

2.0 CONTENTS	2
3.0 PROJECT DATA	4
4.0 SITE PLAN	5
5.0 ZONING DATA	6
6.0 DESIGN GUIDELINES	8
7.0 DESIGN REVIEW -EDG	10
8.0 DESIGN REVIEW RESPONSES	
massing	12
entry and streetscape	13
site	14
materials	16
9.0 DEPARTURES REQUEST	18
10.0 ARCHITECTURAL DESIGN	20
building elevations	20
floor plans	24
building section	28
perspective views	29
landscape design	31
11.0 APPENDIX	37

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PROJECT INFORMATION

SITE ADDRESS	4401, 4411, 4419 FREMONT AVE N
PARCEL NUMBERS	569350-1505, 569350-1345, 569350-1362, 569350-1375
PROJECT NUMBER	3039251-EG, 3039107-LU

PROJECT TEAM

ARCHITECT	Atelier Drome Architecture 119 S. Main St. Suite 310 Seattle, WA 98104
CONTACT	Michelle Linden michelle@atelierdrome.com

PROJECT CRITERIA

ZONING	NC2P-55 (M), LR1 (M)
OVERLAYS	frequent transit
ABUTTING ZONES	LR1(M)
CURRENT USE	restaurant/lounge, retail, office & (1) single family house
LOT AREA	17,042 sf (total) 4401 Fremont: 8,104sf 4411 Fremont: 4,502 sf 4419 Fremont: 4,463 sf
ALLOWABLE FAR	60,348.75 sf
ECAs	None
VEHICULAR PARKING	Yes

PROJECT PROPOSAL

GROSS BUILDING FLOOR AREA	60,306 sf (preferred scheme)
PROPOSED RESIDENTIAL UNITS	118 units (preferred scheme)
PROPOSED COMMERCIAL UNITS	4/5 units, 4,365 sf (preferred scheme)
PROPOSED VEHICULAR PARKING	37 parking spaces (utilizing parking lifts)
NO. OF STORIES	4 stories of residential over 1 level of commercial
DEMOLITION	Existing structures to be demolished

CONTEXT + SITE

The project site is located at the north or “upper” end of the Fremont neighborhood. Fremont extends from the Ship Canal to approximately N 50th Street at the Woodland Park Zoo. This neighborhood, which has consistently been an important area of residential growth for North Seattle, has seen significant growth over the past several years, with single-family homes being supplemented with townhomes and rowhouses, and with larger scale mixed-use buildings to the south in Fremont’s main commercial core, between N 34th and 36th streets.

This edge of the neighborhood is quieter but maintains a small-town, pedestrian friendly atmosphere, with it’s own block of commercial buildings between N Allen and N 42nd, with local businesses that have grown roots - a grocery store, coffee shop, salons, restaurants, specialty bookstore, and medical services and offices. Directly to the north, on 50th, is the Woodland Park Zoo, which draws families and groups from across Seattle. This area is also well served with preschool and elementary schools, providing essential support for families in the area, within walking distance. Transportation is found within walking distance of the site, with bus service to downtown and north Seattle, Ballard, Wallingford, and the U-District.

DEVELOPMENT PROPOSAL

This project proposes a five-story mixed-use building with ground floor commercial, approximately 120 residential apartments, and an underground parking garage. The three existing buildings will be demolished.

DEVELOPMENT OBJECTIVES

Objective 1: Provide visually interesting massing that enhances and respectfully builds upon the north Fremont neighborhood character, while responding to the neighborhood scale.

Objective 2: Design a building that responds to its position as a visual corner on Fremont and N 44th.

Objective 3: Provide housing for a growing neighborhood while maintaining its vibrant and pedestrian-friendly atmosphere; create space to sustain and include local small businesses.

design guideline key

CS1-C Topography

CS2-C-1 Corner Sites

CS2-B-2 Connection to the Street

CS2-D-3 Zone Transitions

CS2-D-4 Massing Choices

CS2-D-5 Respect for Adjacent Sites

DC2-A-2 Reducing Perceived Mass

DC2-C Secondary Architectural Elements

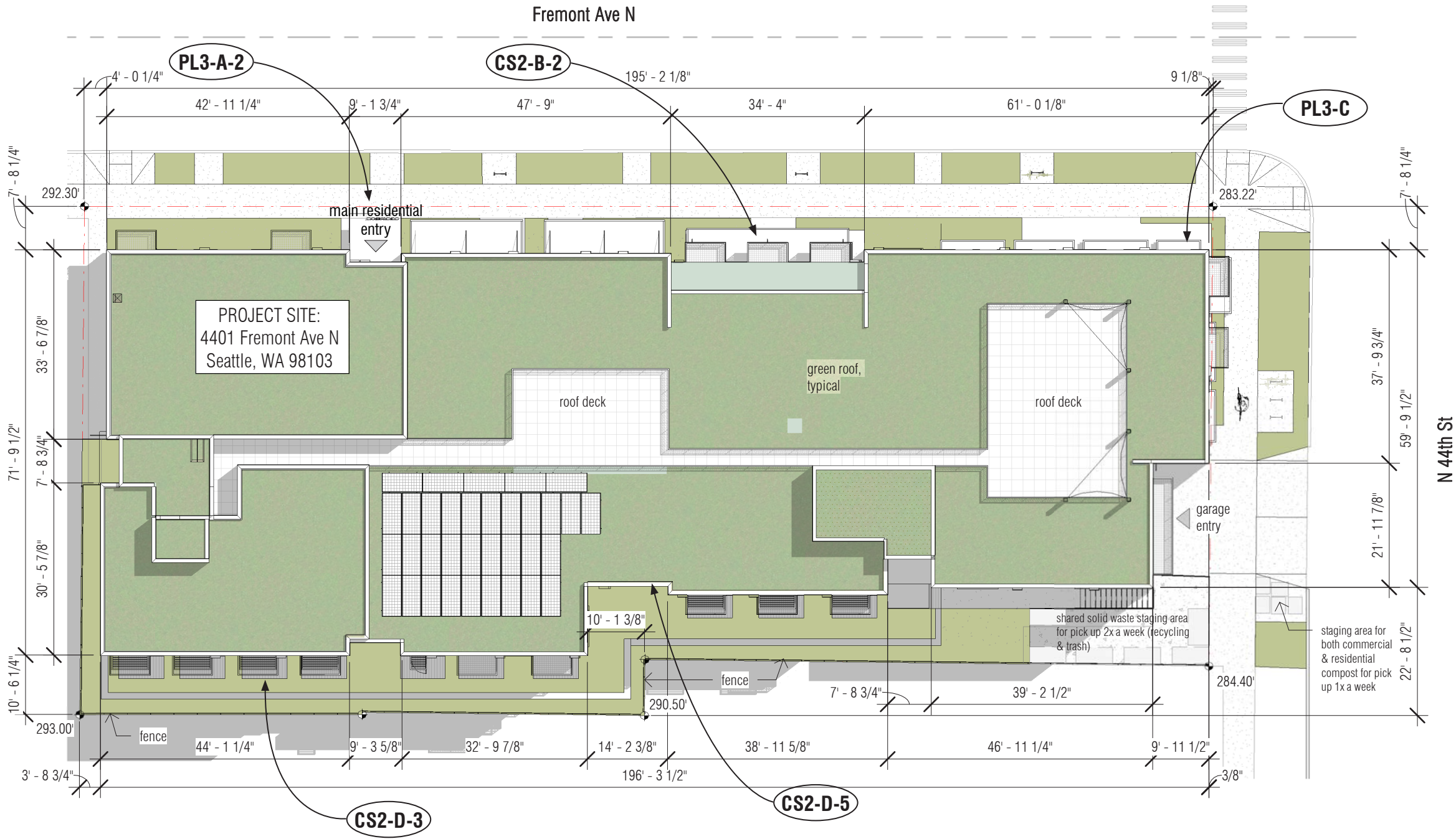
DC4-A-1 Exterior Finish Materials

PL2-D-1 Design as Wayfinding

PL3-A-2 Ensemble of Elements

PL3 Street-Level Interaction

PL3-C Retail Edges



5.0 ZONING DATA

ZONE	ABUTS	INCENTIVES	RESTRICTIONS	OVERLAY	SITE AREA	USES PERMITTED OUTRIGHT
NC2P-55 LR1(M)	LR1(M)	frequent transit	none	none	17,042 sf	residential & commercial uses

LAND USE CODE INFORMATION

DESIGN TEAM RESPONSE

USES PERMITTED OUTRIGHT - 23.47A.004 TABLE A

- Residential uses
- General Sales and services and restaurants limited to 25,000 sf; Multipurpose sales and service limited to 50,000 sf
- The street-level, street-facing facade on a designated principal pedestrian street is limited to certain uses

Project proposes residential and commercial uses (general and multipurpose sales and service, restaurant).

