Compliant]

## CONTINUITY



### OPPORTUNITIES:

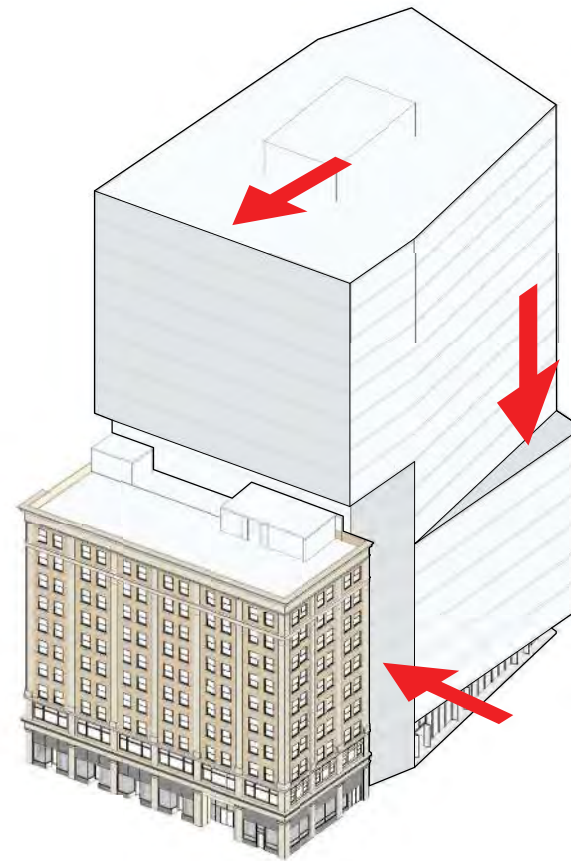
- Meets prescriptive zoning requirements
- Continuation of historic rhythm is sensitive to surrounding context

### CONSTRAINTS:

- Building Entry is not reinforced with massing
- Zoning prescribed step in building mass does not relate to Lloyd datums
- Scale transition between new and old is less responsive than other schemes
- Existing structural of historic building is demolished and replaced

CONCEPT 2

## CONTRAST



### OPPORTUNITIES:

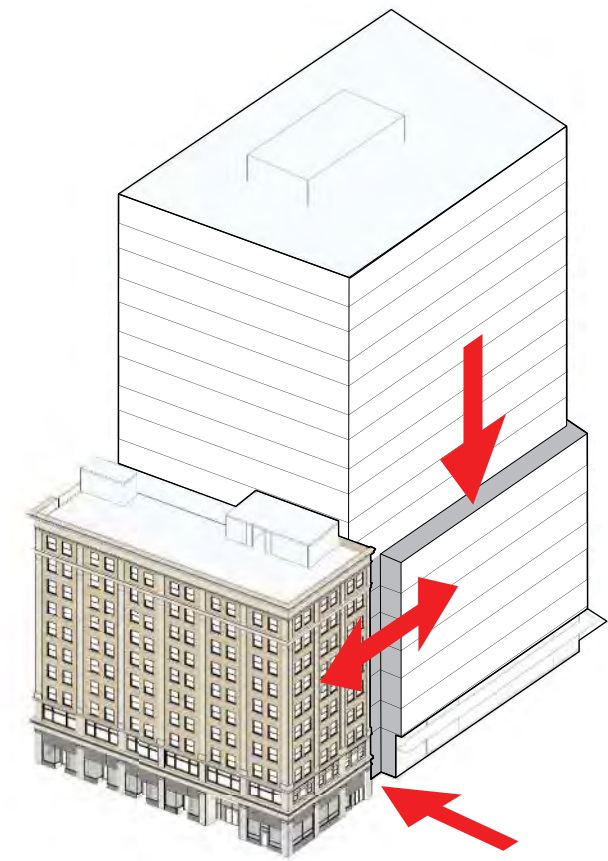
- Sculpted tower is clearly differentiated from historic Lloyd building
- Expressed podium picks up on key datums from Lloyd and responds to pedestrian scale
- Singular expression & sculptural form would provide a 'memorable sense of place'

### CONSTRAINTS:

- Cantilever over Lloyd building is not desirable from Landmarks
- Massing requires (1) departure for maximum facade length
- Ground floor requires (1) Type I Decision for loading and (1) Departure for Street Level Active Use

**PREFERRED:** CONCEPT 3

## HARMONY



### OPPORTUNITIES:

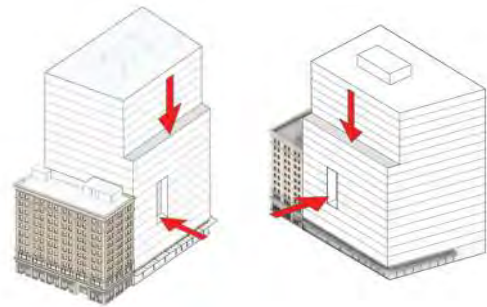
- Recessed gasket clearly differentiates historic from new
- Expressed podium picks up on key datums from Lloyd and responds to both historic context and pedestrian scale
- 'Spatial Stitch' would provide a 'memorable sense of place'
- Continuation of rhythm & texture at the podium is sensitive to surrounding context

### CONSTRAINTS:

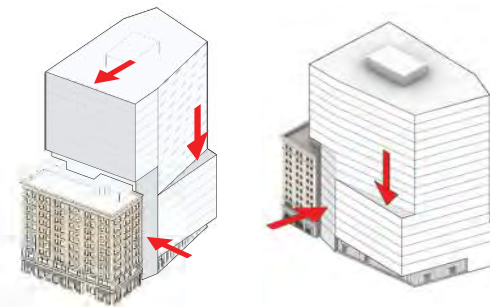
- Massing requires (1) departure for maximum facade length
- Ground floor requires (1) Type I Decision for loading and (1) Departure for Street Level Active Use

# 06\_ARCHITECTURAL MASSING CONCEPTS

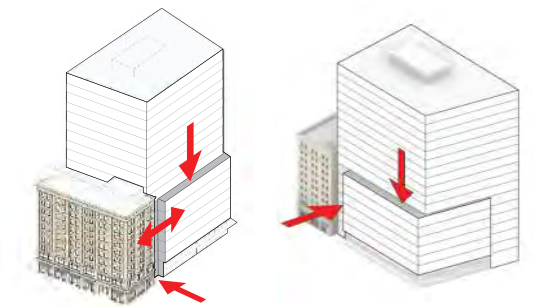
CONCEPT 1  
CONTINUITY



CONCEPT 2  
CONTRAST

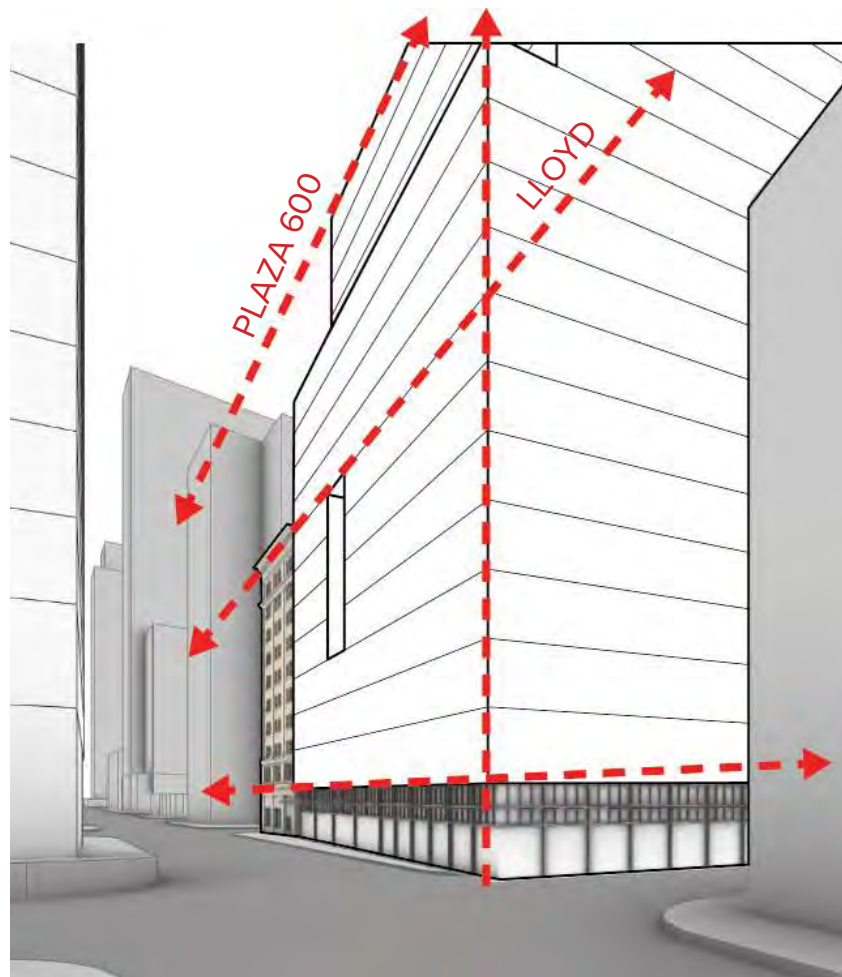


PREFERRED: CONCEPT 3  
HARMONY



## 06\_ CONCEPT I: CONTINUITY

Concept 1 continues the historic rhythm of the Lloyd Building facade, with a simple massing setback at the zoning code prescribed height of 160'. The scheme embraces a singular mass and building expression to establish a 2 part language on the site: historic and new. While clearly differentiating old from new, the continuation of the ground level storefront rhythm and facade depth creates a continuous and seamless pedestrian experience along 6th Ave. Separate lobbies are maintained with the historic Lloyd Building lobby off Stewart Street and the new tower lobby off of 6th Ave. The facade treatment of the new tower embraces a gridded expression with similar proportions and scale to the historic Lloyd facade.

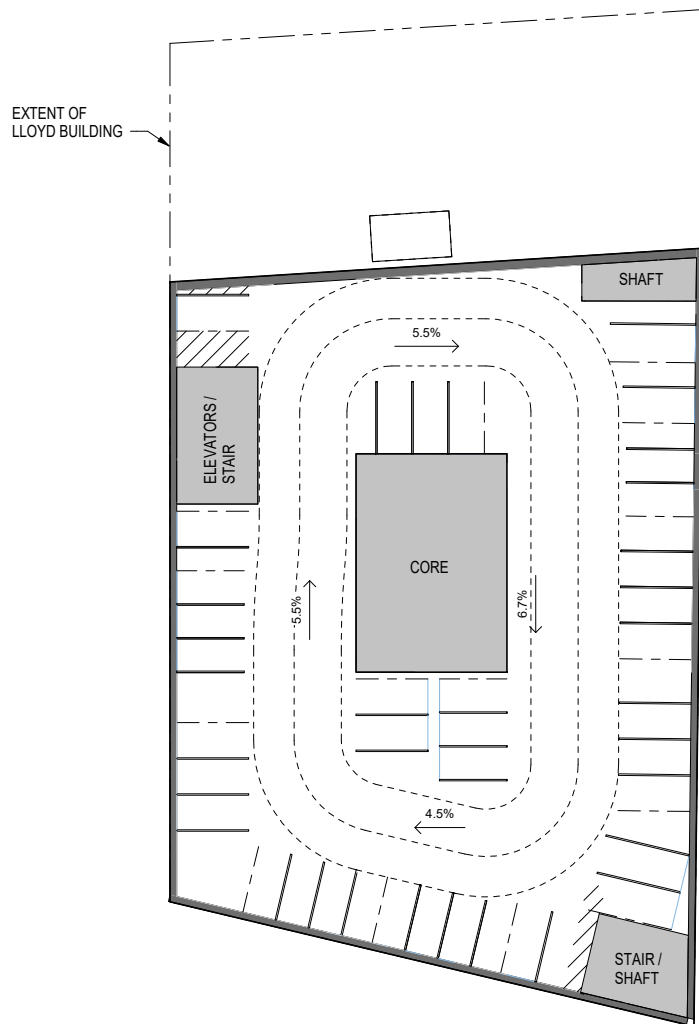


View of the NW corner (6th Avenue and Stewart Street)

# 06\_ CONCEPT I: CONTINUITY

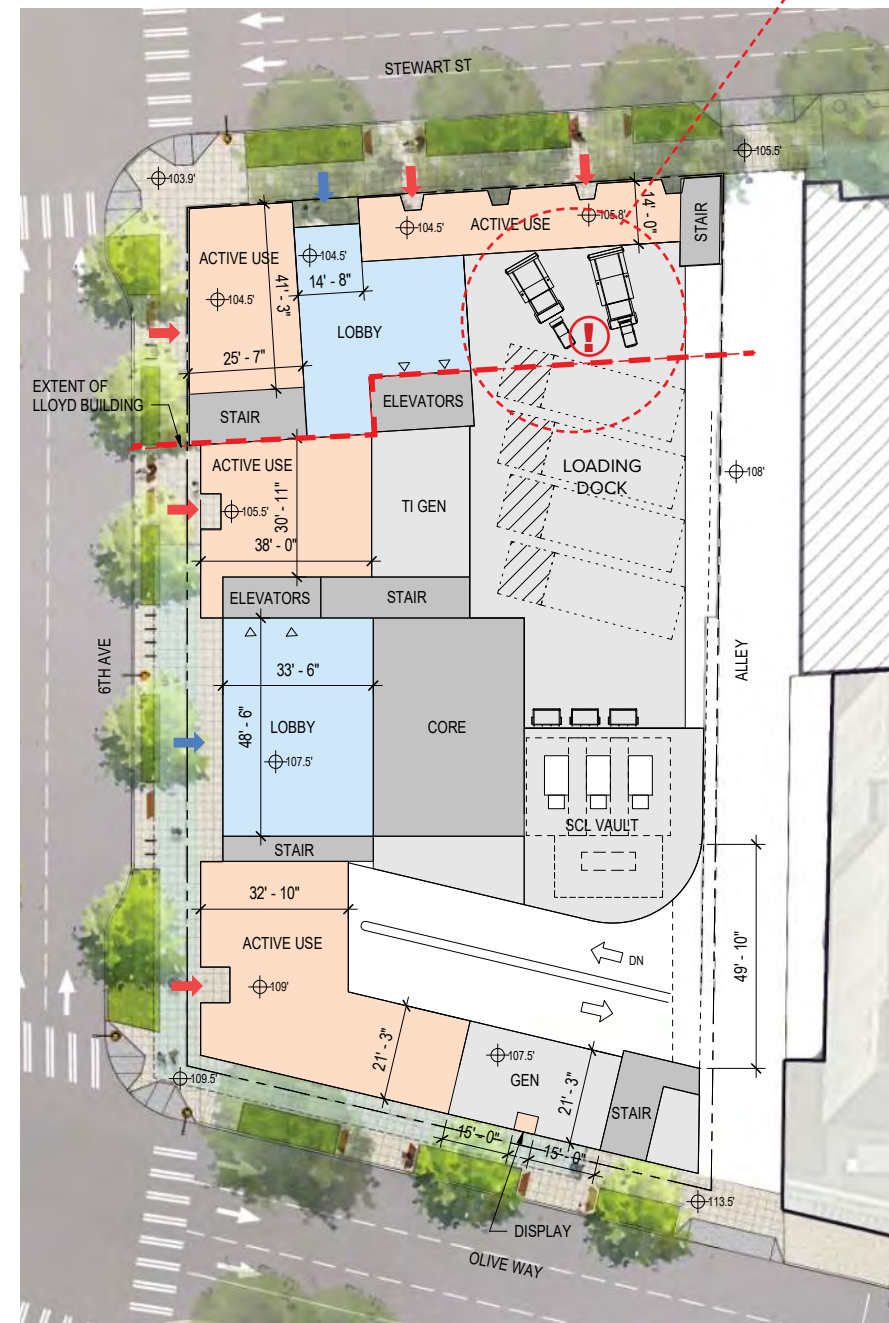
## This scheme assumes no land use code departures:

According to Land Use Code, the combined areas of historic Lloyd and the New Office tower dictate a code requirement of 4 loading berths at 35' long. The below study shows the impact of a code compliant loading dock scheme on the existing Lloyd building, as compared with the schemes requiring a Type I decision on the subsequent pages.



Typical Parking Plan

Portion of loading dock positioned within Lloyd Building necessitates demolition of Lloyd Building superstructure due to challenging floor to floor height and column placement



Ground Floor Plan



## COMMONALITIES BETWEEN ALL SCHEMES:

- Central building core for New Office Tower is necessary for lateral support of new tower and historic Lloyd Building, as well as for parking circulation below grade.
- Parking entry drive aisle is located to preserve Lloyd building superstructure, to work with sloping grades, and coordinated with new office tower superstructure
- SCL Vault and building generators are located at grade for ease of access and ventilation from the alley and drive aisle. Other locations were studied, and resulted in significantly more louver area along street frontages.

## OPPORTUNITIES:

- Meets prescriptive zoning requirements

## CONSTRAINTS:

- Required size of loading dock pushes into the existing Lloyd building floor plate, requiring the demolition of existing structural columns and floor plates in order to meet code required clearances. Ultimately this compromises the ability to preserve the Lloyd building superstructure and would result in a facade preservation project only (a 'facadectomy').
- Active Use retail along Stewart St. (Lloyd Bldg) has compromised depth and is not ideal from leasing standpoint
- Separate building lobbies (Lloyd building entry off of Stewart and New Office tower entry off of 6th Ave) creates wayfinding and building identity challenges

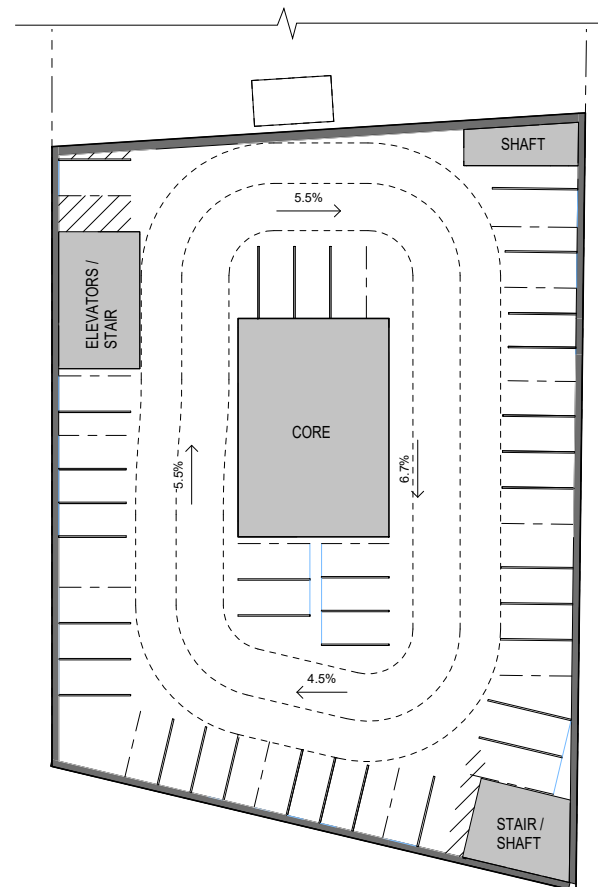
# 06\_CONCEPT I: CONTINUITY

This alternative scheme assumes the below land use code departures:

## TYPE I DECISION:

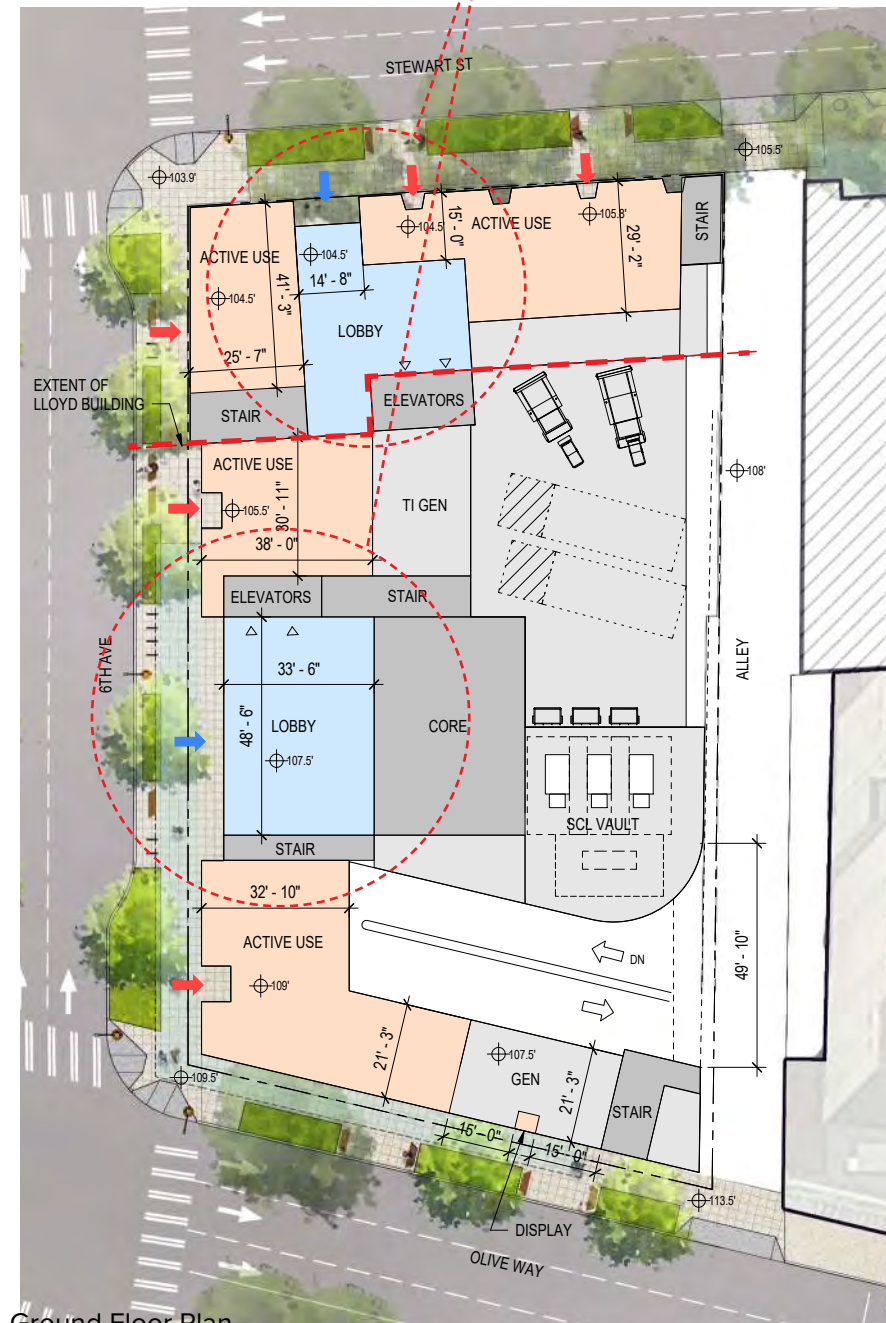
### 23.54.035.A Loading Berths Required

A Type I Loading Berth Analysis was submitted with PCD MUP and is under current review. Traffic and loading analysis has determined that 2 loading berths at 35' in length will be sufficient to service the proposed building program.