COMPLIES

STREET-LEVEL USES - 23.47A.005

- Residential uses may occupy in the aggregate, no more than 20% of the street-level, street-facing facade in a pedestrian-designated zone, facing a designated principal pedestrian street.
- Pedestrian-Designated Zones - One or more of the following uses are required along 80% of the street-level, street-facing facade: arts facilities; community gardens; eating and drinking establishments; entertainment uses; food processing and craft work; medical services; sales and services general (no more than 40’ can contain an office).

Project proposes a residential lobby at the street level.

COMPLIES

Fremont Ave N is a principal pedestrian street in a pedestrian-designated zone. Project proposes eating and drinking establishments and general sales and service, which meet the required 80% min.

COMPLIES

STREET-LEVEL DEVELOPMENT STANDARDS - 23.47A.008

Basic Street-Level Requirements:

- Blank Facades: Segments of street-facing facades between 2 - 8 feet above the sidewalk may not exceed 20 feet in width; total may not exceed 40% of the facade width of the structure along the street.
- Street-level street-facing facades: Shall be located within 10 feet of the street lot line unless wider sidewalks, plazas, or other approved landscaped or open spaces are provided.

The building is located in a pedestrian-designated zone and will comply with the basic street level requirements for blank facades, non-residential uses and pedestrian zones:

COMPLIES

- No blank facades proposed;
- The street-level, street-facing facade is primarily located within 10’ of the street lot line, except where a wider sidewalk and landscaping is provided.

Non-Residential Street Level Requirements:

- Transparency: 60% of street-facing facades between 2 - 8 feet above the sidewalk shall be transparent
- Depth Provisions: Uses > 600sf shall extend an average depth of at least 30’ and 15’ min. from the street-level, street-facing facade. Where < 600 sf, uses shall extend an average depth of at least 20’ and 10’ min.
- Height: Uses shall have a floor-to-floor height of at least 13 feet

- The project will be 60% transparent between 2-8 feet above the sidewalk;
- The commercial spaces will extend an average depth of at least 30’ and a min of 15’ and will have a min. floor to floor height of 13’.

Pedestrian Zones:

- A min. of 80% of a street-level street-facing facade that faces a pedestrian street shall be occupied in pedestrian street level uses. The remaining frontage may contain other permitted uses and/or pedestrian entrances.

- The project proposes eating and drinking establishments and general sales and service, which meet the pedestrian zone requirement.

Overhead Weather Protection:

- Continuous weather protection is required at least 60% of the street frontage of a structure.
- The covering must be at least 6’ wide; the lower edge shall be a min of 8’ and a max of 12’ above the sidewalk.
- Adequate lighting for pedestrians shall be provided. The lighting may be located on the facade of the building or on the weather protection.

- Continuous overhead weather protection is provided along Fremont.
- Adequate lighting for pedestrians will be provided at the canopy.

DEPARTURE REQUEST

Residential Use at Street-Level:

- At least one of the street-level street-facing facades containing a residential use shall have a visually prominent pedestrian entry.
- In pedestrian designated zones, residential uses are limited to 20% of the street-level, street-facing facade.

- The residential use has a prominent pedestrian entry on Fremont Ave N;
- No dwelling units are located on the ground level street-facing facade.
- Project proposes a residential use of 27% of street-level, street-facing facade.

DEPARTURE REQUEST

MAXIMUM STRUCTURE HEIGHT - 23.47A.012

- Maximum structure height = 55 feet
- Certain rooftop features may also extend beyond the height limit (in particular, a stair or elevator penthouse may extend an additional 16 feet), however, some of these features must be located at least 10 feet from the north edge of the roof

The project proposes to comply with the maximum structure height and rooftop features will comply with height requirements.

COMPLIES

ZONE	ABUTS	INCENTIVES	RESTRICTIONS	OVERLAY	SITE AREA	USES PERMITTED OUTRIGHT
NC1-55 LR1(M)	LR1(M)	frequent transit	none	none	17,042 sf	residential & commercial uses

LAND USE CODE INFORMATION

MAXIMUM FAR - 23.47A.013 <ul style="list-style-type: none">NC zone: 3.75; LR zone: 1.3; Total FAR for project site: 60,348.75 sf 485.5 sf in LR x 1.3 = 631.15 sf; total this lot: 15,659.275 sf 496.5 sf in LR x 1.3 = 645.45 sf; total this lot: 15,564.825 sfFAR exempt area: underground stories, portions of a story that extend no more than 4 feet above grade, rooftop greenhouse area	Proposed far is less than the 60,348.75 sf max allowed.	COMPLIES
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SETBACK REQUIREMENTS - 23.47A.014 <ul style="list-style-type: none">Setbacks are required for lots abutting residential zones - Where a lot abuts the intersection of a side lot line and front lot line of a residential zone, which forms a triangle area, the two sides of the triangle extend along the street lot line and side lot line 15’ from the intersection of the residentially zoned lots front lot line and side lot line.Upper-level setback is required for any rear or side lot line that abuts a lot in an LR zone - 10 ft for portions of structures above 13 ft in height to a maximum of 65 ft.Decks with open railings may extend into the required setback, but are not permitted within 5’ of a lot in a residential zone, unless the deck is accessory to a residential use and are no more than 18” above existing or finished grade, whichever is lower, are permitted within 5’ of a lot in a residential zone.	Side lot line triangle setback is provided in all schemes as required; upper level setbacks are provided for the rear and side lot lines where abutting an LR zone.	COMPLIES
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LANDSCAPING & SCREENING STANDARDS - 23.47A.016 <ul style="list-style-type: none">A green factor score of 0.3 or greater is required (functionally equivalent to landscaping 30% of lot)	The proposed project will provide a green factor score of .3 or greater.	COMPLIES
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LIGHT & GLARE STANDARDS - 23.47A.022 <ul style="list-style-type: none">Exterior lighting must be shielded and directed away from adjacent uses	Exterior light will be shielded and directed away from adjacent uses.	COMPLIES
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REQUIRED AMENITY AREA - 23.47A.024 <ul style="list-style-type: none">5% of the total gross floor area in residential use required (area excludes mech equipment and parking)All residents shall have access to at least one common or private amenity area; Amenity areas shall not be enclosedNo common amenity area shall be less than 250sf and shall have a minimum horizontal dim of 10 feetPrivate balconies and decks shall have a minimum area of 60sf and no horizontal dimension shall be less than 6 feet	All schemes are provided with at least 5% of the total gross floor area as amenity area. The amenity area will be located on the roof as a roof deck & visual amenity to landscaping; the amenity area will not be enclosed.	COMPLIES
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REQUIRED PARKING & EXCEPTIONS - 23.54.015 Table A, Table B, Table D & 23.54.020 <ul style="list-style-type: none">1 Space per dwelling unit (residential) with a frequent transit reduction of 50% where the property is not located in an urban center, urban village, or station area overlay district.In all commercial zones, no parking is required for the first 1500 sf of each business establishmentBicycle parking for commercial uses: eating and drinking establishments: 1 long term bicycle space per 5,000 sf; 1 short term bicycle space per 1,000 sf. sales and services, general: 1 long term bicycle space per 4,000 sf; 1 short term bicycle space per 2,000 sf. offices: 1 long term bicycle space per 2,000 sf; 1 short term bicycle space per 10,000 sf.Bicycle parking for residential uses = 1 bicycle space per dwelling unit (long term); 1 short term space per 20 dwelling units	36 parking stalls are provided for the residential use. None required for the commercial uses due to size of the business establishment. Bike parking will be provided for the commercial and residential uses - see the basement ground floor level in all schemes, as verified by code.	COMPLIES
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MANDATORY HOUSING AFFORDABILITY - 23.58C.040 TABLE B <ul style="list-style-type: none">Medium area; Zones with a (M) suffix - \$13.25/sf developer contribution (residential) / \$5.78/sf (commercial)	DEVELOPER CONTRIBUTION WILL COMPLY
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simple modulation with variety of materials at human scale