Typical Parking Plan

Separate lobbies for historic building and new tower at ground level



Ground Floor Plan



## COMMONALITIES BETWEEN ALL SCHEMES:

- Central building core for New Office Tower is necessary for lateral support of new tower and historic Lloyd Building, as well as for parking circulation below grade.
- Parking entry drive aisle is located to preserve Lloyd building superstructure, to work with sloping grades, and coordinated with new office tower superstructure
- SCL Vault and building generators are located at grade for ease of access and ventilation from the alley and drive aisle. Other locations were studied, and resulted in significantly more louver area along street frontages.

## OPPORTUNITIES:

- Loading dock footprint falls outside of existing Lloyd building, preserving the ability to restore and preserve the Lloyd building superstructure
- Retail spaces along street frontages are optimized for marketable lease depths

## CONSTRAINTS:

- Loading dock requires Type I decision for reduced loading dock berth quantity
- Separate building lobbies (Lloyd building entry off of Stewart and New Office tower entry off of 6th Ave) creates wayfinding and building identity challenges

## 06\_ CONCEPT I: CONTINUITY



View of storefront along 6th Avenue

# 06\_CONCEPT I: CONTINUITY

## UPPER LEVELS: DEPARTURE ANALYSIS

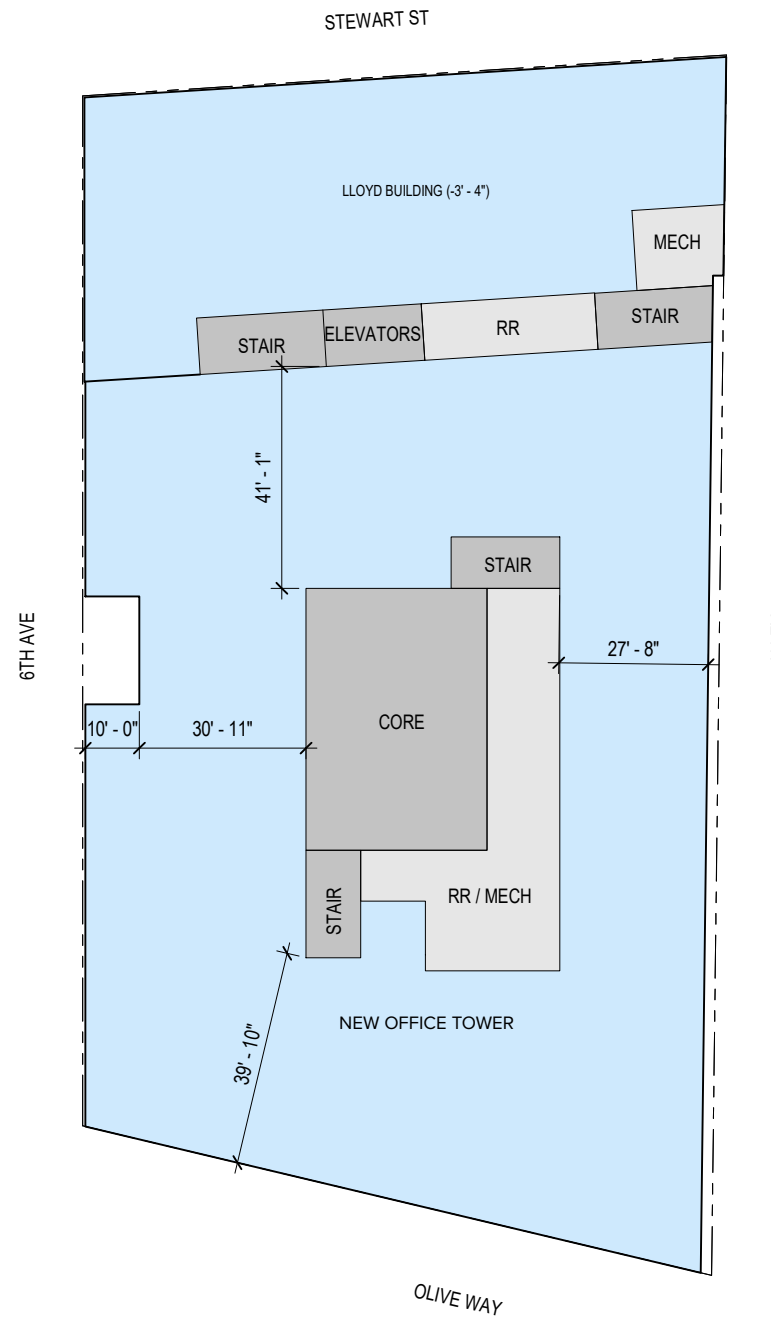
This scheme assumes no land use code departures.

### OPPORTUNITIES:

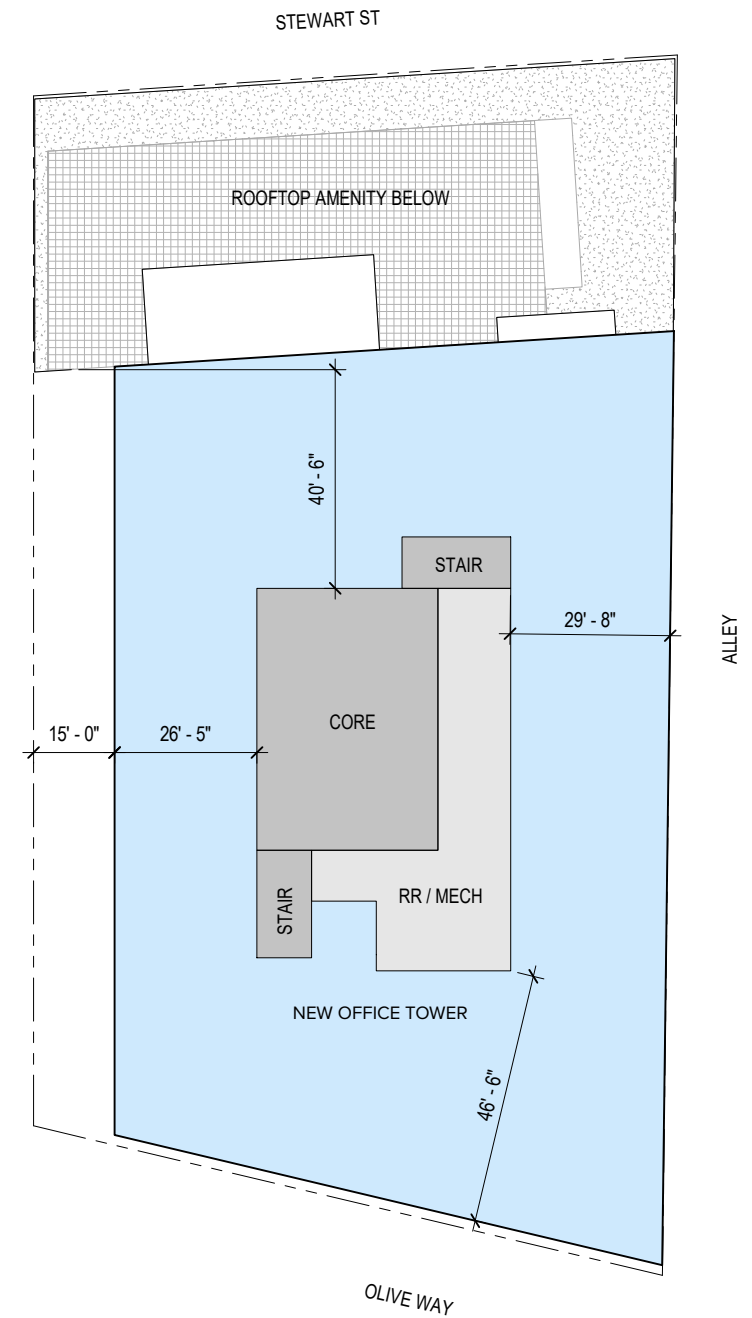
- Code compliant massing (no departures required)
- Since lobbies and elevators for the Lloyd building and the new office tower are distinct and separate, floor plates between old and new do not connect. Lloyd building is leased separately.

### CONSTRAINTS:

- Lloyd building rooftop amenity is not accessible to New Office Tower tenants (Lloyd building tenants only)
- Leasability of Lloyd building is compromised due to below-market floor to floor heights.



Typical Podium Floor Plan (3rd Floor)



Typical Tower Floor Plan (12th Floor)



# 06\_CONCEPT I: CONTINUITY



1. NE corner (Stewart Street and Alley)



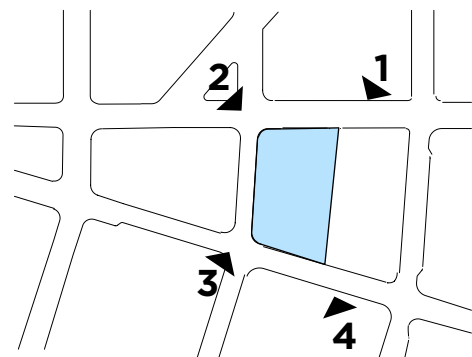
2. NW corner (6th Avenue and Stewart Street)



3. SW corner (6th Avenue and Olive Way)

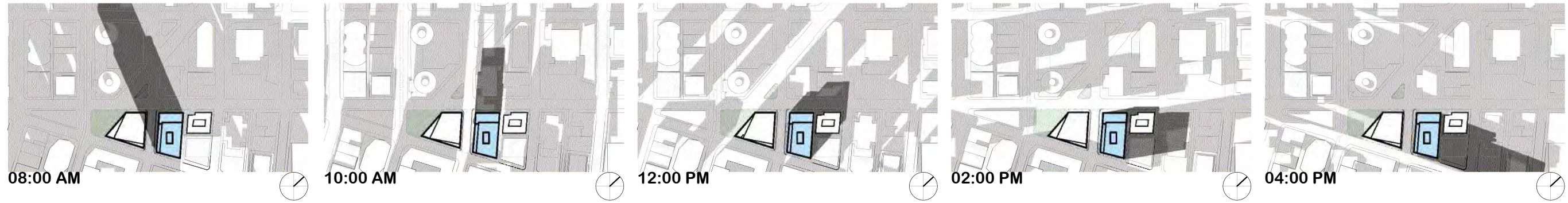


4. SE corner (Olive Way and Alley)

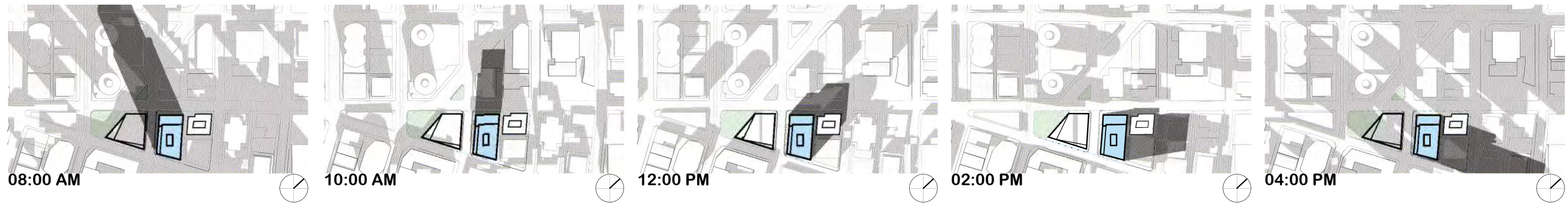


# 06\_ CONCEPT I: SHADOW STUDY

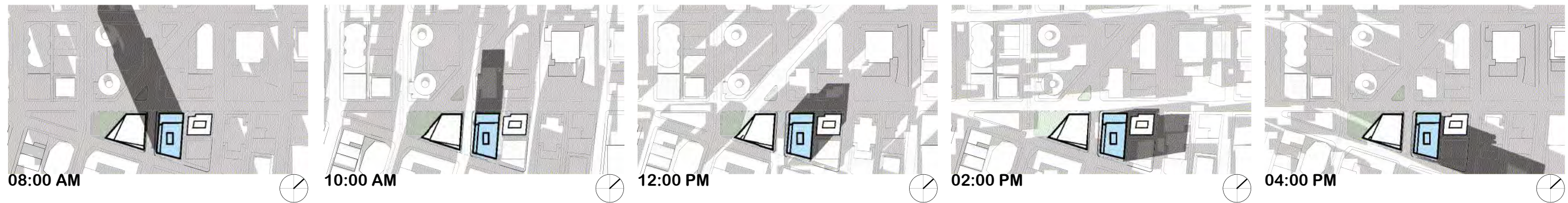
## VERNAL EQUINOX



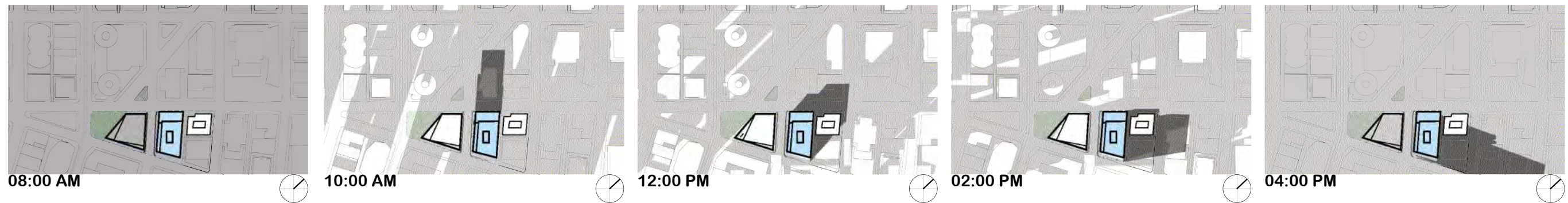
## SUMMER SOLSTICE



## AUTUMNAL EQUINOX

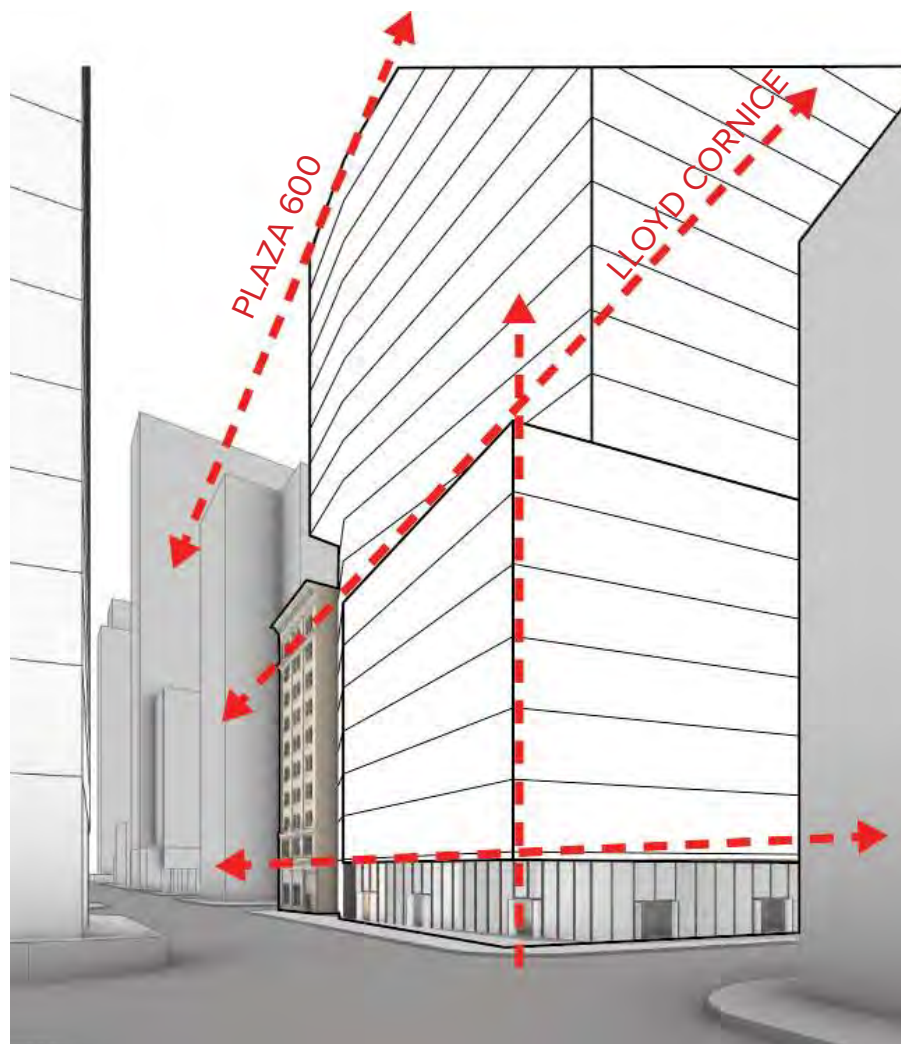


## WINTER SOLSTICE



## 06\_CONCEPT 2: CONTRAST

Concept 2 aims to provide a distinctly contrasting form from the rectilinear and simple form of the Lloyd Building. The faceted form of the new office tower responds to the subtle shift in the street grid, while also picking up on key datums from the historic Lloyd building facade to inform the massing shifts. The dynamic tower cantilevers over the roof of the historic structure- creating a spatial interlock between the forms. This scheme explores a combined lobby and shared core services between the two structures.