residential scale materials in modern application



multiple entries dotting facade



recessed façade at ground plane

PL3: Street Level Interaction

C. Retail Edges, 1. Porous Edge

Engage passersby with opportunities to interact visually with the building interior using glazing and transparency. Create multiple entries where possible and make a physical and visual connection between people on the sidewalk and retail activities in the building.

Response:

With a long façade facing Fremont Ave and a shorter facade facing 44th Ave, there are multiple opportunities for street level interaction. Multiple entries dot the length of the Fremont façade, providing a variety of opportunities for visual and physical connection. The design team anticipates the fremont façade will be highly glazed and transparent for further engagement with passersby.

CS2: Urban Pattern & Form

C. Relationship to the block, 1. Corner Sites

Corner sites can serve as gateways or focal points; both require careful detailing at the first three floors due to their high visibility from two or more streets and long distances. Consider using a corner to provide extra space for pedestrians and a generous entry, or build out to the corner to provide a strong urban edge to the block.

Response:

The design team’s scheme has a recessed expression at the southeast corner, in order to provide a more generous entry and pedestrian experience. Above the extra tall first floor, the building extends out to the corner to provide a strong urban edge to the block, visible from long directions. A unique detailing expression for the corner mass, further enhances it as a focal point.

CS2: Urban Pattern & Form

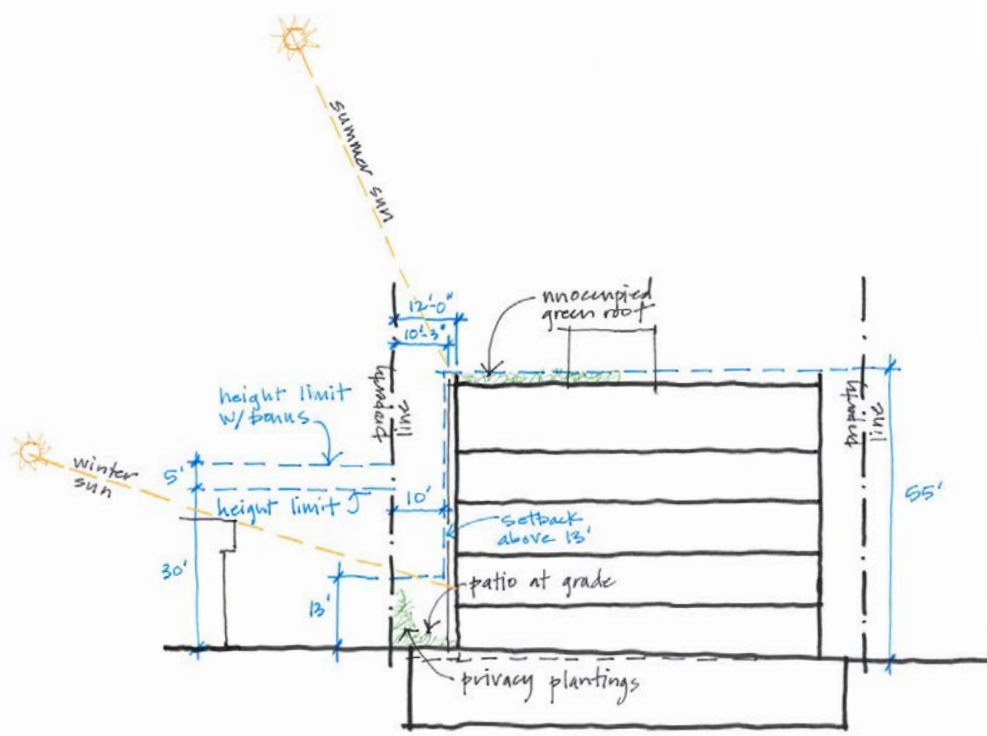
D. Height, Bulk and Scale, 3. Zone Transitions

For projects located at the edge of different zones, provide an appropriate transition or complement to the adjacent zone(s). Projects should create a step in perceived height, bulk and scale between the anticipated development potential of the adjacent zone and the proposed development. Factors to consider: a. Distance to the edge of a less (or more) intensive zone; b. Differences in development standards between abutting zones; c. The type of separation from adjacent properties (e.g. separation by property line only, by an alley or street or open space, or by physical features such as grade change); d. Adjacencies to different neighborhoods or districts; adjacencies to parks, open spaces, significant buildings or view corridors; and e. Shading to or from neighboring properties.

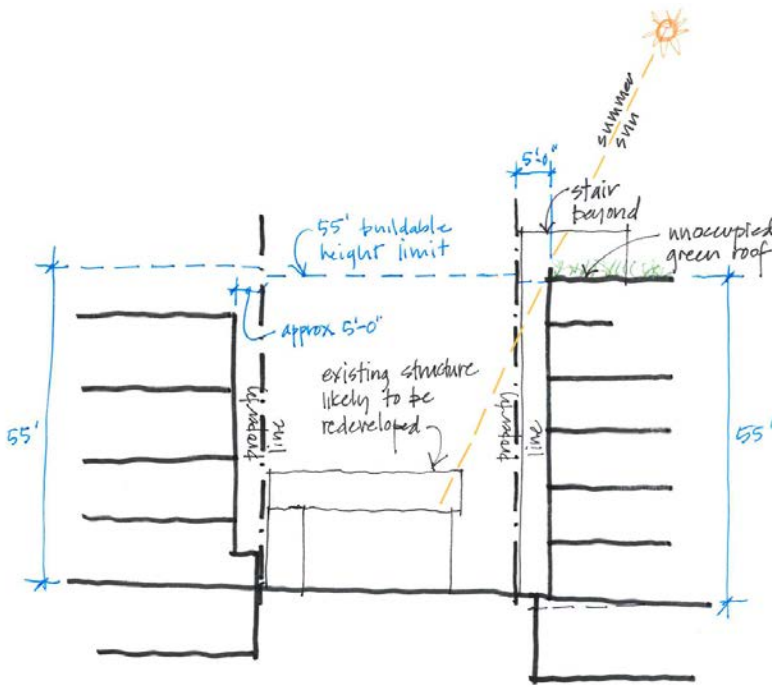
Response:

This project site abuts an LR zone to the west. The street containing the LR zone is currently filled with a mix of single family homes, townhomes, and low-rise apartment structures. Because the project site is abutting the LR zone via a lot line, versus a street or alley separation, the design team has opted to provide separation between the zones greater than what is required by code.

The zoning code requires a setback at the upper floors, however it allows for the lower floor to extend to the lot line. The design team has opted to set back all floors from the lot line, increasing this beyond the minimum requirement, where feasible. This will allow for a greater separation between zones, accommodate at grade patios and plantings, and reduce shading. The team is providing additional modulation on the LR facing zone in order to respond to the less intensive LR zone’s façade modulation requirements.



WEST PROPERTY LINE DIAGRAM



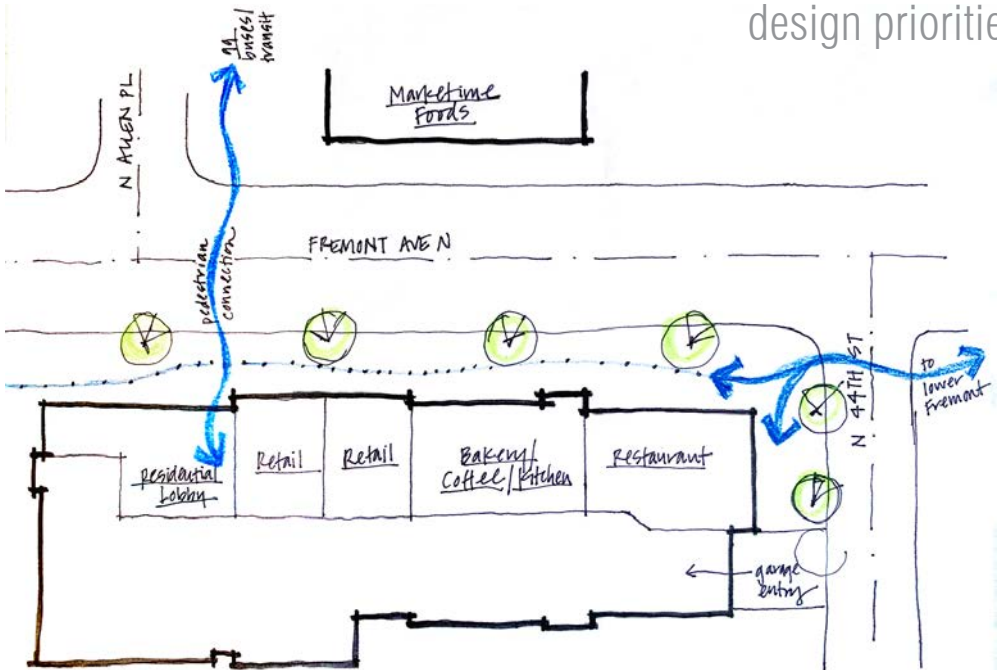
NORTH PROPERTY LINE DIAGRAM



strong urban edge



sense of place from neighborhood history



response to existing site connections



variety of Pedestrian Experiences

PL1: Connectivity

B. Walkways and connections, 1. Pedestrian Infrastructure

Connect on-site pedestrian walkways with existing public and private pedestrian infrastructure, thereby supporting pedestrian connections within and outside the project.

Response:

With the project’s long frontage along Fremont Ave, it is important that the pedestrian experience is well connected at each commercial and residential entry. The building modulates along the eastern edge in order to provide a variety of pedestrian experiences, as well as allowing for pedestrian connections to a variety of floor heights. The connection to Allen Place is maintained, which provides a direct route to the transit options on Aurora and the neighborhood grocery store.

CS3: Architectural Context & Character

A. Emphasizing Positive Neighborhood Attributes, 4. Evolving Neighborhoods

In neighborhoods where architectural character is evolving or otherwise in transition, explore ways for new development to establish a positive and desirable context for others to build upon in the future.

Response:

The Fremont neighborhood has historically been an artistically eclectic and vibrant neighborhood. Much of the larger development has been focused on the southern end of Fremont. This northern section or “Upper Fremont” area extends towards

Wallingford and Phinney Ridge; the neighborhood is beginning to evolve as newer, larger developments are being constructed.

This project draws from the neighborhood context through massing and exterior materials. Additionally, it builds upon the pedestrian scale and connections already established in neighboring buildings.

CS3: Architectural Context & Character

B. Local History and Culture, 1. Placemaking

Explore the history of the site and neighborhood as a potential placemaking opportunity. Look for historical and cultural significance, using neighborhood groups and archives as resources.

Response:

The existing neighborhood has a very desirable pedestrian scale, with a great deal of walkability and human-scaled mixed-use buildings. Many of these existing buildings were built between 1900 and 1930 and maintain a simple, classic form with a clear hierarchy of materials. New structures need to take into consideration both the existing smaller scale fabric of the neighborhood, as well as the larger developments that are coming.

This project anticipates providing the pedestrian scale that is already utilized by existing historic buildings, with a finer grain modulation and utilizing a combination of simple massing moves (to reflect the scale and proportion of the existing fabric structures) and a variety of materials at a human scale.

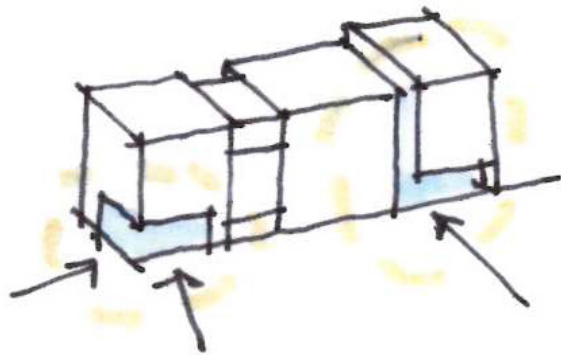
DC4 Exterior Elements & Finishes

A. Building Materials, 1, Exterior Finish Materials

Building exteriors should be constructed of durable and maintainable materials that are attractive even when viewed up close. Materials that have texture, pattern, or lend themselves to a high quality of detailing are encouraged.

Response:

The design team is utilizing a variety of attractive materials in order to help break down the overall mass and length of the façade, as well as relate to the neighborhood context (both the residential to the west and commercial to the east).



OPTION 3 (PREFERRED)

This zoning code compliant scheme provides visual interest and movement along all facades. The overall mass is broken down vertically, stacking the masses side-by-side, in direct response to the upper fremont neighborhood’s historic fabric.

Two important nodes - the highly visible corner at 44th and Fremont and the termination of N Allen Place at Fremont - are celebrated with cut aways at the ground floor. These recesses enhance the pedestrian realm and identify key points of the building, with the residential lobby aligned with the termination of N Allen Place.