View of the NW corner (6th Avenue and Stewart Street)

# 06\_CONCEPT 2: CONTRAST

This scheme assumes the below land use code departures:

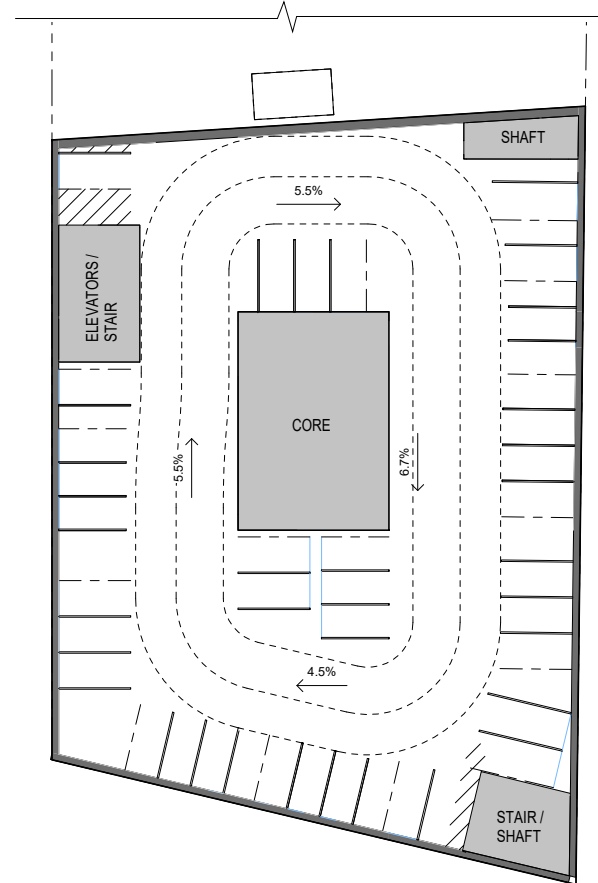
## 23.49.009 Street Level Use

In this scheme, the New Office Tower lobby is on the corner of 6th Avenue and Olive Way. This reduces the active use program along Olive Way and triggers a departure for Street Level Use.

## TYPE I DECISION:

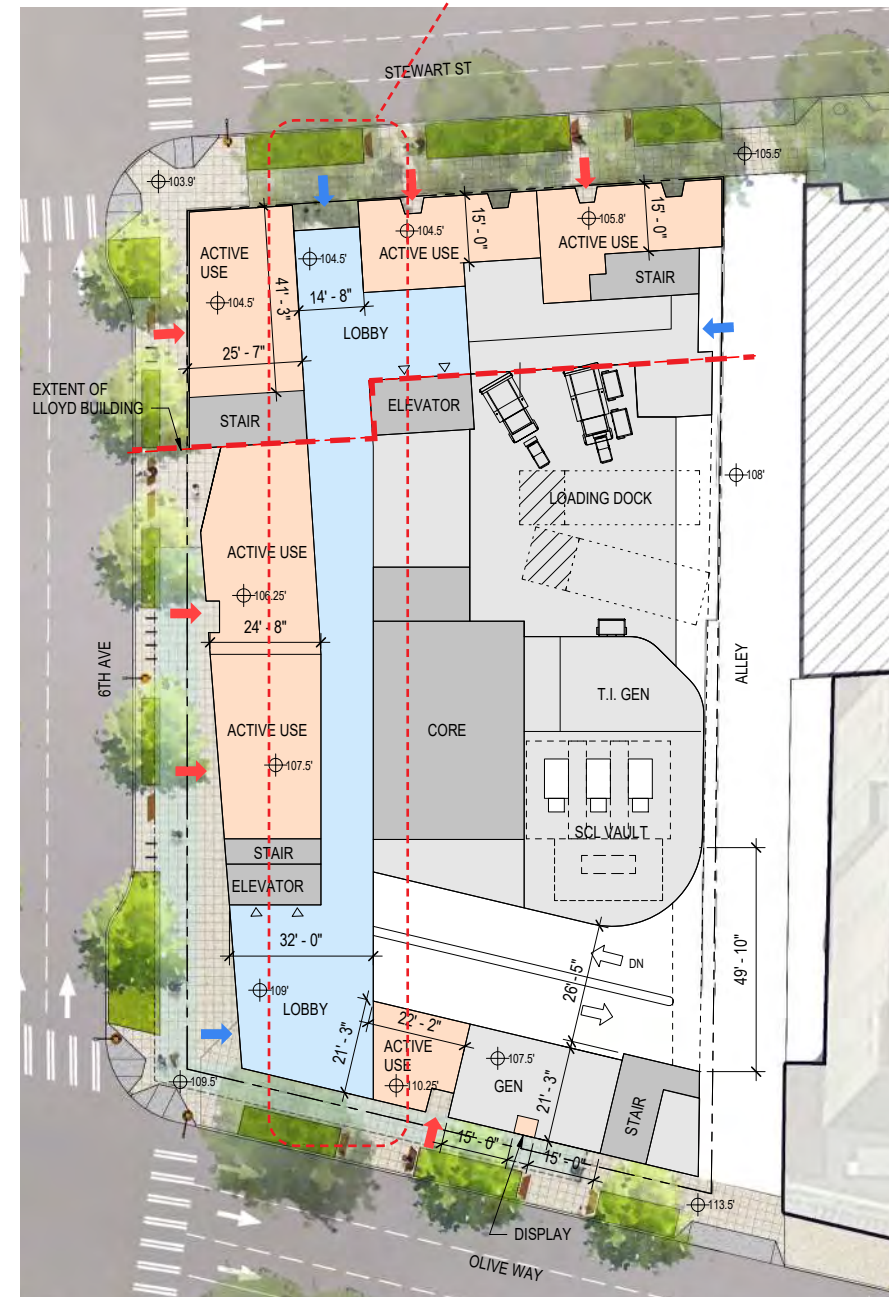
### 23.54.035.A Loading Berths Required

A Type I Loading Berth Analysis was submitted with PCD MUP and is under current review. Traffic and loading analysis has determined that 2 loading berths at 35' in length will be sufficient to service the proposed building program.



Typical Parking Plan

Connected lobbies for historic building and new tower at ground level



Ground Floor Plan

## COMMONALITIES BETWEEN ALL SCHEMES:

- Central building core for New Office Tower is necessary for lateral support of new tower and historic Lloyd Building, as well as for parking circulation below grade.
- Parking entry drive aisle is located to preserve Lloyd building superstructure, to work with sloping grades, and coordinated with new office tower superstructure
- SCL Vault and building generators are located at grade for ease of access and ventilation from the alley and drive aisle. Other locations were studied, and resulted in significantly more louver area along street frontages.

## OPPORTUNITIES:

- Loading dock footprint falls outside of existing Lloyd building, preserving the ability to restore and preserve the Lloyd building superstructure
- Retail spaces along street frontages are optimized for marketable lease depths
- Placement of new office tower building lobby at the corner of 6th and Olive 'reinforces building entry'

## CONSTRAINTS:

- Loading dock requires Type I decision for reduced loading dock berth quantity
- Active Use retail sizes and depths are not ideal from a leasing standpoint
- Building entries and lobbies of the Lloyd Building and New Office tower are connected, but distant with long pedestrian walk times between the two and ramping to accommodate differences in grade.
- Due to the lobby functions at 6th and Olive, a street level use departure may be required for 6th Avenue and Olive Way



## 06\_ CONCEPT 2: CONTRAST



View of storefront along 6th Avenue

# 06\_CONCEPT 2: CONTRAST

## UPPER LEVELS: DEPARTURE ANALYSIS

This scheme assumes the below land use code departures:

### 23.49.058.B.2 Upper Façade Modulation

This tower scheme embraces a sculptural, singular expression that meets the intent of this code section by minimizing perceived bulk and mass through sculptural form.

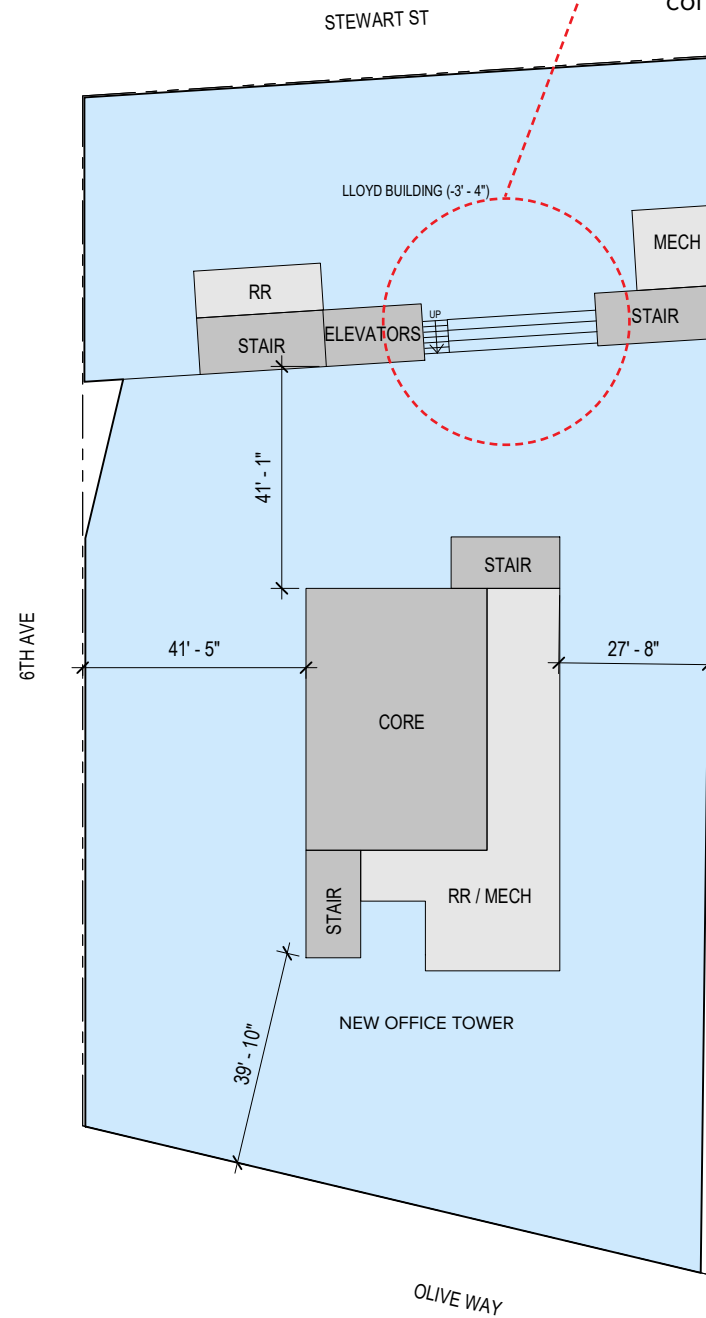
### OPPORTUNITIES:

- Internal connection between the Lloyd and New Office Tower floor plates increases function and use of Lloyd Building
- Lloyd Building rooftop amenity is accessible to all building occupants
- By tying the Lloyd Building and New Office Structures together into one building, building infrastructure and support can be primarily located within the New Office Tower, freeing up the Lloyd floor plate and facade for more program space.

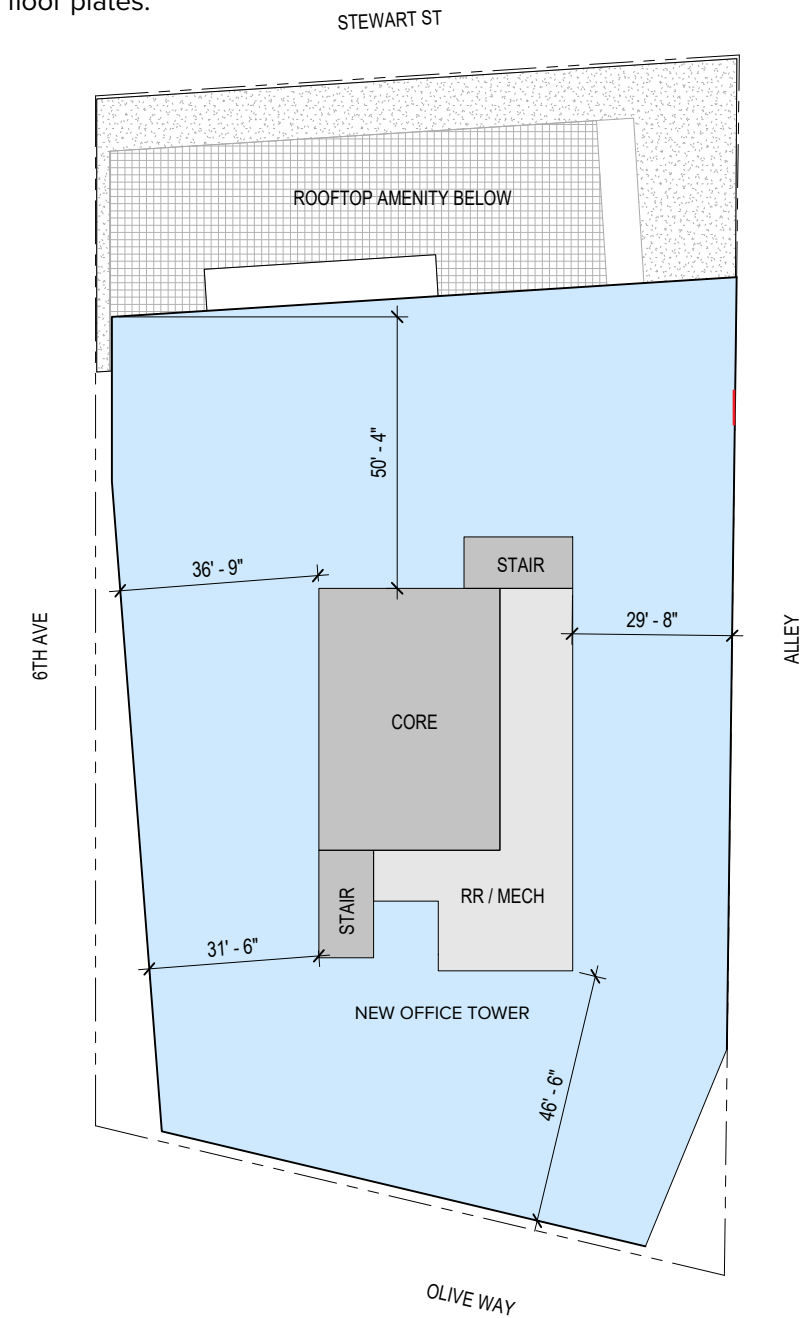
### CONSTRAINTS:

- Upper Level Façade Modulation Departure is required.

Separate cores serve historic and new buildings with centralized connection between floor plates.



Typical Podium Floor Plan (3rd Floor)



Typical Tower Floor Plan (12th Floor)



# 06\_CONCEPT 2: CONTRAST



1. NE corner (Stewart Street and Alley)



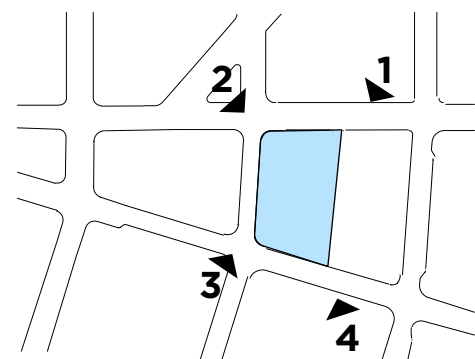
2. NW corner (6th Avenue and Stewart Street)



3. SW corner (6th Avenue and Olive Way)



4. SE corner (Olive Way and Alley)

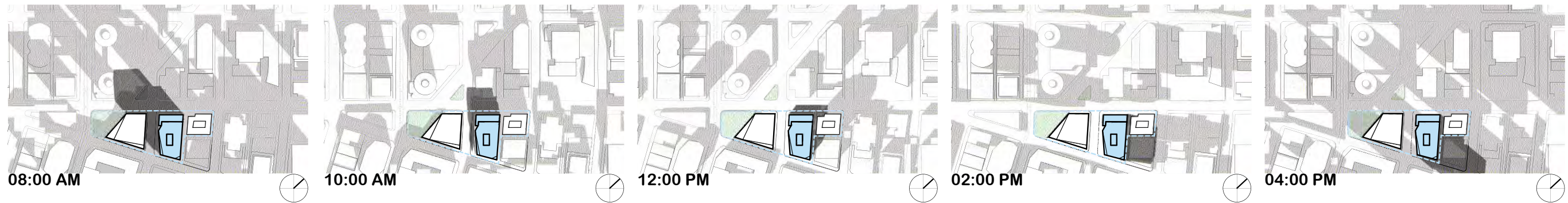


# 06\_CONCEPT 2: SHADOW STUDY

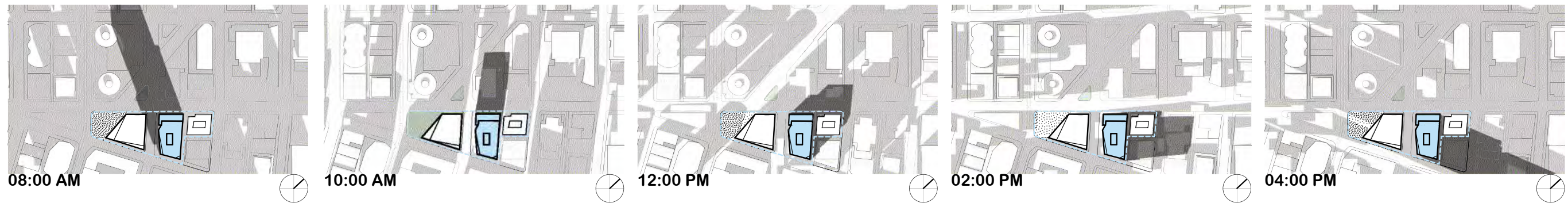
## VERNAL EQUINOX



## SUMMER SOLSTICE



## AUTUMNAL EQUINOX



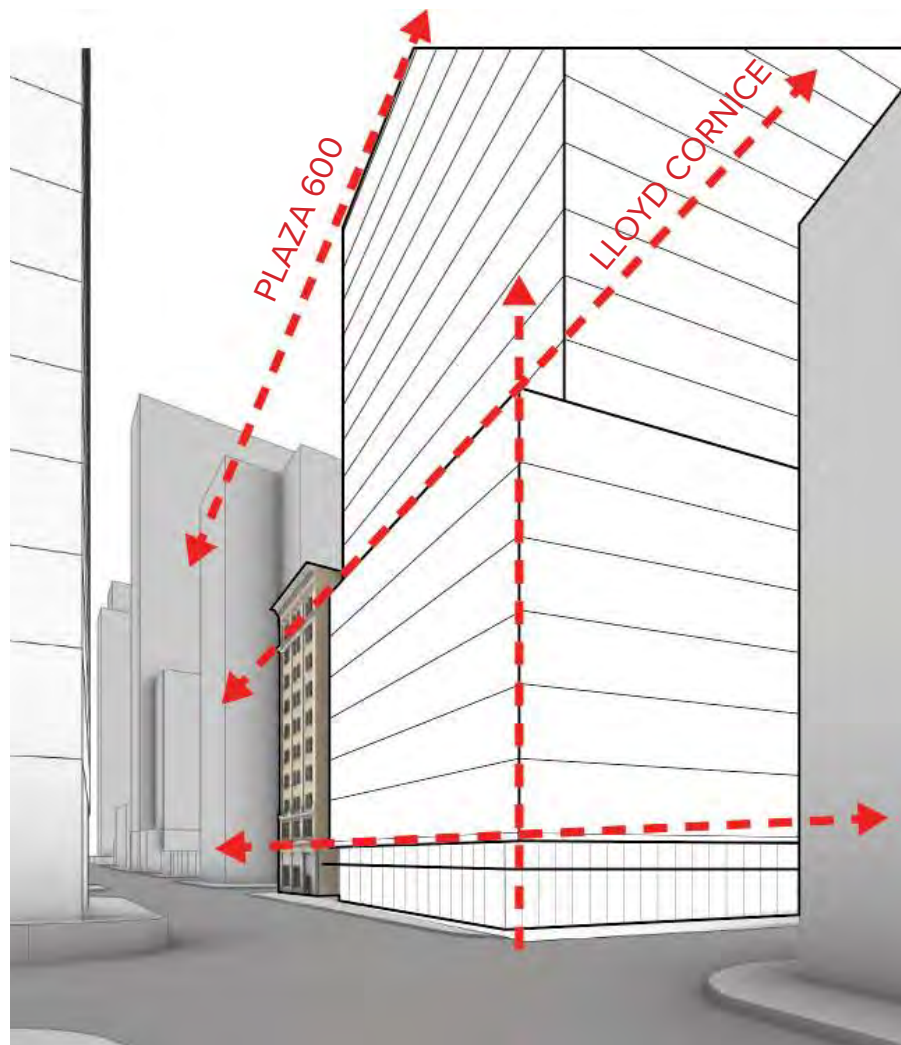
## WINTER SOLSTICE



PREFERRED

## 06\_CONCEPT 3: HARMONY

The preferred concept harmoniously balances the new office tower building mass with the historic Lloyd Building by setting back the upper tower along a key datum of the historic facade, and allowing an expressed podium to pick up on the texture and pedestrian scale of the historic context. The podium mass is separated from the Lloyd building structure by a recessed gasket, which demarcates the building lobby and highlights the 'spatial stitch' between the two structures. The proximity and convergence of the two lobbies is celebrated at the gasket with a double height space.