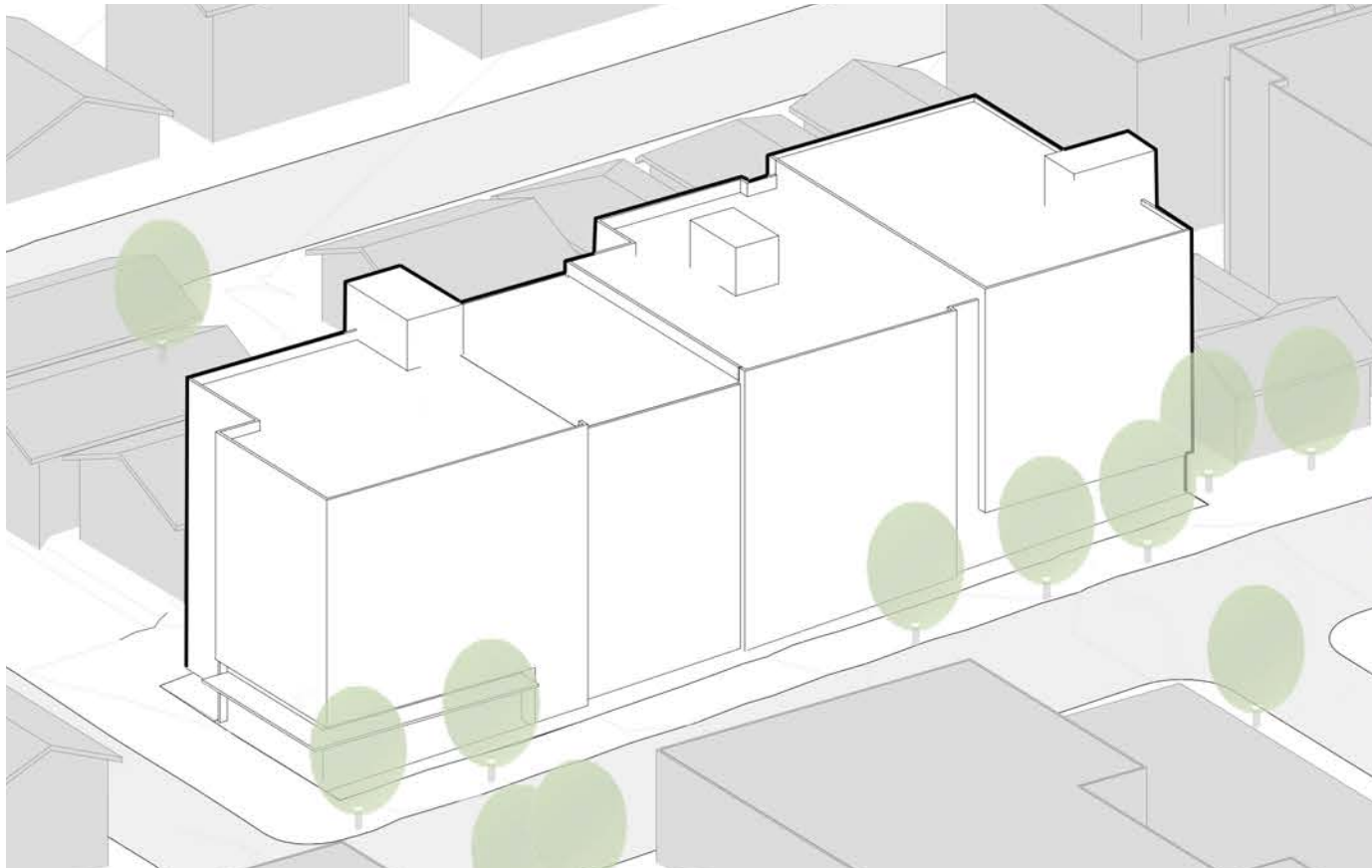
The ground floor commercial spaces have been broken up in order to provide a streetscape compatible with the pedestrian experience along Fremont Ave N. and helps to create an engaging experience at the pedestrian level.

COMMERCIAL USE:	4/5 SPACES, 4,365 SF TOTAL
RESIDENTIAL USE:	66 SEDUS, 44 1-BEDS, 6 2-BEDS, 2 3-BEDS (118 TOTAL)
PARKING:	36 STALLS

- BENEFITS:
- CS2.C1 - Steps the ground floor commercial level back from the street to enable spillover into the public realm, enhancing the pedestrian experience and taking advantage of the building corner.
 - CS3.A3 - Modulation provides visual interest on street facing facades and relates to the historic context of the neighborhood.
 - DC3.B4 - Roof deck with multiple levels and lounge/seating areas provide views to the east, west and south, with views to downtown, the olympics, mount rainier and the cascades.
 - CS2.D3 - Generous setback and modulation on the west facade for the neighboring LR zone, with balconies and garden apartments to enhance the residential fee.

- CONCERNS:
- The more complex form is best served by a simpler fenestration / material patterning.

- DEPARTURES:
- Residential use at ground floor
 - overhead weather protection





EDG BOARD RECOMMENDATION

In agreement with public comment, the Board unanimously supported Option 3, the applicant’s preferred massing option. The Board appreciated the illustrations of the progression of design thought for the massing options, as shown on page 25 of the EDG packet, clarifying a refinement of massing form from a basic Code-compliant option to the best context-adapted option.

The Board specifically supported the rhythm of vertical modulations in Option 3, which break down the long front façade length into contextually scaled modules. The Board noted that the proportions of the modulations appeared logical as well as aesthetically pleasing in the overall façade composition.

The Board noted that although the graphics appeared to create a contextual scale, the building was not shown in context with other neighborhood structures for comparison. The Board requested the scale of modulation be shown with actual context in the Recommendation package to demonstrate the modulations are staying true to the massing and scale concept.

The Board noted that Option 3 emphasized the corner with a unique massing volume with a ground-level commercial unit wrapping substantially onto the side street.

The Board especially appreciated that the vertical modulations that provide scale on the east street-facing frontage are also articulated on the west (rear) façade, creating relatable scale at the residential zone transition.

The Board additionally supported the 11’ setback along the rear property line and the lower height of Option 3 (as compared to the grade-averaged Option 2 massing) as contributing to the rational design response along the zone transition.

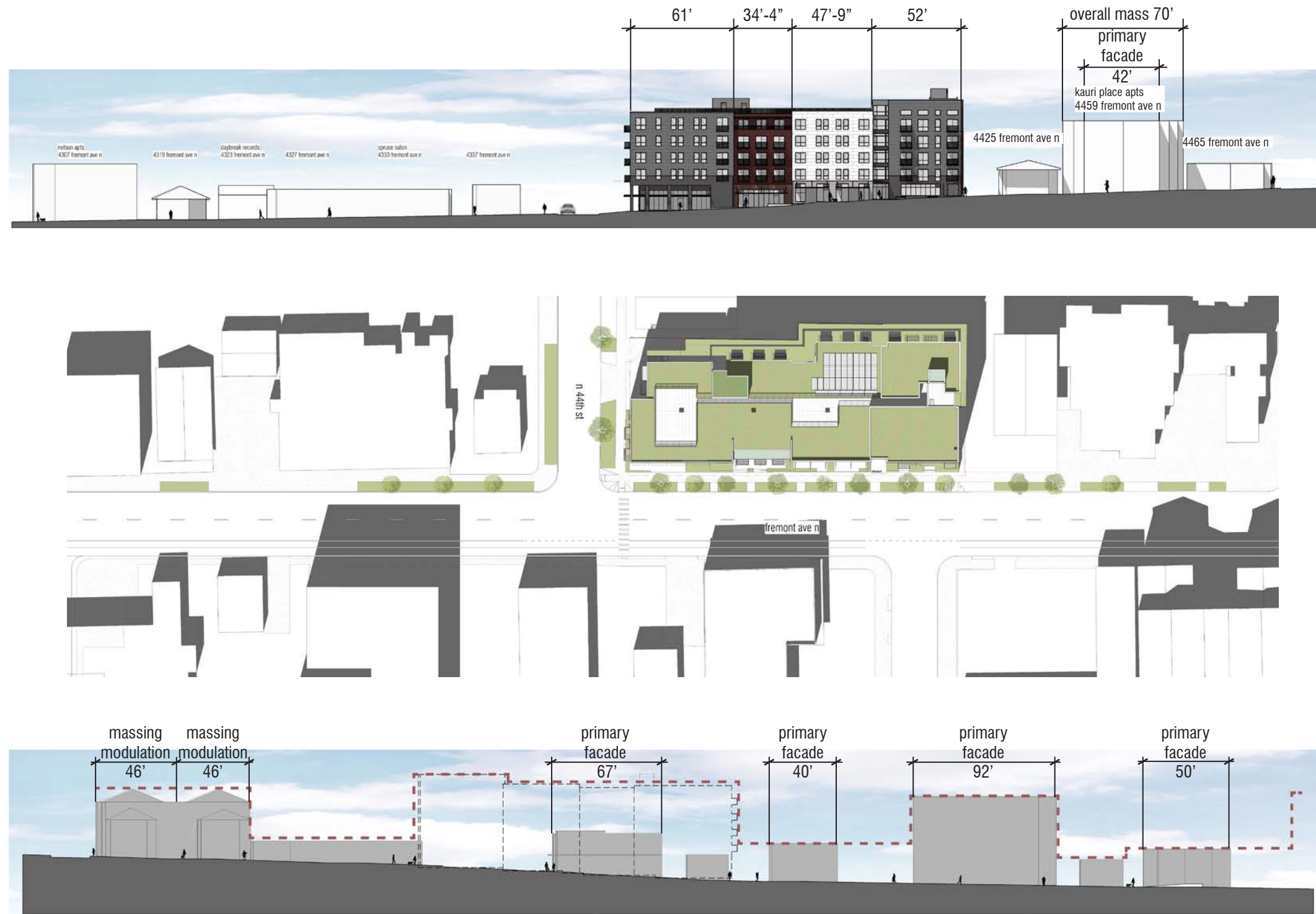
DESIGN RESPONSE

The vertical modulation presented in the approved EDG concept has been maintained by the design team. In evaluating the neighborhood structures for comparison, there is not a consistent breakdown in façade width or modulation. However, when analyzing the larger scale apartment complex to the north, as well as the more developed side of the street – the majority of developments have a primary façade or major massing move between 40’ and 70’ in width. This is consistent with the modulation presented in our proposed project.

The design team has maintained the preferred corner element with the ground floor wrapping on two sides, the vertical modulations extending through to the west of the site, the 11’ rear yard setback, and the split height of Option 3 as requested by the board.

DESIGN GUIDELINES

- CS2-D-4. Massing Choices
- DC2-A-2. Reducing Perceived Mass
- CS3-A-4. Evolving Neighborhoods
- CS2-C-1. Corner Sites
- CS2-D-3. Zone Transitions



EDG BOARD GUIDANCE

The Board specifically supported the street-level layout of Option 3 where the commercial units step floor levels to meet the grades as the sidewalk climbs along Fremont Ave N. The stepped ground levels, aligned with the upper-level modulations, create a legible progression of spaces along the frontage.

The Board also supported the differentiated façade treatments at the ground level that create scale in relation to the other storefronts in this neighborhood commercial area.

The Board specifically supported the setback of the corner commercial unit at both the Fremont Ave and N 44th St frontages, allowing for commercial spill out space and creating a visually porous edge between the public and private realms at the corner.

The Board supported the location of the residential entry at the northern edge of the structure long the Fremont Ave frontage. The Board noted that the location of the residential use was logical in terms of progression of the street level uses from commercial at the busy corner to residential at the north edge. They also noted that the entry was oriented towards the 'T' intersection with N Allen Pl for convenient access to the crosswalk.

DESIGN RESPONSE

The design team has maintained the stepped floor levels at the commercial spaces in order to align with the sidewalk and the upper level modulations.

Each vertical modulation is expressed with a similar material, with color aiding in distinguishing the massing modulations. At the ground plane facing the commercial street, the facades are all clad in brick – a nod to the pedestrian nature of the street as well as the historic precedent in the neighborhood. Above the commercial levels, a tone on tone material is applied in order to provide a cohesive vertical expression.

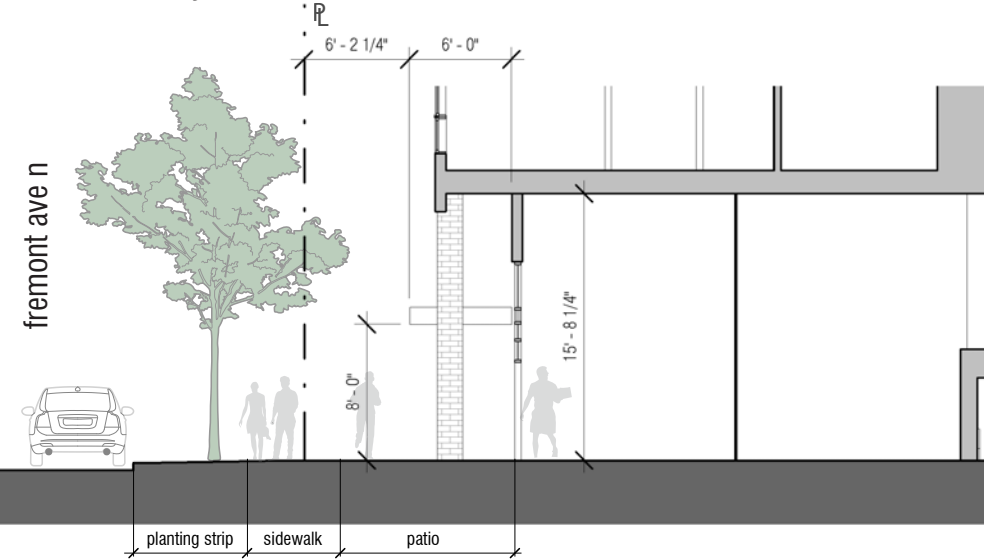
Windows and storefront vary at each commercial façade, relating to the fenestration above while still providing a cohesive expression.

At the corner commercial unit, spillout space has been increased for even greater activity. Additionally this space is distinguished from the others through the use of a wood storefront system.

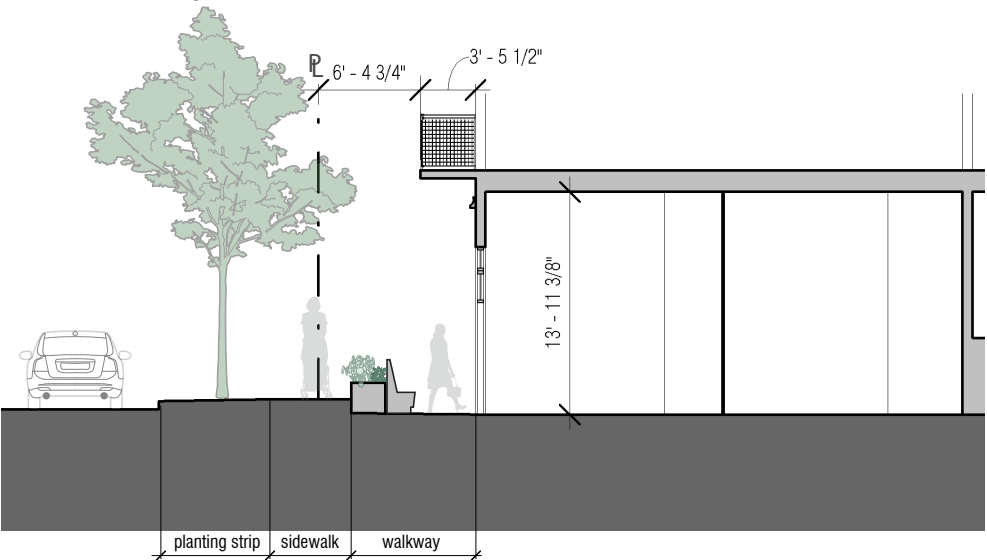
DESIGN GUIDELINES

- CS2-B-2. Connection to the Street
- CS1-C Topography
- PL3-C Retail Edges
- PL2-D-1. Design as Wayfinding
- CS3-A-4. Evolving Neighborhoods
- CS2-C-1. Corner Sites
- PL3-C-3. Ancillary Activities
- PL3-A-2. Common Entries