View of the NW corner (6th Avenue and Stewart Street)

PREFERRED

## 06\_CONCEPT 3: HARMONY

This scheme assumes the below land use code departures:

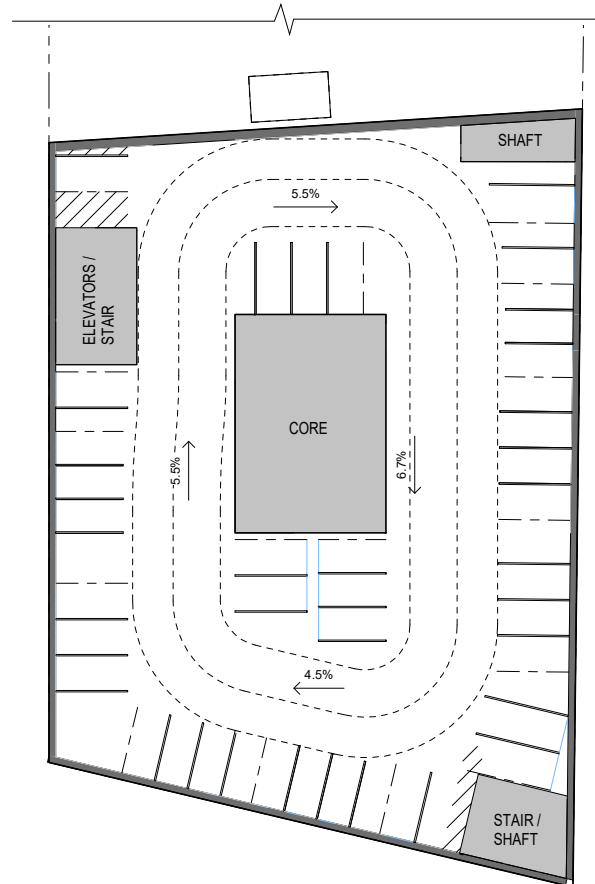
### 23.49.009 Street Level Use

In this preferred scheme, the New Office Tower lobby is adjacent to the historic Lloyd structure within the 'gasket'. This, in combination with required exiting, reduces the active use program along 6th Avenue to just below the 75% minimum threshold.

### TYPE I DECISION:

### 23.54.035.A Loading Berths Required

A Type I Loading Berth Analysis was submitted with PCD MUP and is under current review. Traffic and loading analysis has determined that 2 loading berths at 35' in length will be sufficient to service the proposed building program.



Typical Parking Plan

Connected lobbies for historic building and new tower in centralized space



Ground Floor Plan

### COMMONALITIES BETWEEN ALL SCHEMES:

- Central building core for New Office Tower is necessary for lateral support of new tower and historic Lloyd Building, as well as for parking circulation below grade.
- Parking entry drive aisle is located to preserve Lloyd building superstructure, to work with sloping grades, and coordinated with new office tower superstructure
- SCL Vault and building generators are located at grade for ease of access and ventilation from the alley and drive aisle. Other locations were studied, and resulted in significantly more louver area along street frontages.

### OPPORTUNITIES:

- Loading dock footprint falls outside of existing Lloyd building, preserving the ability to restore and preserve the Lloyd building superstructure
- Retail spaces along street frontages are optimized for marketable lease depths
- Placement of new office tower building lobby at the recessed gasket between historic Lloyd and the New Office Tower 'reinforces building entry' and celebrates the connection between new and old

### CONSTRAINTS:

- Loading dock requires Type I decision for reduced loading dock berth quantity
- Due to lobby functions long 6th Avenue, a Street Level Use departure may be required.



PREFERRED

## 06\_ CONCEPT 3: HARMONY



View of storefront along 6th Avenue

PREFERRED

## 06\_CONCEPT 3: HARMONY

### UPPER LEVELS: DEPARTURE ANALYSIS

This scheme assumes the below land use code departures:

#### 23.49.058.B.2 Upper Façade Modulation

Although the existing Lloyd building will read as a separate structure from the exterior, it is considered one building and one facade according to land use code and therefore the entire length of the development exceeds the code maximum facade length without prescriptive modulation.

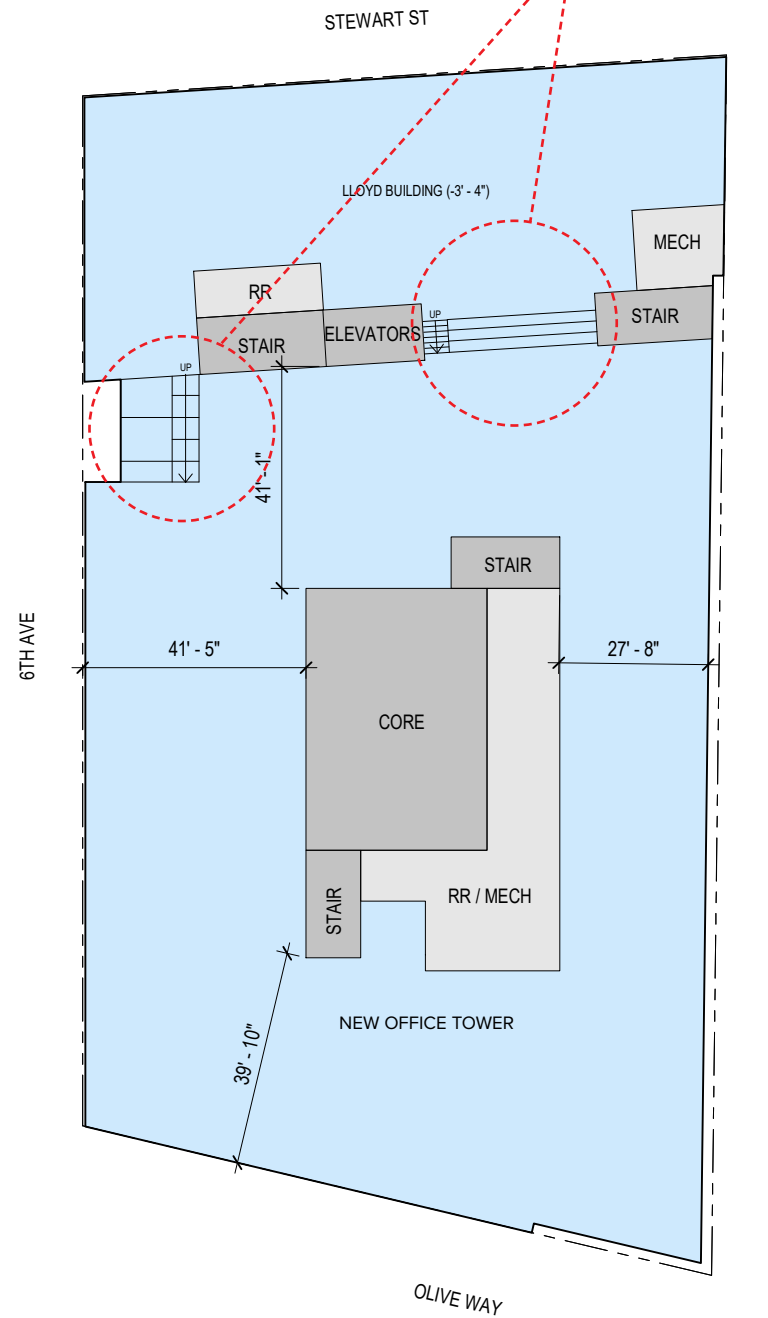
#### OPPORTUNITIES:

- Internal connection between the Lloyd and New Office Tower floor plates increases function and use of Lloyd Building and is celebrated by being visible to the pedestrian from 6th Avenue
- Lloyd Building rooftop amenity is accessible to all building occupants
- By tying the Lloyd Building and New Office Structures together into one building, building infrastructure and support can be primarily located within the New Office Tower, freeing up the Lloyd floor plate and facade for more program space.
- Simple form of New Office Tower provides a backdrop to the historic and textural Lloyd building facade, allowing the historic structure to remain the prominent feature of the site.

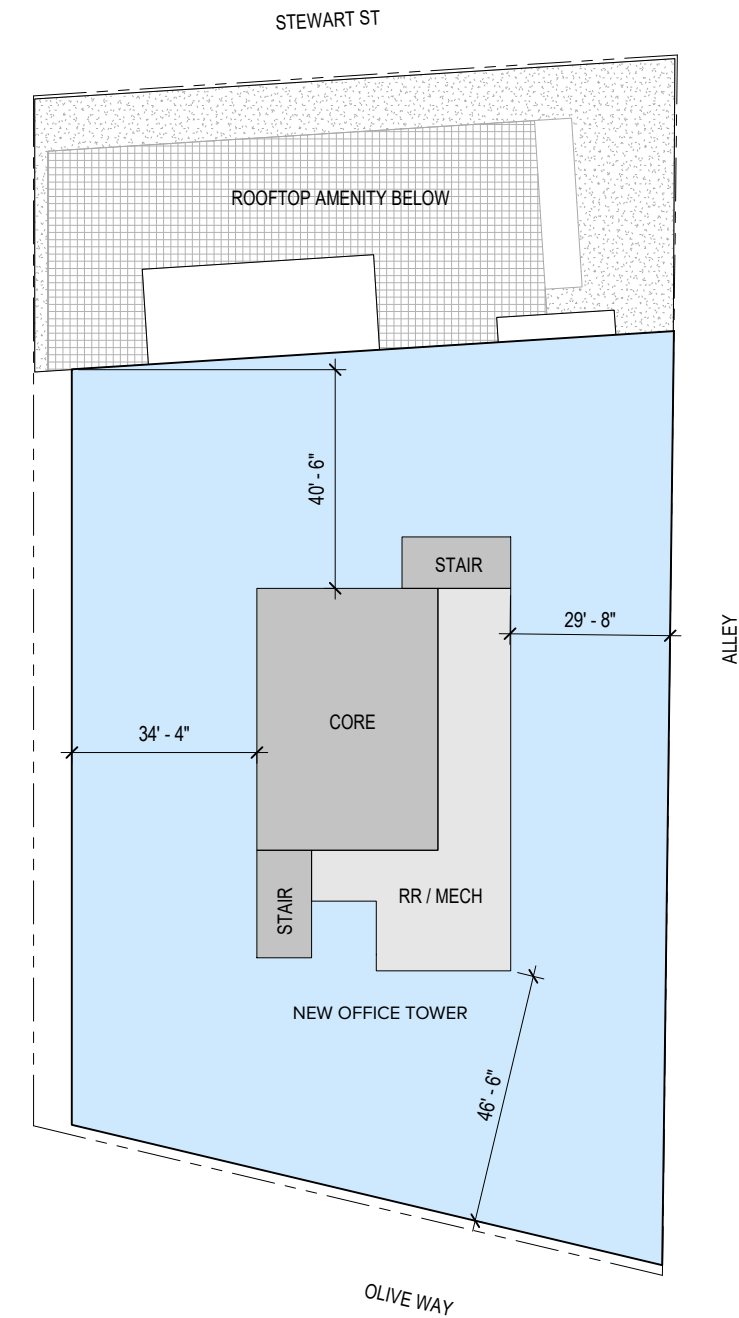
#### CONSTRAINTS:

- Upper Level Façade Modulation Departure is required.

Separate cores serve historic and new buildings with multiple connections between floor plates.



Typical Podium Floor Plan (3rd Floor)



Typical Tower Floor Plan (12th Floor)



PREFERRED

# 06\_CONCEPT 3: HARMONY



1. NE corner (Stewart Street and Alley)



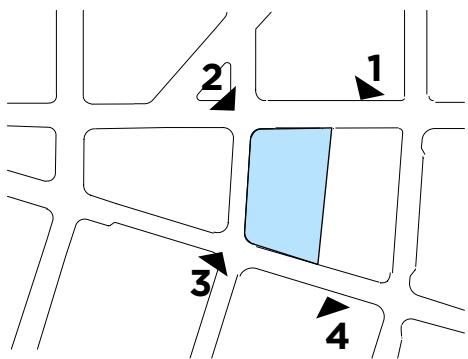
2. NW corner (6th Avenue and Stewart Street)



3. SW corner (6th Avenue and Olive Way)



4. SE corner (Olive Way and Alley)

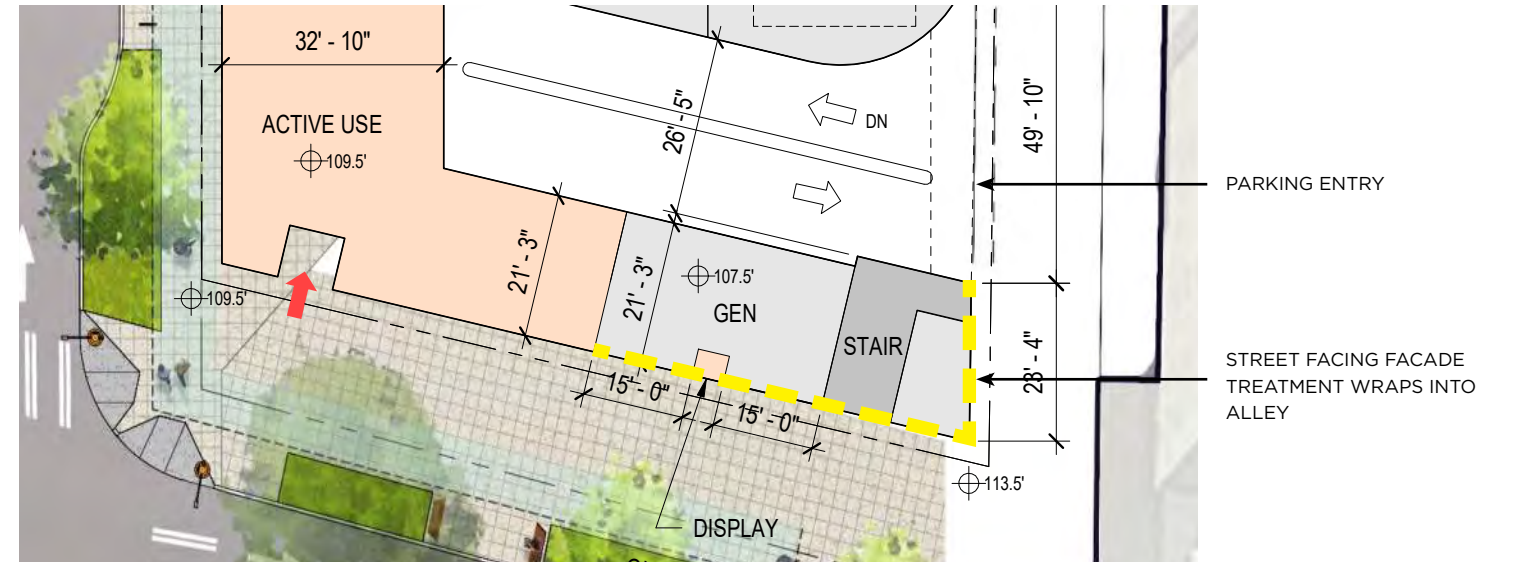
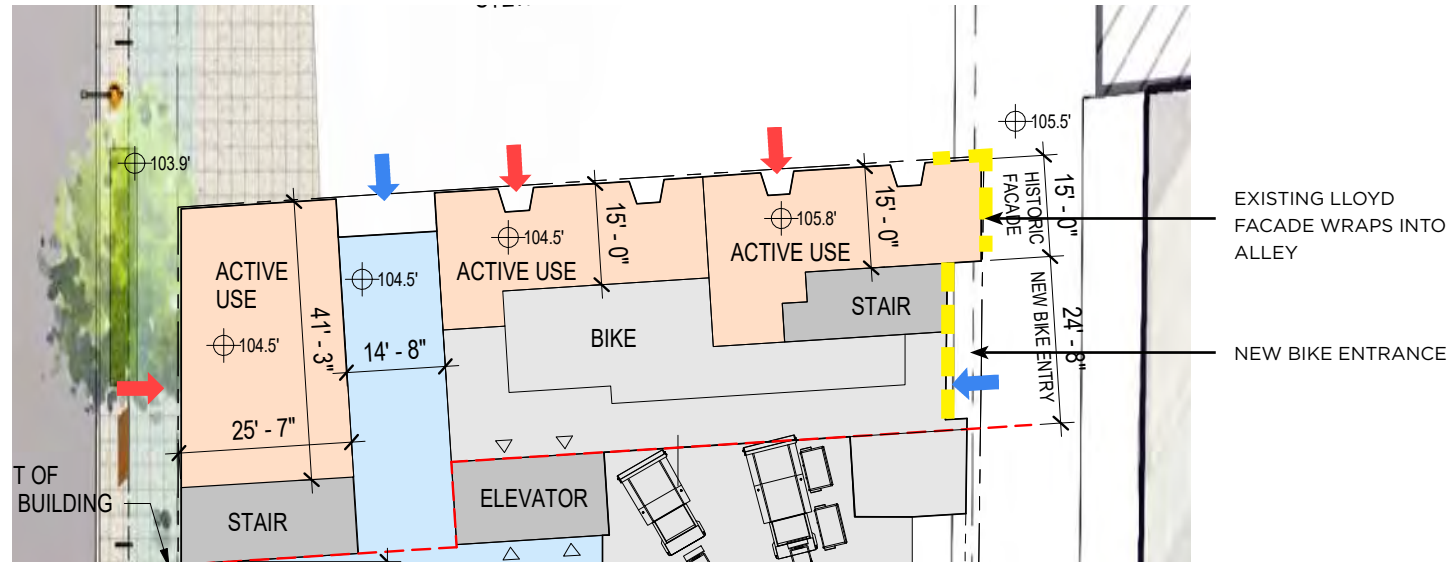


 NORTH

PREFERRED

# 06\_CONCEPT 3: HARMONY

## ALLEY TREATMENT



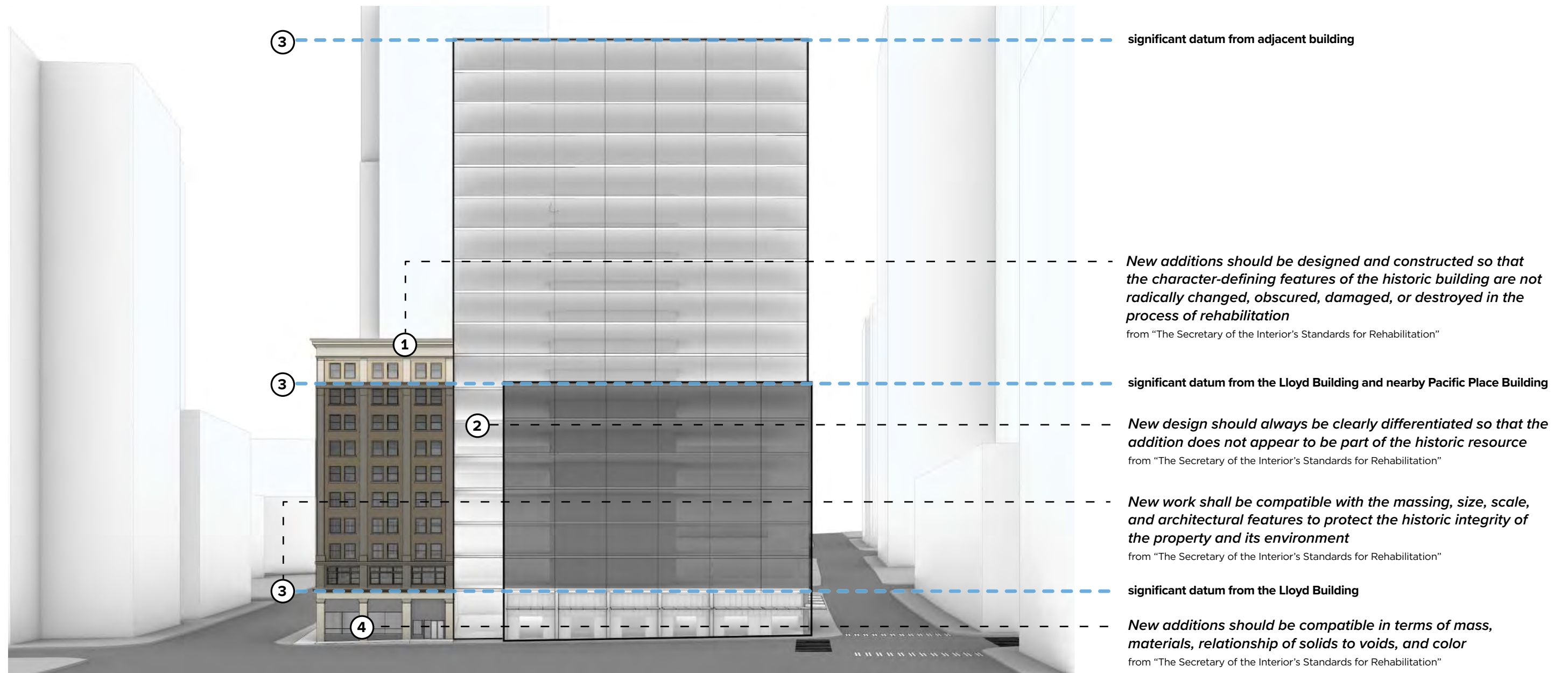
NORTH ALLEY PERSPECTIVE

SOUTH ALLEY PERSPECTIVE

## 06\_CONCEPT 3: HARMONY

- ① Character-defining features of the historic Lloyd shall be preserved
- ② New design will be thoughtfully differentiated from the old
- ③ The design proposal will reference key datums from the Lloyd Building facade and respond to the scale of adjacent context buildings, such as Pacific Place and 600 Stewart St.
- ④ The new building form and expression will be compatible with the Lloyd Building and surrounding urban context

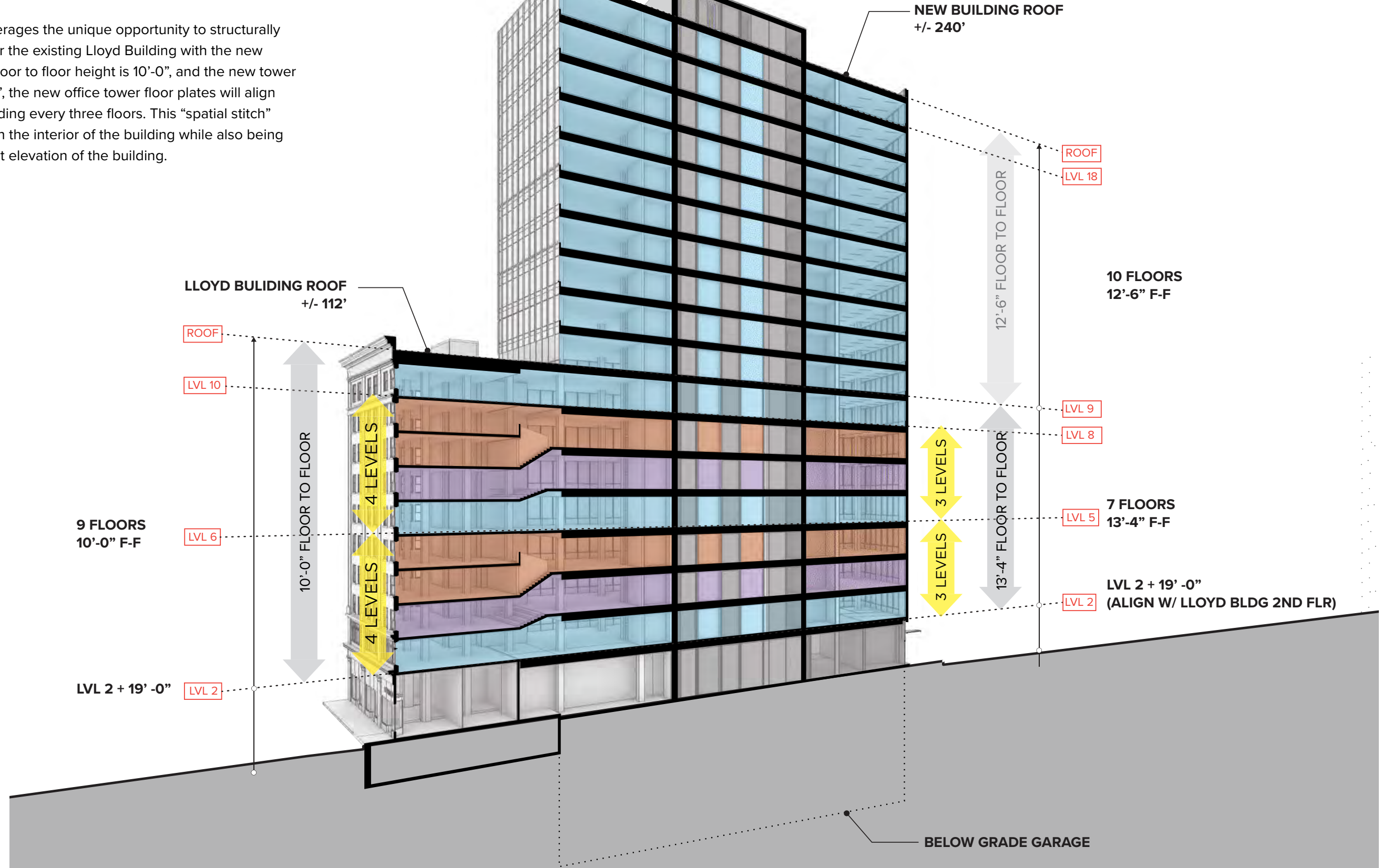
### EXCERPTS FROM THE SECRETARY OF THE INTERIOR'S STANDARDS FOR REHABILITATION



PREFERRED

## 06\_ CONCEPT 3: HARMONY

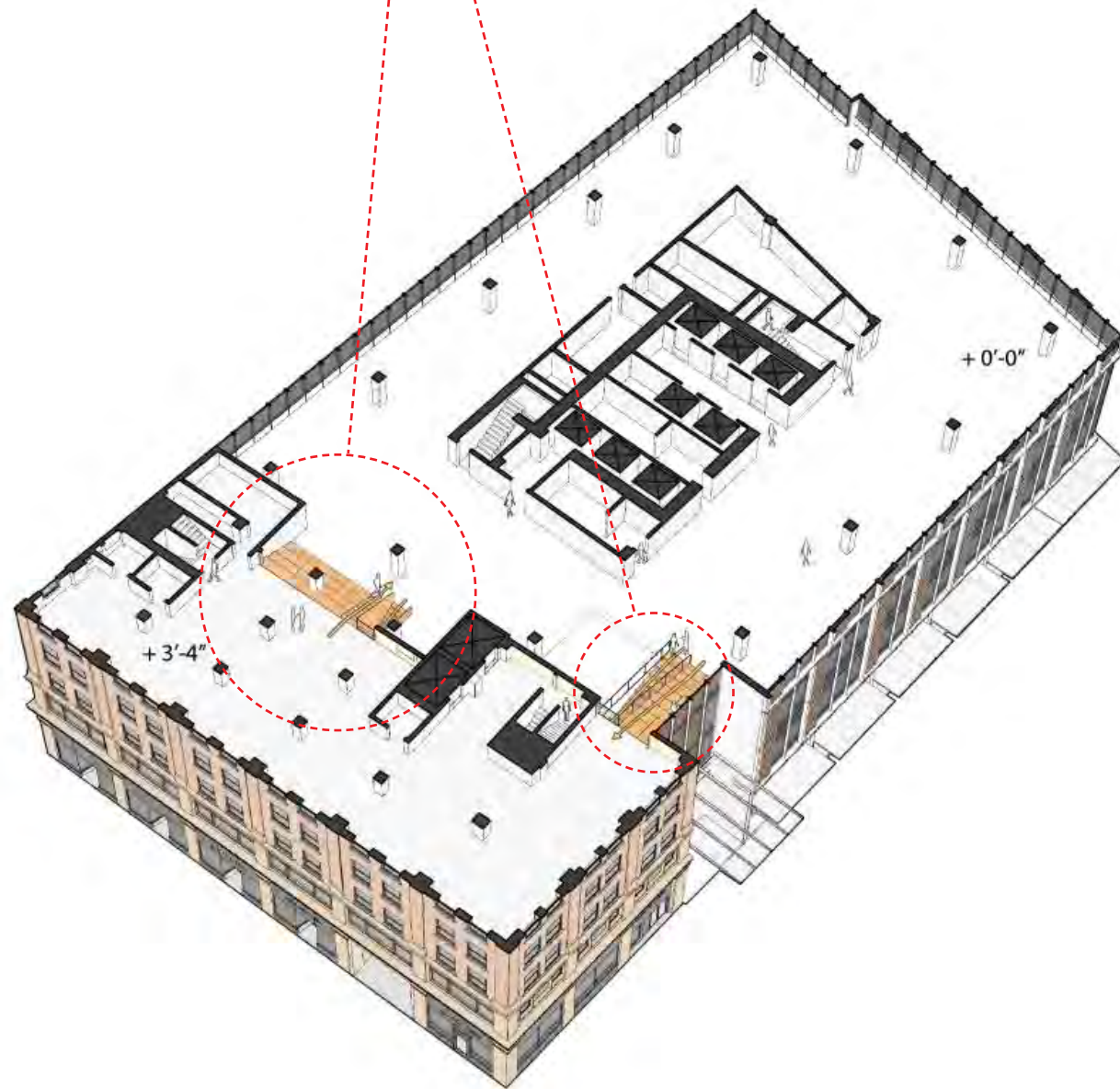
The preferred concept leverages the unique opportunity to structurally and spatially stitch together the existing Lloyd Building with the new tower. The Lloyd building floor to floor height is 10'-0", and the new tower floor to floor height is 13'-4", the new office tower floor plates will align with the Historic Lloyd Building every three floors. This "spatial stitch" condition is experienced on the interior of the building while also being visually evident on the west elevation of the building.



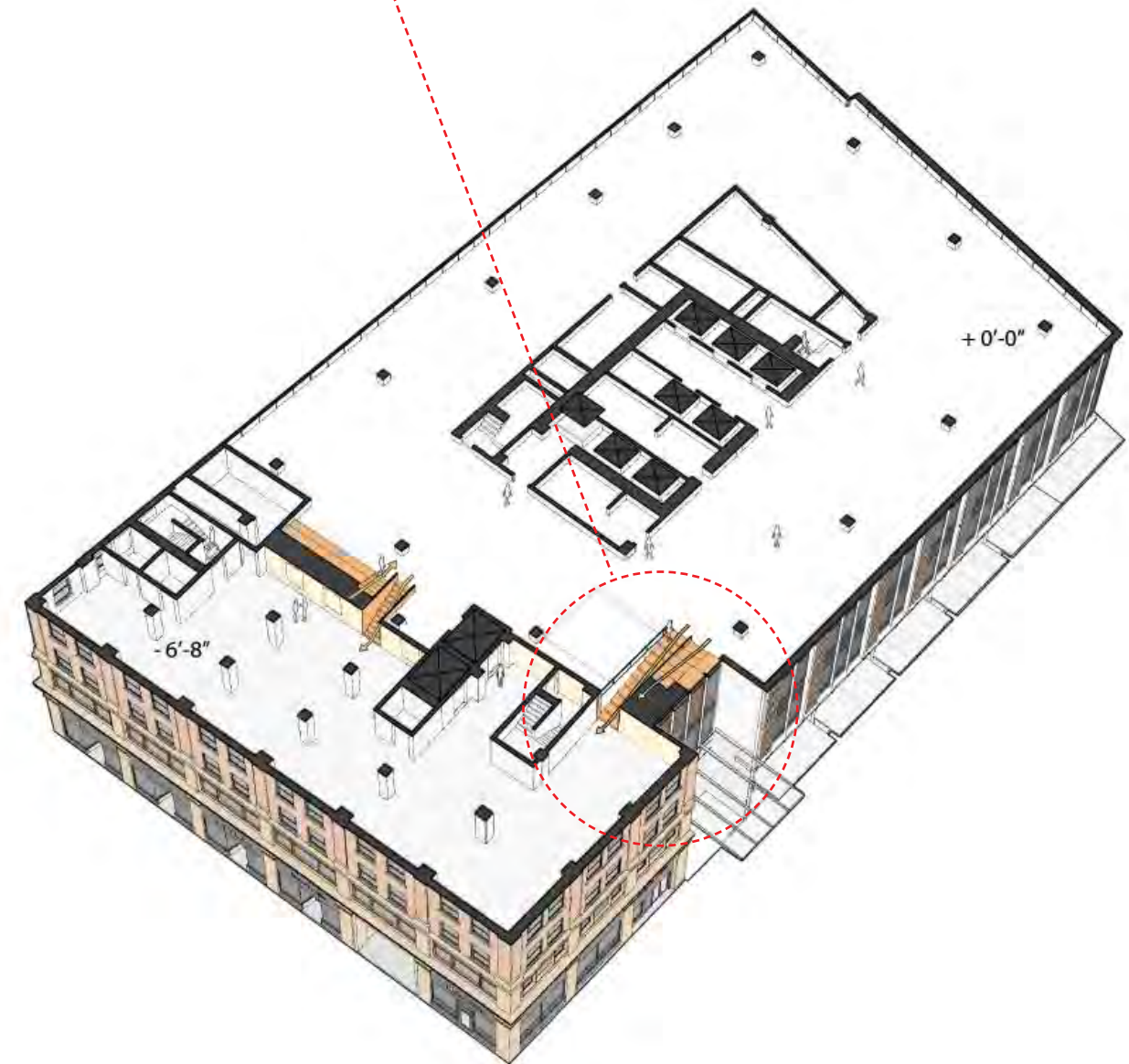
PREFERRED

## 06\_CONCEPT 3: HARMONY

Separate cores serve historic and new buildings with multiple connections between floor plates.

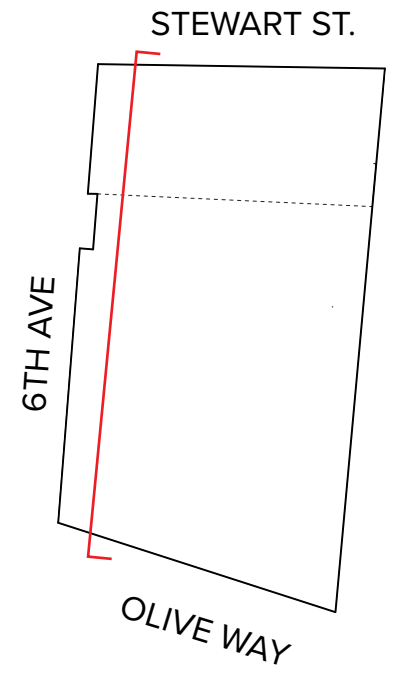


Western "Stitch" is visible on 6th Ave facade and communicates the relationship between buildings to the public realm



PREFERRED

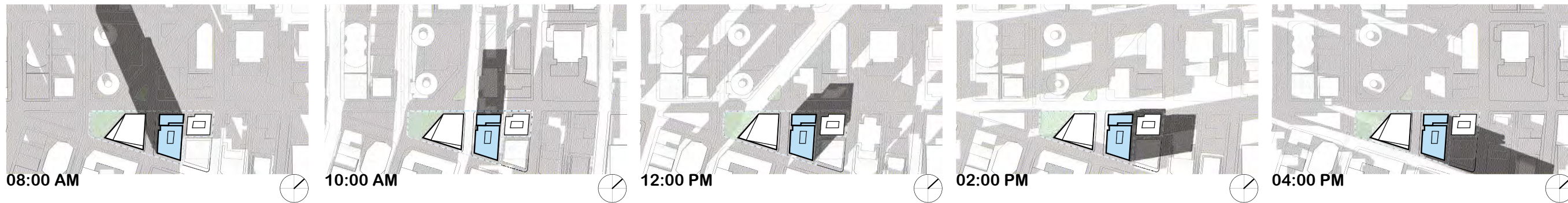
# 06\_CONCEPT 3: HARMONY



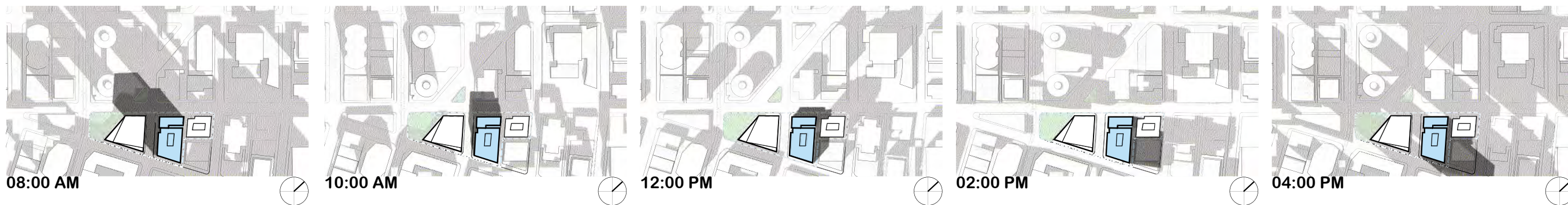
PREFERRED

# 06\_ CONCEPT 3: SHADOW STUDY

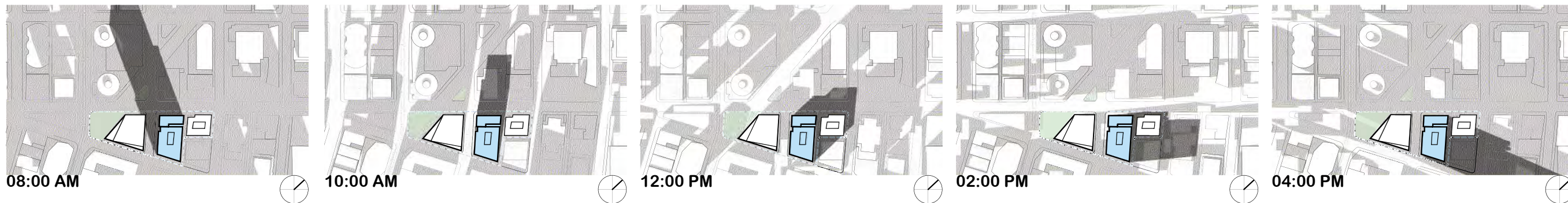
## VERNAL EQUINOX



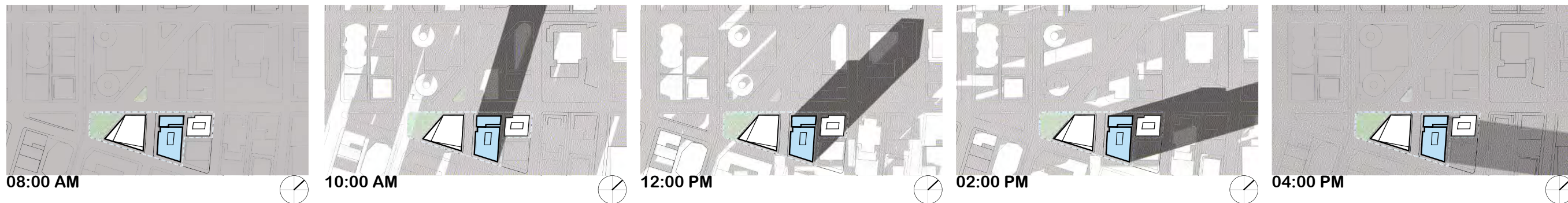
## SUMMER SOLSTICE



## AUTUMNAL EQUINOX



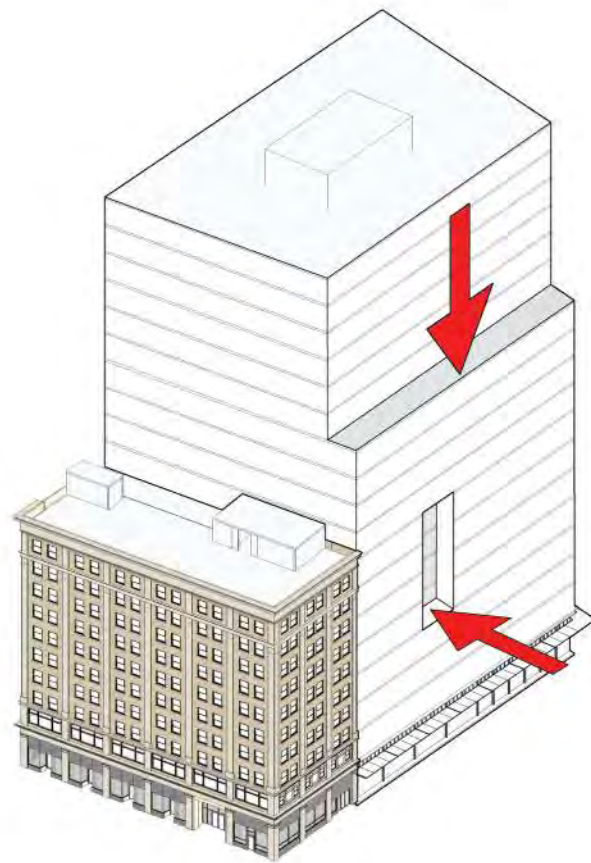
## WINTER SOLSTICE



# 06\_SUMMARY: CONCEPT MASSING OPTIONS

CONCEPT 1

## CONTINUITY



### OPPORTUNITIES:

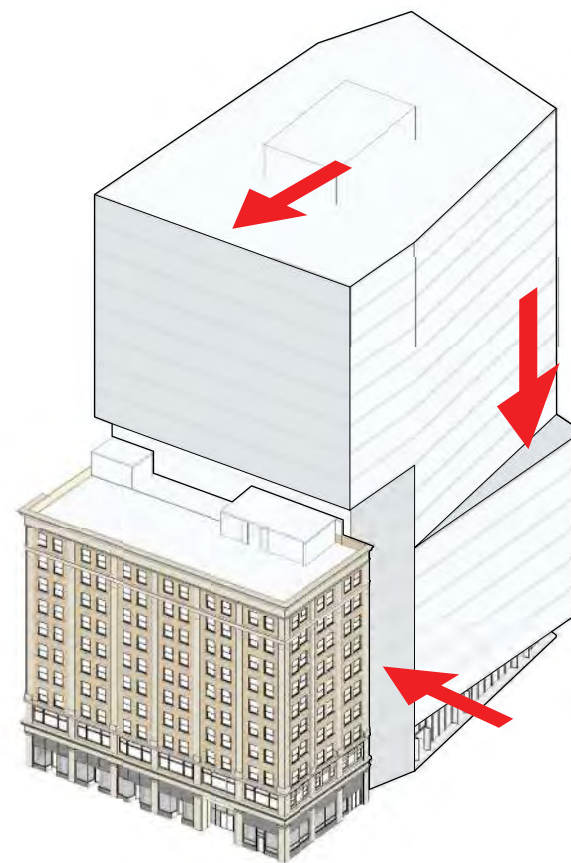
- Meets prescriptive zoning requirements
- Continuation of historic rhythm is sensitive to surrounding context