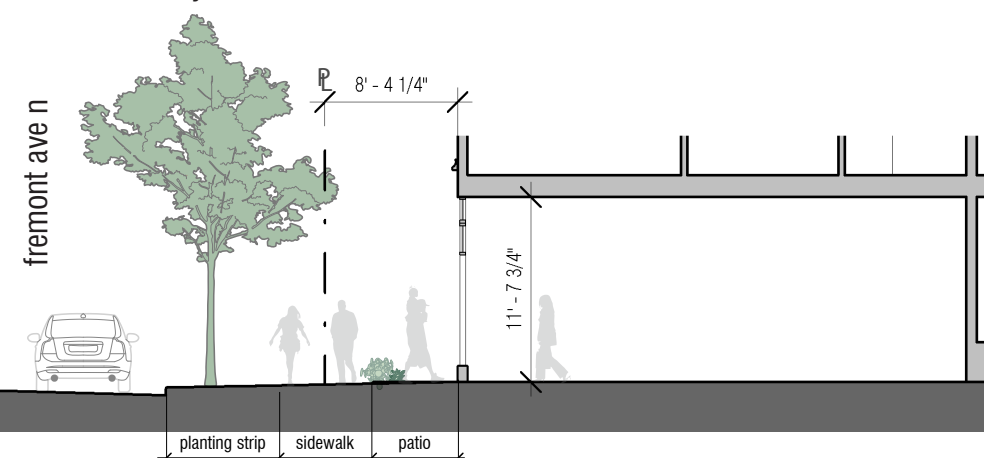
commercial entry 1



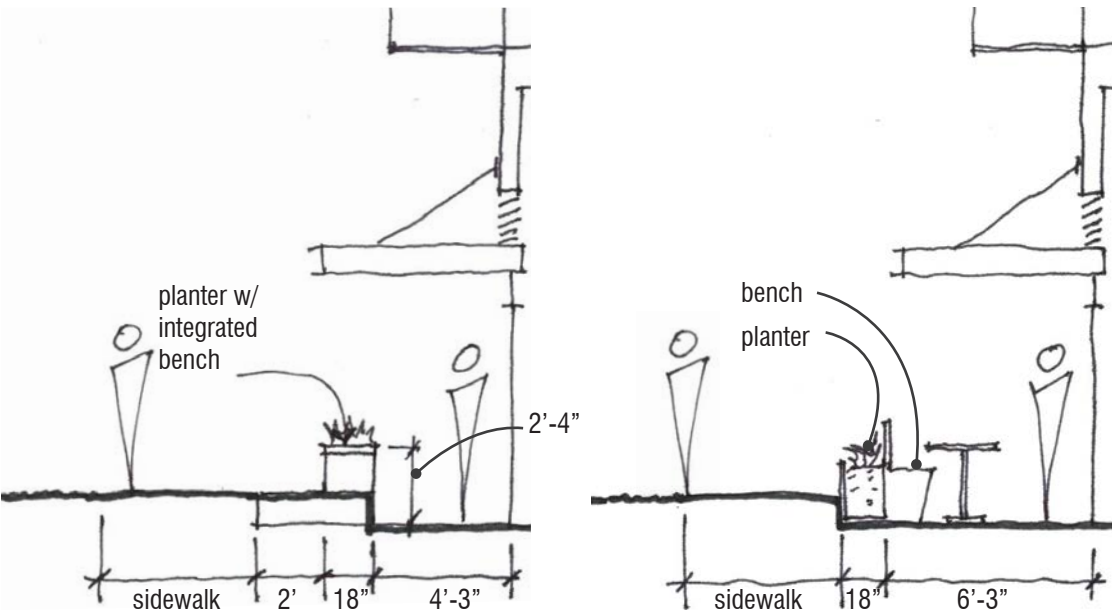
commercial entry 2 & 3



commercial entry 4 + 5



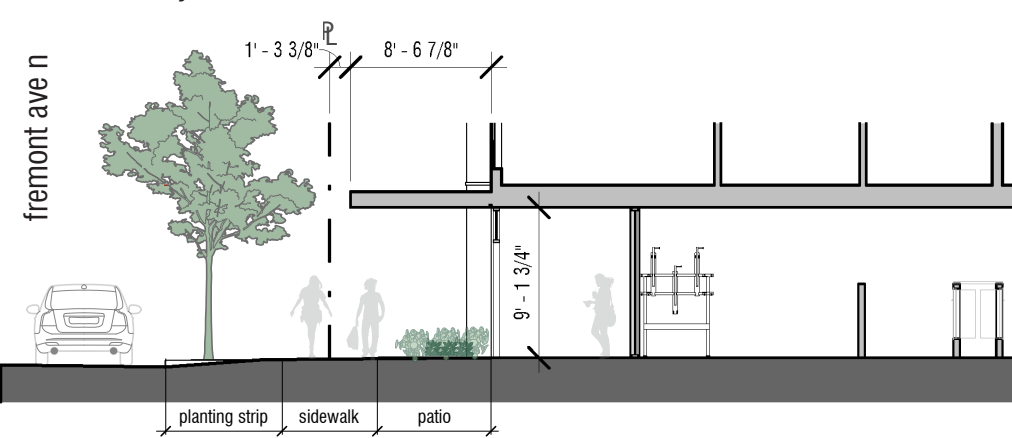
studies @ grade transition



SDOT requires sidewalk facing benches to be setback 2' from sidewalk reducing usable space

planter acts as a buffer at the grade transition, providing maximum usable patio at the commercial space

residential entry



EDG BOARD GUIDANCE

The Board discussed the possible addition of balconies as the design progresses and generally supported the potential inclusion of balconies on the west and south facades that could add activation along the street frontages.

The applicant noted that they would likely limit balconies on the west façade due to privacy concerns along the zone transition. The Board noted that balconies on the west façade could provide valuable amenity to units. They requested any balconies proposed on the west façade be studied to ensure impacts of sight lines, visibility and noise are limited on the adjacent residential properties.

DESIGN RESPONSE

Balconies have been explored as a tenant amenity on the west façade. A combination of balconies, juliets, and larger windows are provided. Balconies have been minimized on the sections of façade closest to the adjacent single family homes.

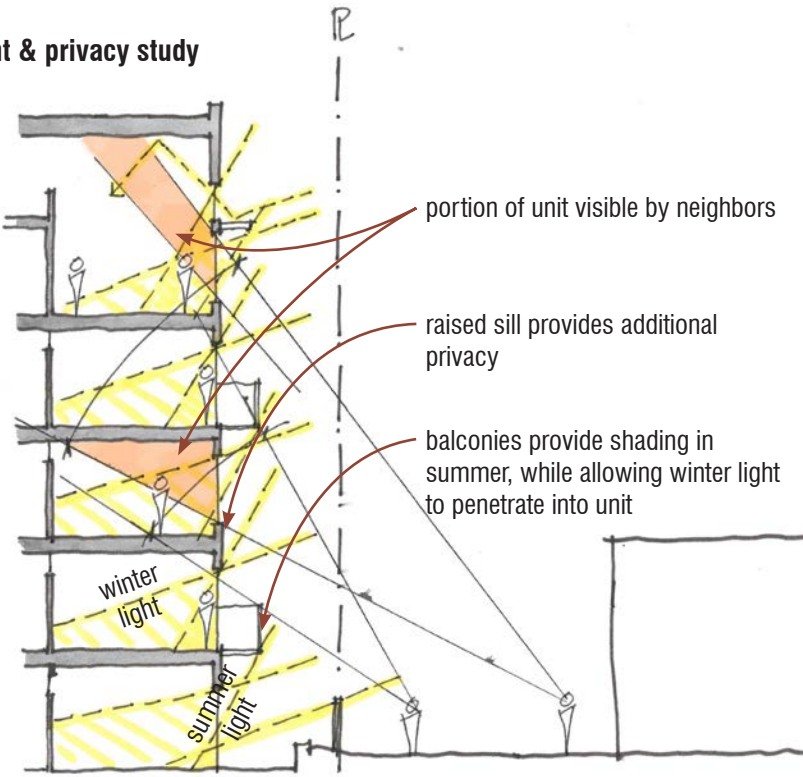
In addition to a valuable amenity for the residents, the balconies and juliets offer relief to the facade.

For residents of our building, raised sills and balconies aid in privacy from neighbors, while increased privacy can also be provided by utilizing top down, bottom up shades.