### CONSTRAINTS:

- Building Entry is not reinforced with massing
- Zoning prescribed step in building mass does not relate to Lloyd datums
- Scale transition between new and old is less responsive than other schemes
- Existing structure of historic building is demolished and replaced (with code compliant loading scheme)

CONCEPT 2

## CONTRAST



### OPPORTUNITIES:

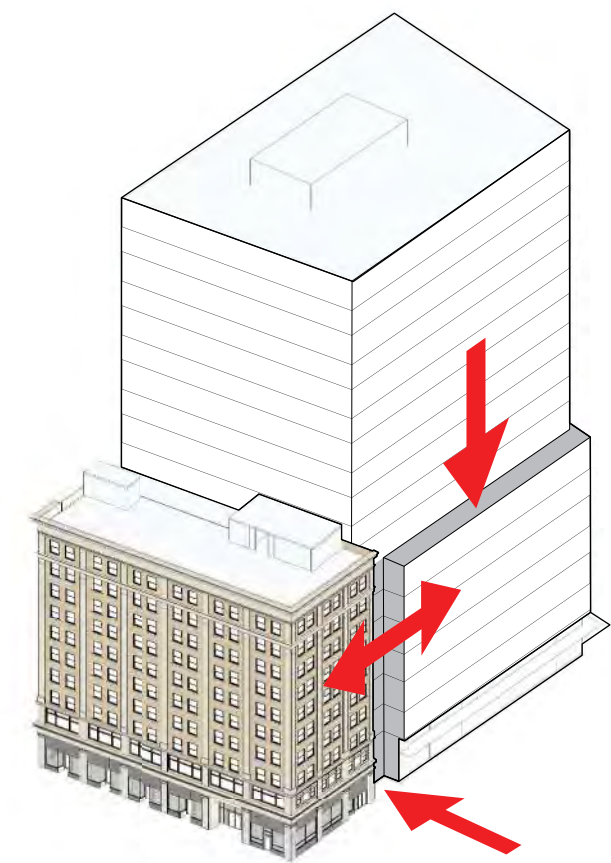
- Sculpted tower is clearly differentiated from historic Lloyd building
- Singular expression & sculptural form would provide a 'memorable sense of place'

### CONSTRAINTS:

- Cantilever over Lloyd building is not desirable from Landmarks and presents constructibility challenges
- Massing requires departure for maximum facade length
- Ground floor requires departures

PREFERRED: CONCEPT 3

## HARMONY



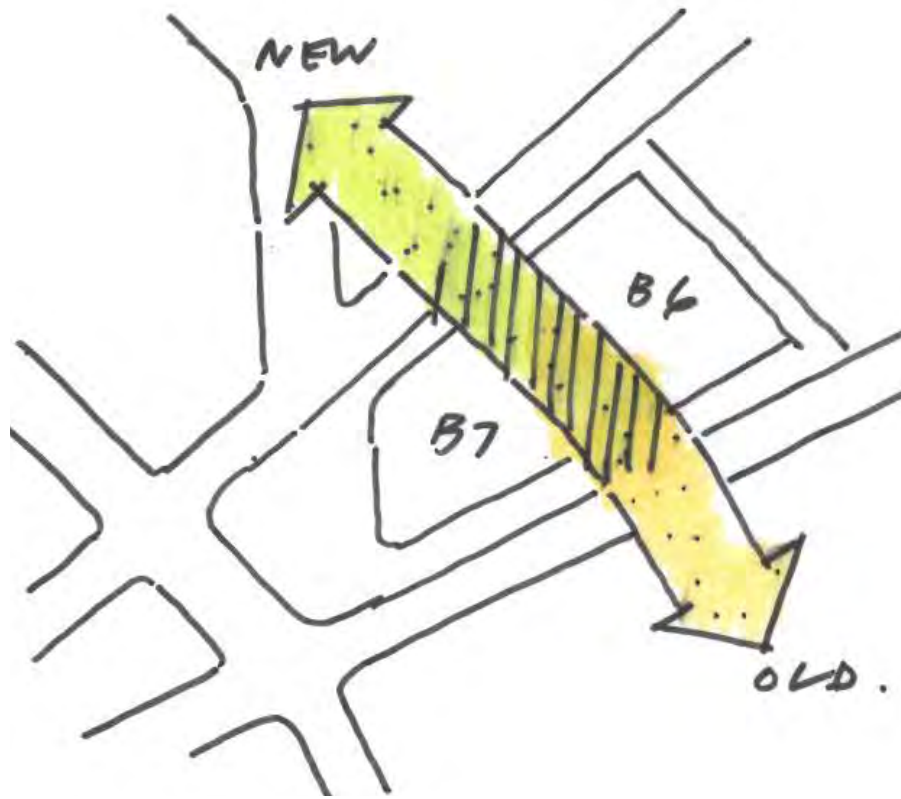
### OPPORTUNITIES:

- Recessed gasket clearly differentiates old from new
- Exposed podium picks up on key datums from Lloyd and responds to both historic context and pedestrian scale
- 'Spatial Stitch' would provide a 'memorable sense of place'
- Continuation of rhythm & texture at the podium is sensitive to surrounding context

### CONSTRAINTS:

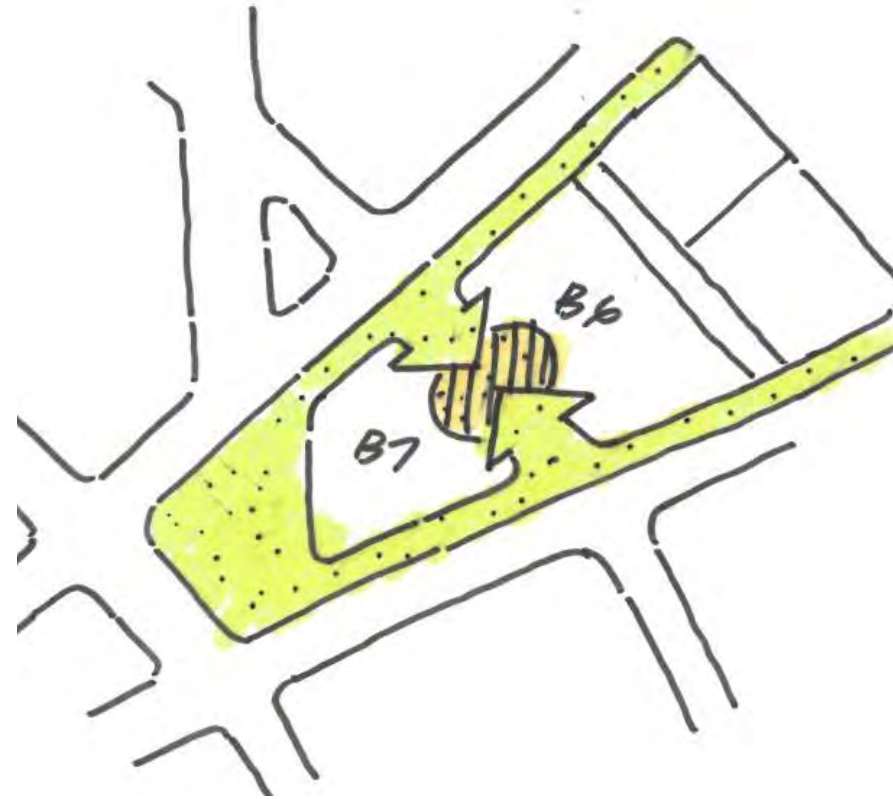
- Massing requires departure for maximum facade length
- Ground floor requires departures

## 06\_GUIDING PRINCIPALS FOR THE PUBLIC REALM



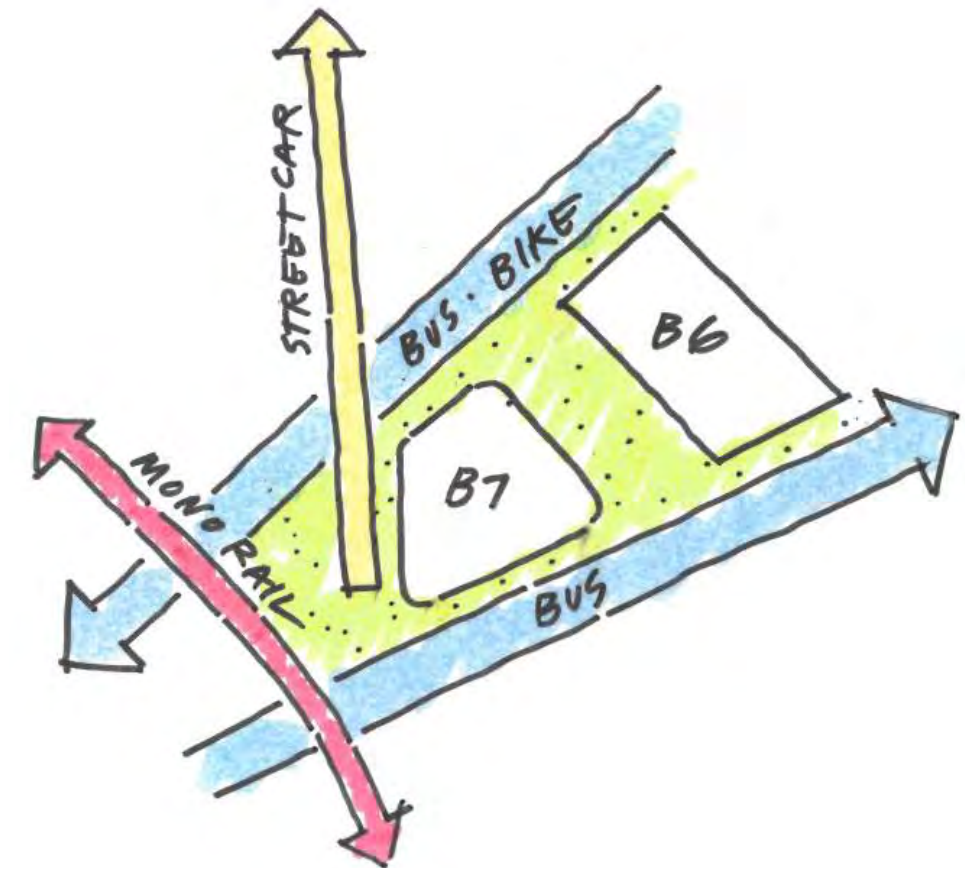
### TRANSFORMING THE RIGHT-OF-WAY

The Sixo blocks sit at the border of old and new Seattle, linking Downtown to the rapidly redeveloping Denny Regrade neighborhood. 6th Ave provides the opportunity to transform the right-of-way to a people centric space like the new public realm of the Denny Regrade while transitioning to the existing street environment to the south.



### EXTENDING OPEN SPACE

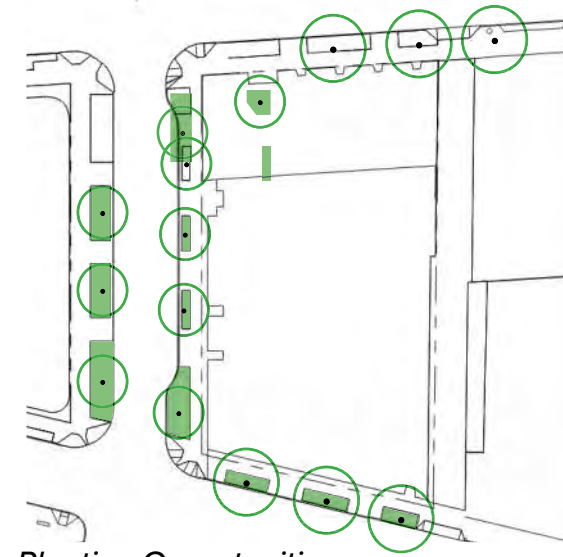
McGraw Square is a vibrant and active open space managed and curated by SDOT and the DSA. The development of the Sixo project ground plane seeks to extend this open space visually and physically to 6th Ave through varied and site specific activation strategies including landscape plantings, pedestrian amenities, and wayfinding.



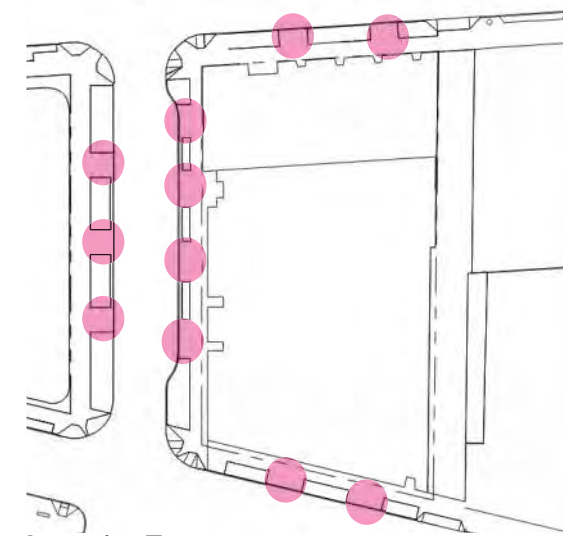
### EMBRACING THE TRANSPORTATION HUB

The Sixo blocks sit at the nexus of one of the City's important transportation hubs that will continue to grow with the City Center Connector and ST3 developments. Open space should embrace this hub through visual cues and wayfinding strategies that provide inviting and clear pedestrian circulation.

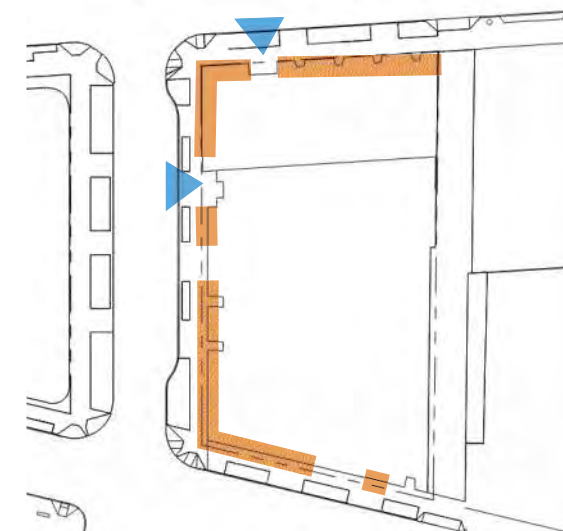
# 06\_SITE CONCEPT PLAN



Planting Opportunities



Amenity Zones



Active Edges

# 06\_LANDSCAPE CONCEPTS: STEWART STREET

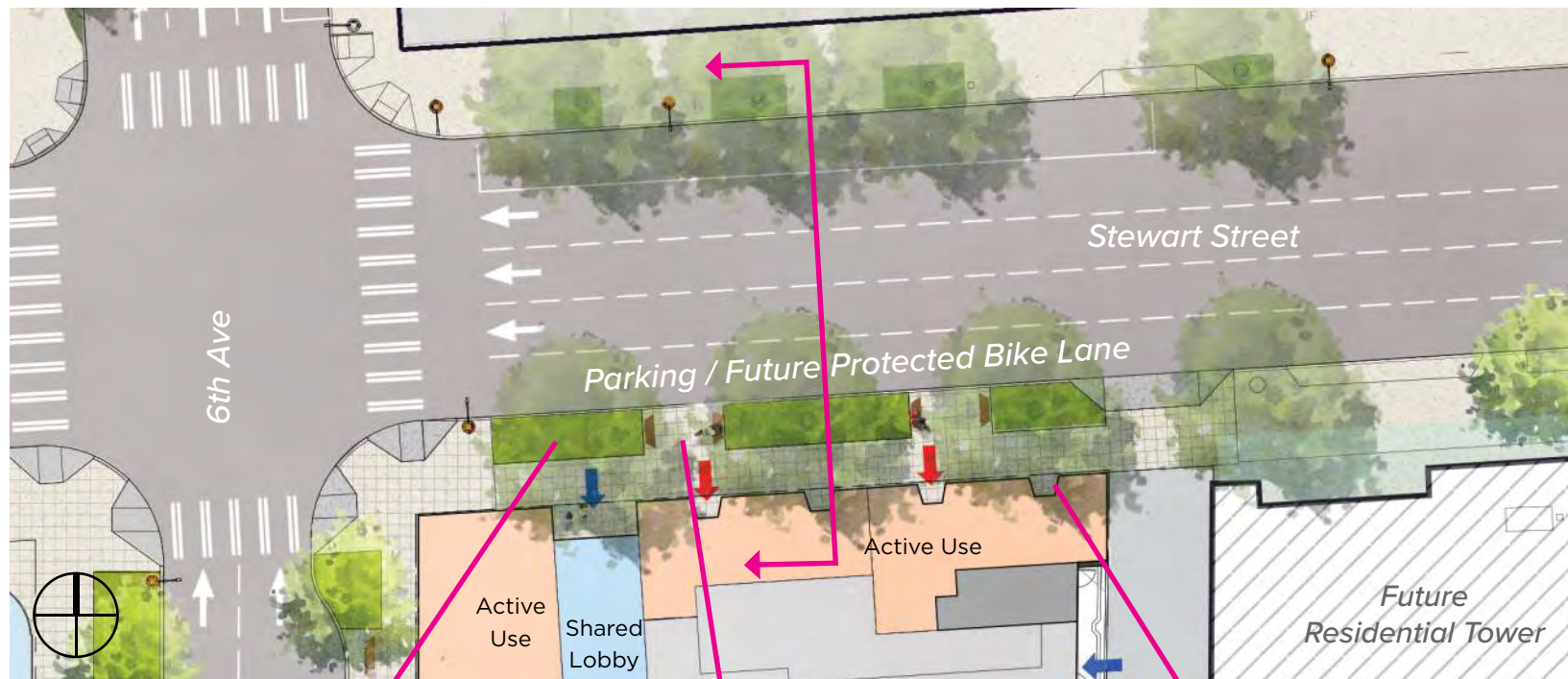
## Applicable Downtown Design Guidelines

### C-1 Promote Pedestrian Interaction

- Reinforce retail connections
- Multiple building entries and large windows
- Existing and proposed landscaping with seating opportunities
- Authentic building materials (historic Lloyd)

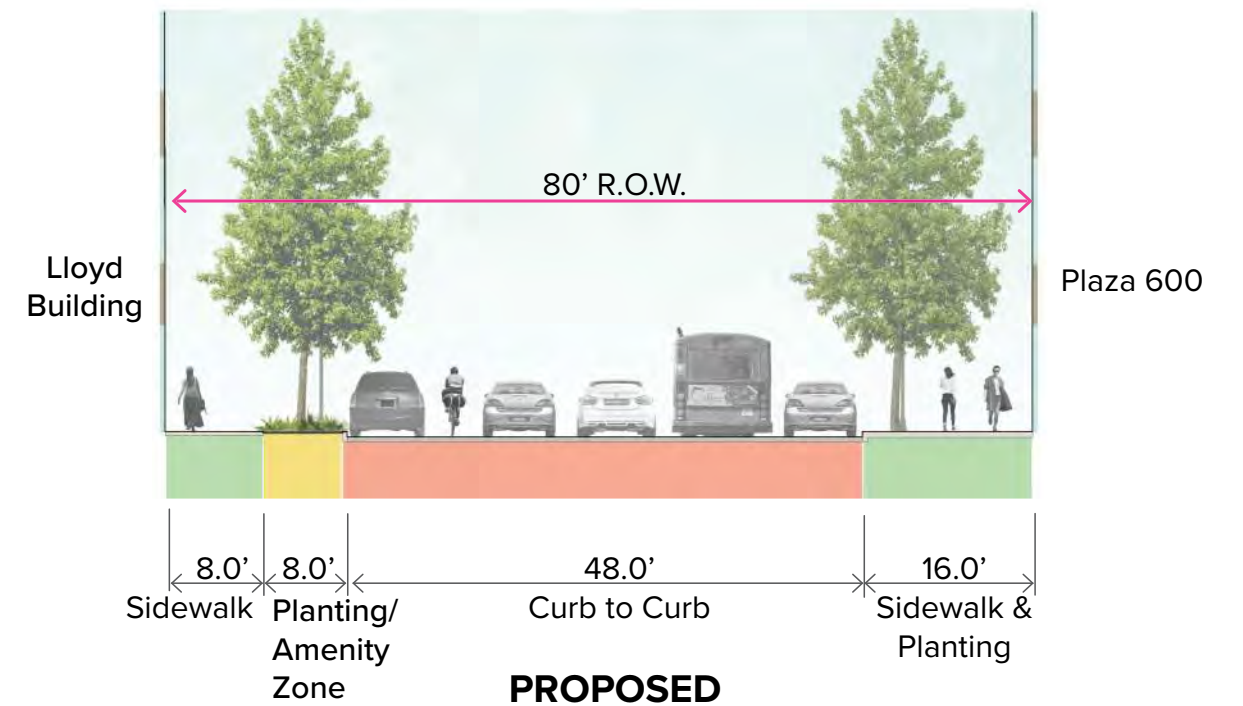
## SDOT Guidance / Design Opportunities

- Existing curb location to remain. Allow for future Protected Bike Lane,
- Protect existing American Sweetgum trees. Enlarge planting strips to enhance root zones and allow for access to parking and seating zones
- Active uses at building frontage with multiple access points



48' CURB TO CURB

**EXISTING**



**PROPOSED**



Protect and enhance root zones for existing American Sweetgum trees

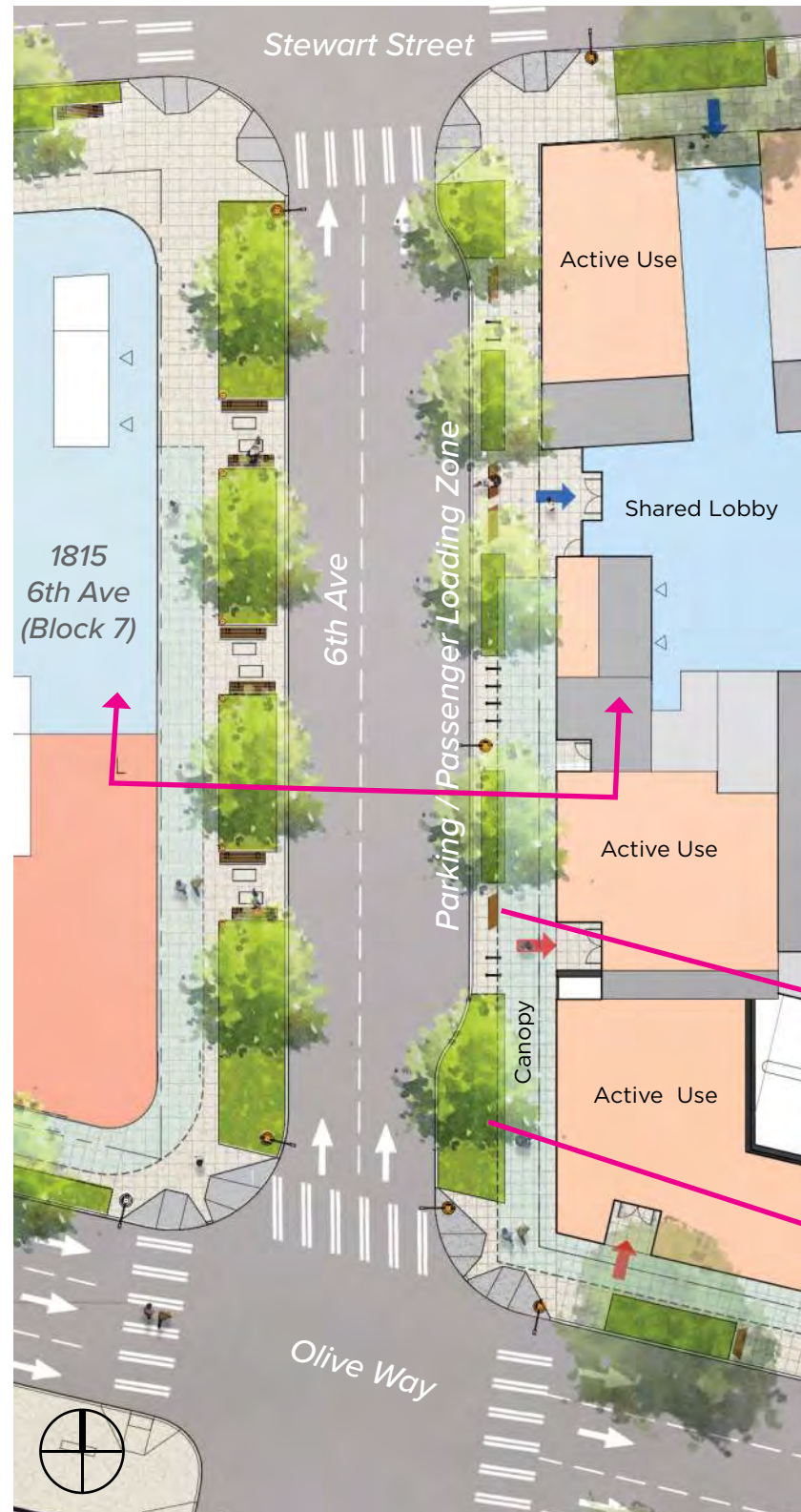


Furnishings and materials should respect the historic character of the Lloyd Building



Small and varied storefronts activate the public realm

# 06\_LANDSCAPE CONCEPTS: 6TH AVENUE



## Applicable Downtown Design Guidelines

### C-1 Promote Pedestrian Interaction

- Transparent facade with views into building
- Enhanced street planting, seating and lighting
- Active frontages include lobby and retail

### C-5 Encourage overhead weather protection

## SDOT Guidance / Design Opportunities

- Promote 6th Ave as a pedestrian street by reducing travel lanes to provide opportunities for larger sidewalk, planting and pedestrian amenities
- Parking / loading zone on east side of street
- Active uses at building frontage with multiple access points, complimenting proposed Sixo Tower (Block 7) development west side of 6th Ave
- Curb bulbs at corners to shorten pedestrian crossings and slow traffic speeds



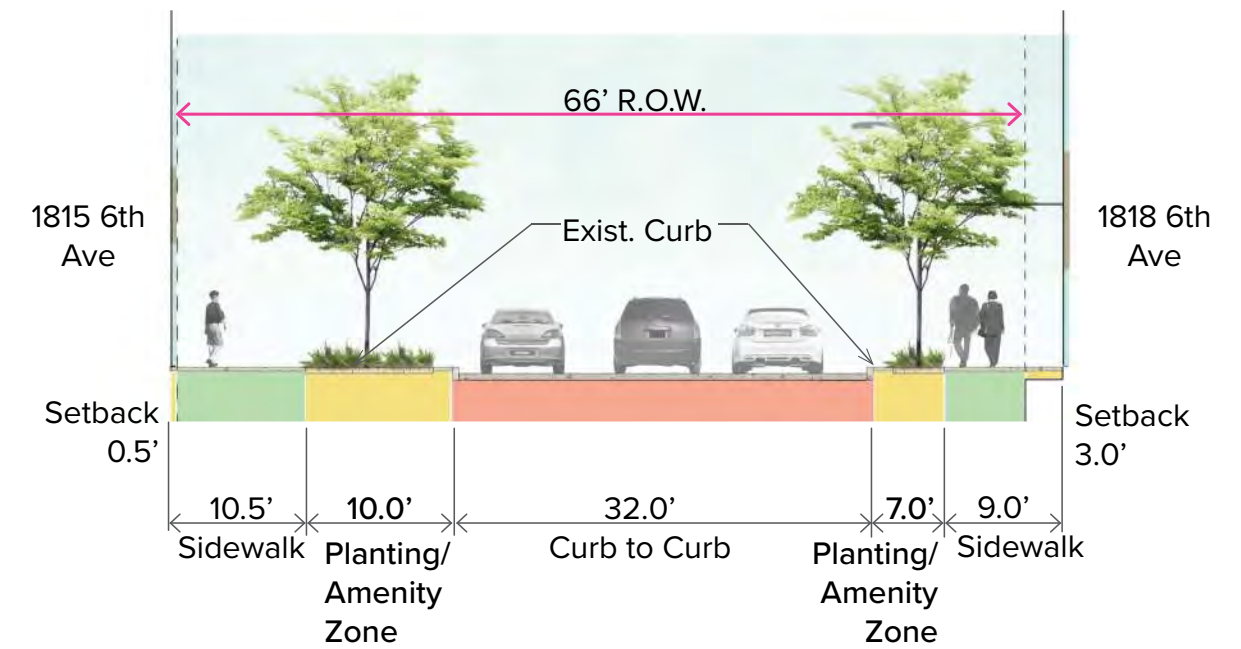
Generous landscape areas provide scale and texture



Amenity zones provide gathering spaces and buffer vehicle loading



**EXISTING**



**PROPOSED**

# 06\_LANDSCAPE CONCEPTS: OLIVE WAY

## Applicable Downtown Design Guidelines

### C-1 Promote Pedestrian Interaction

- Transparent facade with views into building
- Active frontage provide retail opportunity and outdoor seating

### C-3 Provide active - non blank - facades

- Provide art / historical poster boards at blank facade

### C-5 Encourage overhead weather protection

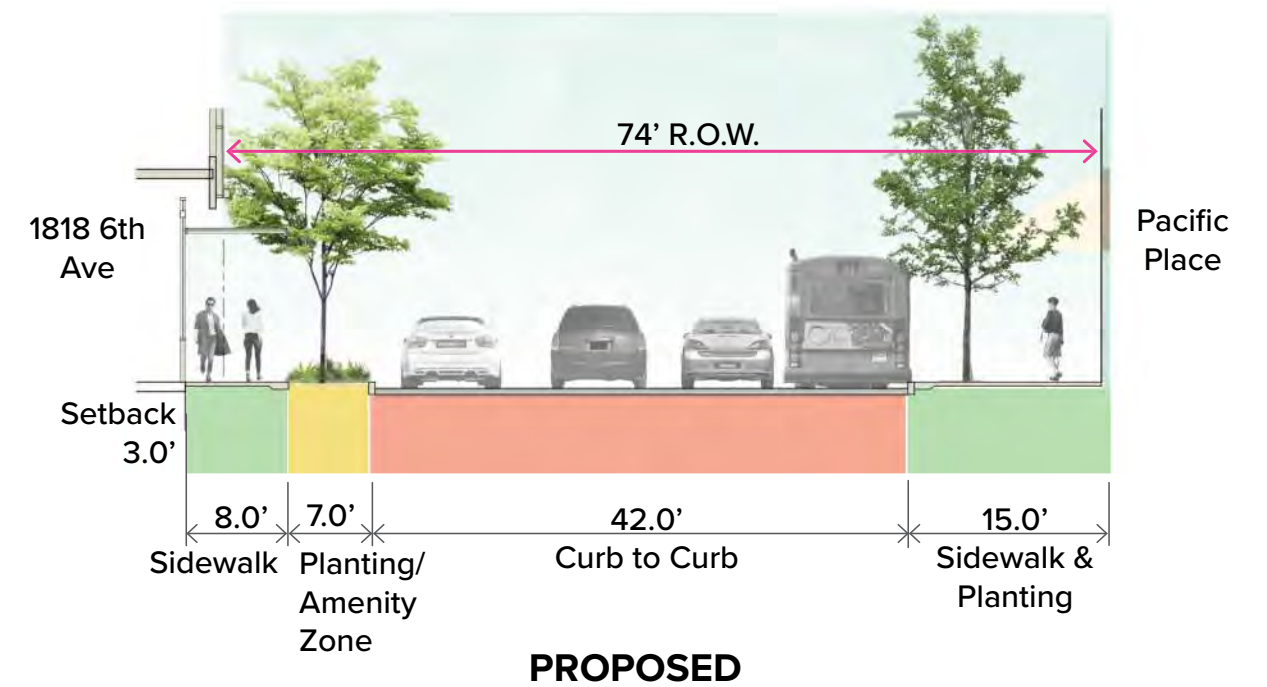
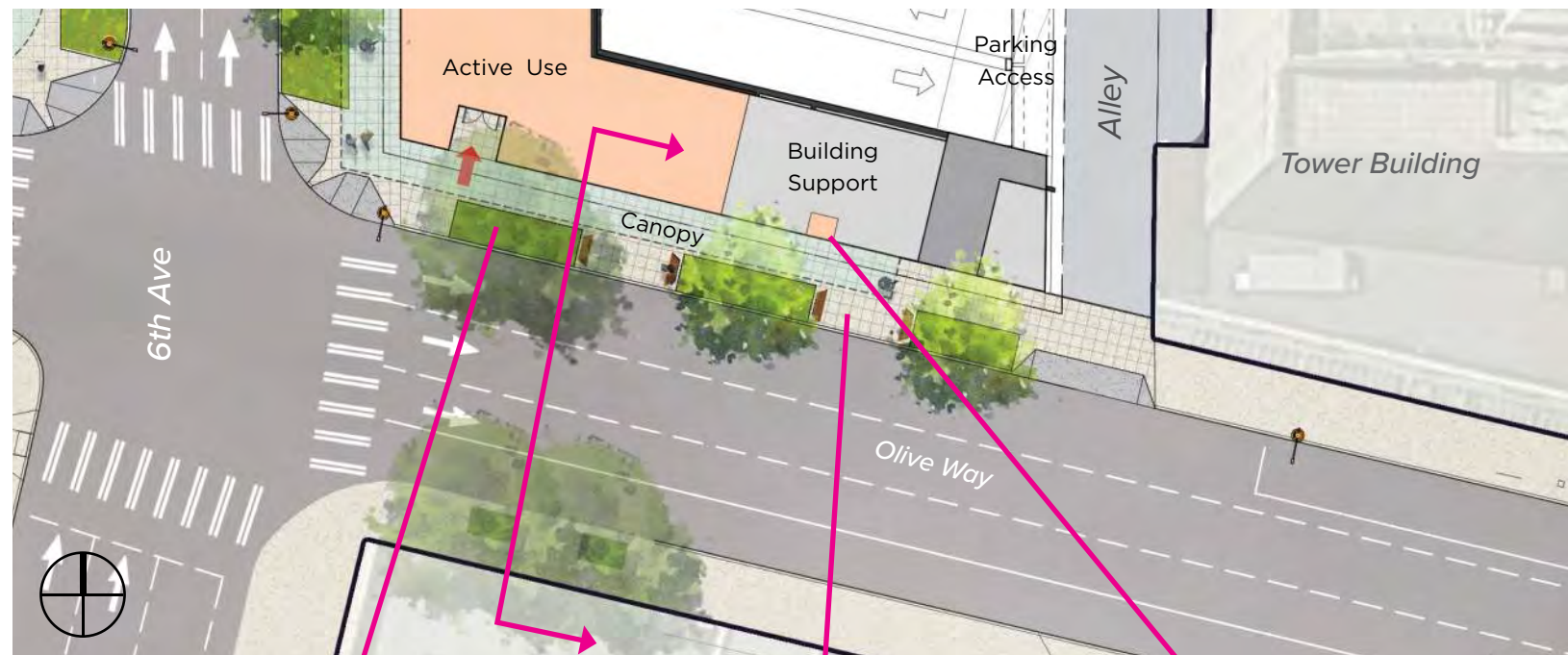
## SDOT Guidance / Design Opportunities

- Existing curb location to remain. Allow for future Protected Bike Lane,
- Protect existing American Sweetgum trees. Enlarge planting strips to enhance root zones and allow for access to parking and seating zones
- Active uses at building frontage with multiple access points



42' CURB TO CURB

**EXISTING**



Significant landscape areas buffer busy arterial street



Amenity zones provide places to gather



Display walls activate blank frontage

[THIS PAGE LEFT INTENTIONALLY BLANK]

**07**  
**DEPARTURE REQUESTS**

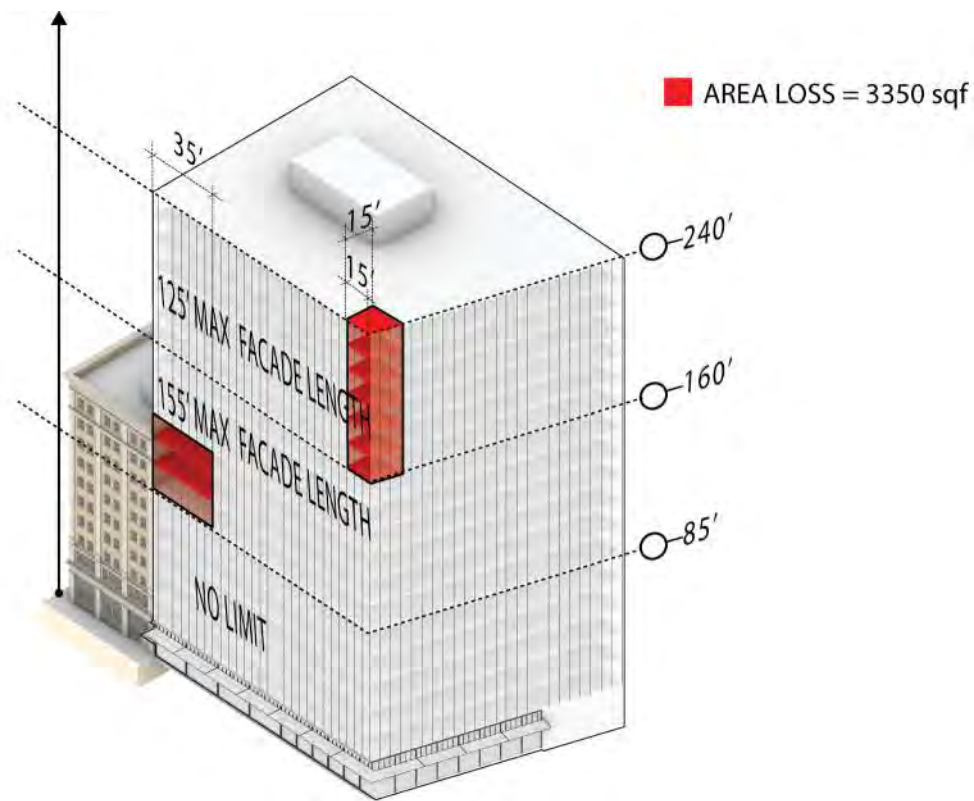
## DEPARTURES - CONCEPT 2

	CODE CITATION	REQUIREMENT	DEPARTURE	RATIONALE	DESIGN GUIDELINES
<b>DEPARTURES</b>	23.49.058.B.2 Upper Façade Modulation	Unmodulated façade width not to exceed lengths described in Table A of Section 23.49.058	6th Avenue facade exceeds maximum facade length of 125' above 160' in height.	This tower scheme embraces a sculptural, singular expression that meets the intent of this code section by minimizing perceived bulk and mass through sculptural form.	B-3: Reinforce the positive urban form and architectural attributes of the immediate area  D-3: Provide elements that define place
	23.49.009 Street Level Use	Except as provided in subsection 23.49.009.B.1.b, a minimum of 75 percent of each street frontage at street level where street-level uses are required must be occupied by uses listed in subsection 23.49.009.A. The remaining 25 percent of the street frontage at street level may contain other permitted uses and/or pedestrian or vehicular entrances.	On Olive Way: approximately 30% of the street frontage at street level is a permitted use, less than 75% of the required. The predominant use of non-compliant street level frontage is the entry and lobby for the new tower and the building generator	The façade on 6th Avenue is highly transparent and visually engaging space with multiple entries that connect the Historic Lloyd and new tower with a new shared lobby. The "Spatial Stitch" of the preferred concept reinforces a new entry on 6th with a gasket between the historic building and new podium and tower. The lobby conditions are highly transparent to the street frontage.	C-2 Design facades of many scales  C-4 Reinforce building entries
<b>TYPE I</b>	23.54.035.A Loading Berths Required	4: 35' berths required	2: 35' berths to be provided per Transpo Loading Berth Analysis.	Expected demand will not require 4 berths. Type I request will be submitted with the MUP application.	

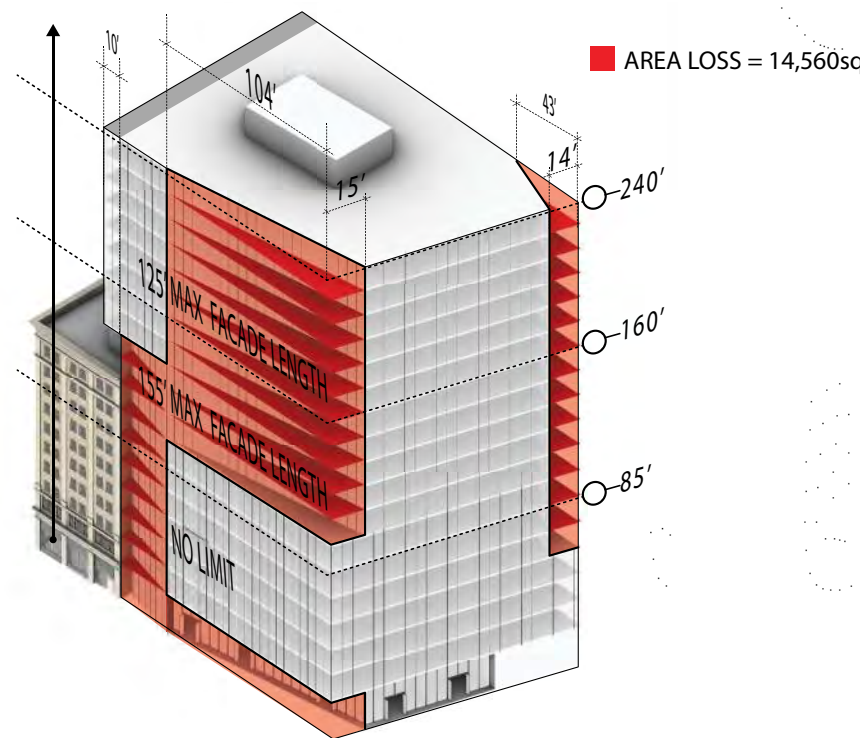
# DEPARTURES - CONCEPT 2

## ① Upper Façade Modulation

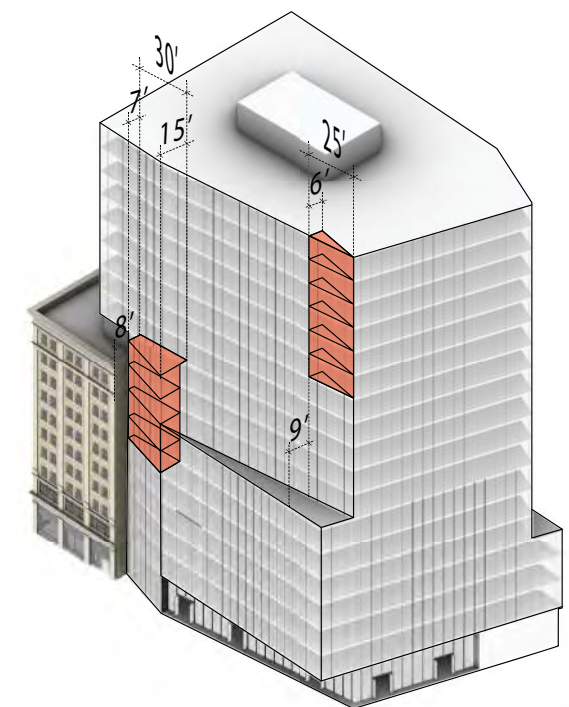
CODE CITATION	REQUIREMENT	DEPARTURE	RATIONALE	DESIGN GUIDELINES
23.49.058.B.2 Upper Façade Modulation	Unmodulated façade width not to exceed lengths described in Table A of Section 23.49.058	6th Avenue facade exceeds maximum facade length of 125' above 160' in height.	The 6th Avenue facade has a voluntary setback above the podium height of approximately 9' across the entire length of the facade to distinguish the podium massing from the tower. This amount of displaced area exceeds the amount of area that would be displaced by the prescriptive facade modulation.	B-3: Reinforce the positive urban form and architectural attributes of the immediate area  D-3: Provide elements that define place



Prescriptive Façade Modulation



Proposed Façade Modulation

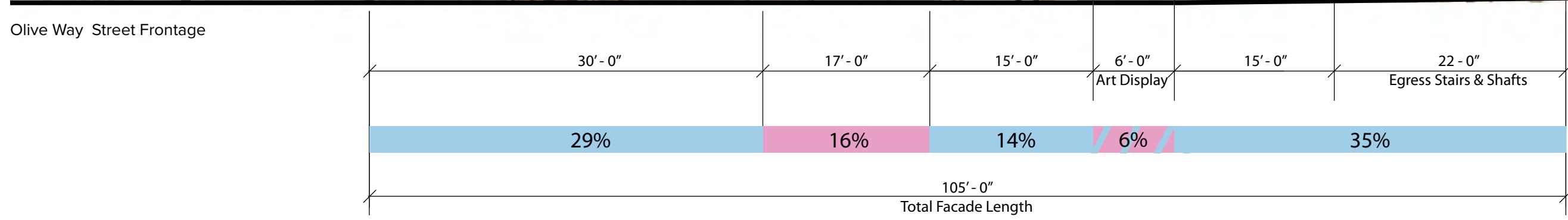
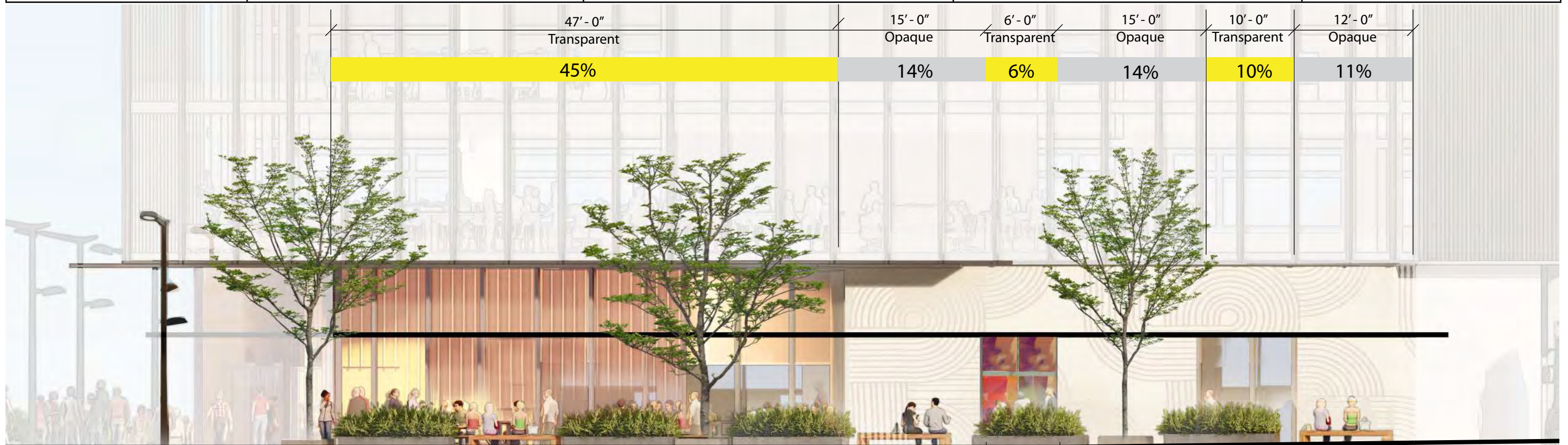


Non Compliant Façade

# DEPARTURES - CONCEPT 2

## ② Street Level Use

CODE CITATION	REQUIREMENT	DEPARTURE	RATIONALE	DESIGN GUIDELINES
23.49.009 Street Level Use	Except as provided in subsection 23.49.009.B.1.b, a minimum of 75 percent of each street frontage at street level where street-level uses are required must be occupied by uses listed in subsection 23.49.009.A. The remaining 25 percent of the street frontage at street level may contain other permitted uses and/or pedestrian or vehicular entrances.	Approximately 16% of the street frontage along Olive Way is a permitted use, less than 75% of the required. The predominant use of non-compliant street level frontage is the entry and lobby for the new office tower and the building generator and exit stairs which are treated architecturally with a combination of wall textures and display windows.	In this scheme, the New Office Tower lobby is on the corner of 6th Avenue and Olive Way, a prominent corner which would reinforce building entry. Where mechanical equipment is necessary to occur at grade, the presence has been screened and minimized through a combination of architectural wall treatments and display windows.	C-2 Design facades of many scales C-4 Reinforce building entries E-3 Minimize the presence of service areas



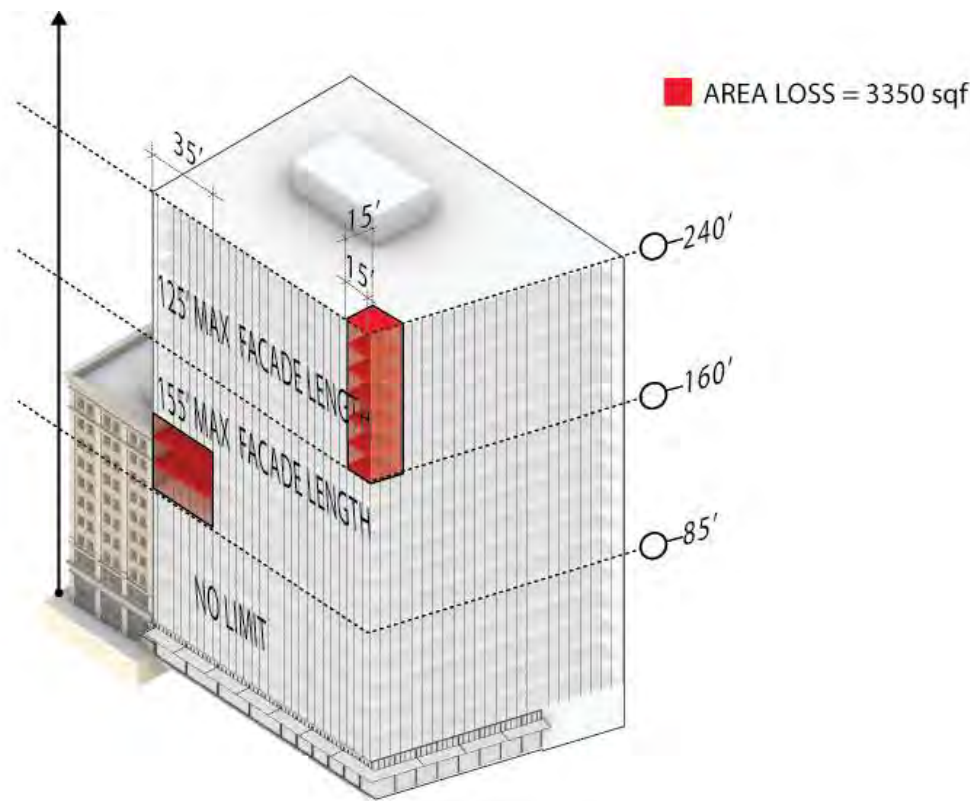
## DEPARTURES - CONCEPT 3 [PREFERRED]

	CODE CITATION	REQUIREMENT	DEPARTURE	RATIONALE	DESIGN GUIDELINES
<b>DEPARTURES</b>	23.49.058.B.2 Upper Façade Modulation	Unmodulated façade width not to exceed lengths described in Table A of Section 23.49.058	6th Avenue facade exceeds maximum facade length of 125' above 160' in height.	The 6th Avenue facade has a voluntary setback above the podium height of approximately 9' across the entire length of the facade to distinguish the podium massing from the tower. This amount of displaced area exceeds the amount of area that would be displaced by the prescriptive facade modulation.	B-3: Reinforce the positive urban form and architectural attributes of the immediate area  D-3: Provide elements that define place
	23.49.009 Street Level Use	Except as provided in subsection 23.49.009.B.1.b, a minimum of 75 percent of each street frontage at street level where street-level uses are required must be occupied by uses listed in subsection 23.49.009.A. The remaining 25 percent of the street frontage at street level may contain other permitted uses and/or pedestrian or vehicular entrances.	Approximately 63% of the street frontage at street level is a permitted use, less than 75% of the required. The predominant use of non-compliant street level frontage is the entry and lobby for the new tower.	The proposed facade design along 6th Avenue is highly transparent with multiple points of activation and entry. The portion of the frontage that is not considered active use is the new, shared office lobby that celebrates the connection between new and old. The proposed display window adjacent to the entry lobby will help further define and reinforce building entry.	C-2 Design facades of many scales  C-4 Reinforce building entries
<b>TYPE I</b>	23.54.035.A Loading Berths Required	4: 35' berths required	2: 35' berths to be provided per Transpo Loading Berth Analysis.	Expected demand will not require 4 berths. Type I request will be submitted with the MUP application.	

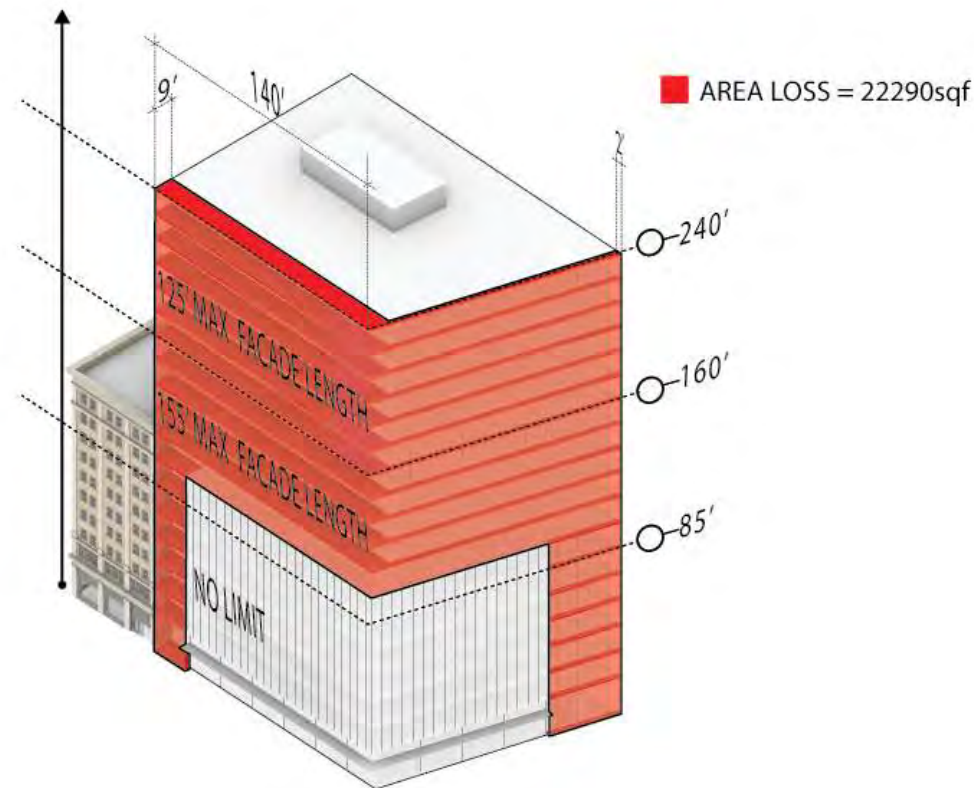
# DEPARTURES - CONCEPT 3 [PREFERRED]

## ① Upper Façade Modulation

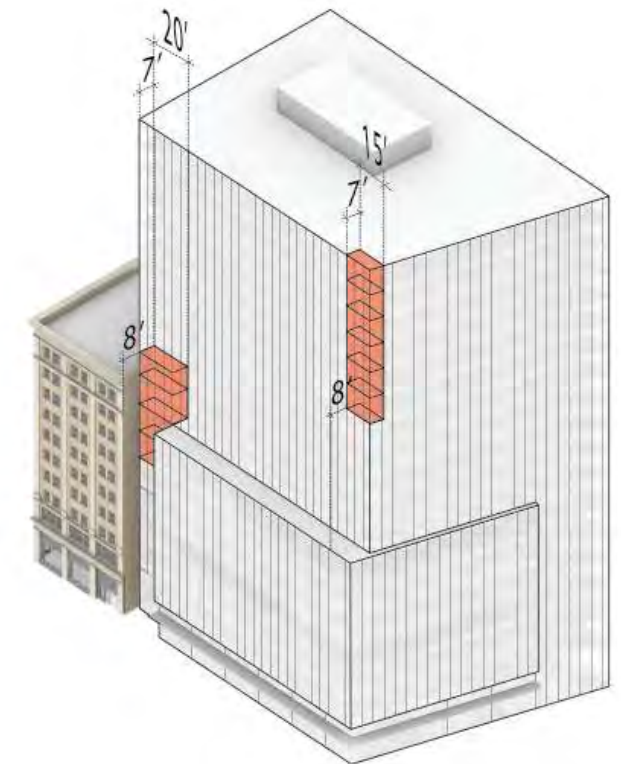
CODE CITATION	REQUIREMENT	DEPARTURE	RATIONALE	DESIGN GUIDELINES
23.49.058.B.2 Upper Façade Modulation	Unmodulated façade width not to exceed lengths described in Table A of Section 23.49.058	6th Avenue facade exceeds maximum facade length of 125' above 160' in height.	The 6th Avenue facade has a voluntary setback above the podium height of approximately 9' across the entire length of the facade to distinguish the podium massing from the tower. This amount of displaced area exceeds the amount of area that would be displaced by the prescriptive facade modulation.	B-3: Reinforce the positive urban form and architectural attributes of the immediate area  D-3: Provide elements that define place



Prescriptive Façade Modulation



Proposed Façade Modulation



Non Compliant Façade

# DEPARTURES - CONCEPT 3 [PREFERRED]

## ② Street Level Use

CODE CITATION	REQUIREMENT	DEPARTURE	RATIONALE	DESIGN GUIDELINES
23.49.009 Street Level Use	Except as provided in subsection 23.49.009.B.1.b, a minimum of 75 percent of each street frontage at street level where street-level uses are required must be occupied by uses listed in subsection 23.49.009.A. The remaining 25 percent of the street frontage at street level may contain other permitted uses and/or pedestrian or vehicular entrances.	Approximately 63% of the street frontage at street level is a permitted use, less than 75% of the required. The predominant use of non-compliant street level frontage is the entry and lobby for the new office tower.	The proposed facade design along 6th Avenue is highly transparent with multiple points of activation and entry. The portion of the frontage that is not considered active use is the new, shared office lobby that celebrates the connection between new and old. The proposed display window adjacent to the entry lobby will help further define and reinforce building entry.	C-2 Design facades of many scales  C-4 Reinforce building entries