DESIGN GUIDELINES

DC2-C Secondary Architectural Features
CS2-D-5. Respect for Adjacent Sites

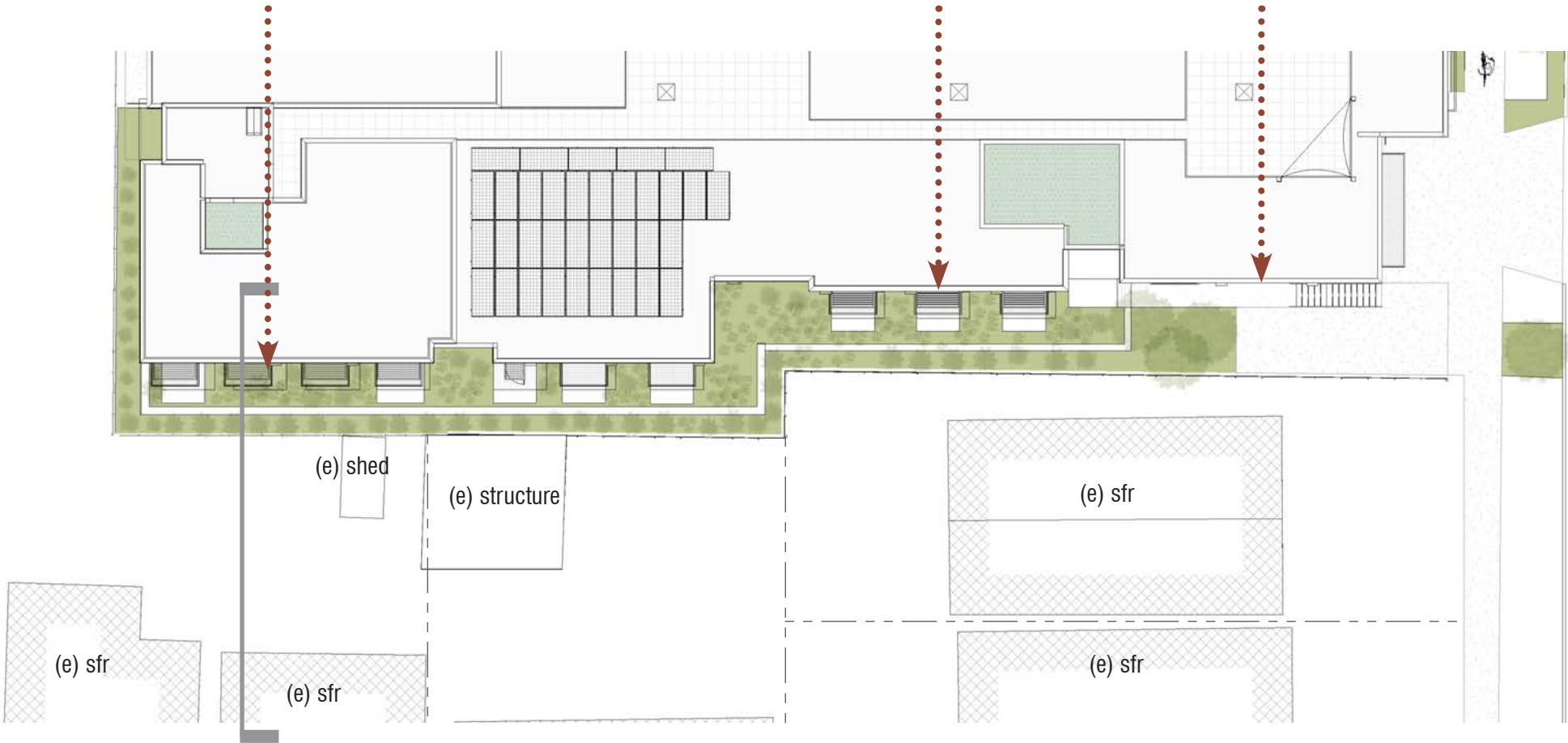
light & privacy study



combination of balconies & windows for increased privacy

combination of balconies / juliets to reduce impact on neighboring properties

reduced balconies @ areas closest to single family homes



EDG BOARD GUIDANCE

The Board agreed that the Option 3 building massing created the best opportunities for development of appropriate and relevant site plan.

The Board specifically supported creation of additional outdoor space by setting back the edges of the corner commercial unit. The Board noted that this creates the opportunity to blur the edges between public and private outdoor space where the grade allows. The eroded edge along the Fremont Ave frontage, as well as the setback along N 44th St, create a porous pedestrian space that emphasize the very visible corner location.

The Board supported the inclusion of streetscape amenities, like the benches shown at the sidewalk edge on the landscape plan on page 44 of the EDG packet, that create opportunities for engagement and activation at the property line.

The Board supported the 'jogs' shown in the site plan along the street frontage at the commercial unit entries. They supported the concept of retaining the grade between the commercial entrances, creating unique entrance areas. They noted that this layout gave scale to the site design and helped to create a 'small shop' feel to the commercial units.

The Board generally supported the establishment of wider planting strips and new street trees along both street frontages, as required by SDOT. The Board suggested including logical breaks in the planting strips for access to/from street parking.

The Board also noted the high pedestrian use of the public realm in this commercial area and suggested a selection of sturdy plantings that will withstand the high use and high visibility of this location as the project landscape design moves forward.

DESIGN RESPONSE

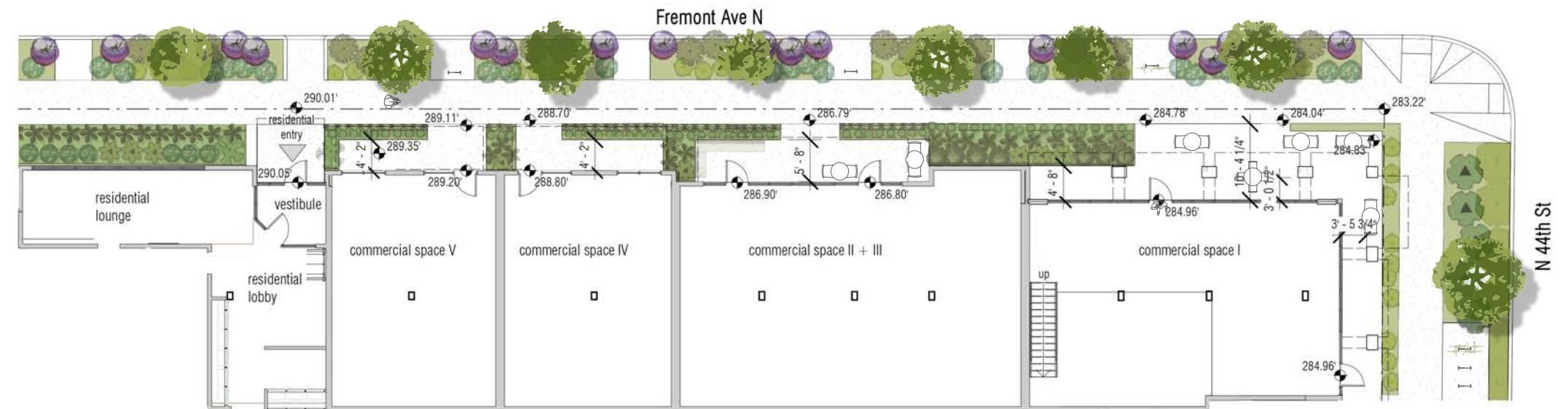
At the corner commercial unit, spillout space has been increased for additional activity. This space is distinguished from the others through the use of a wood storefront system.

Benches directly adjacent to the sidewalk have been provided adjacent to the residential entry with additional benches, bike parking, and planters within the patio spaces in front of each commercial entrance. These unique spaces provide usable exterior areas for each commercial unit while still providing an active and engaged pedestrian experience. The individual patios also help contribute to the pedestrian scale of the structure. Breaks in the planting strips provide easy access from parking to the commercial spaces as well as short term bike parking.

DESIGN GUIDELINES

DC3-A Building-Open Space Relationship
CS2-C-1. Corner Sites

enlarged plan



EDG BOARD GUIDANCE

The Board supported the concept of material application presented in the architectural precedent images associated with Option 3, as shown on page 35 of the EDG packet. The applicant noted that they did not have a specific materials palette in mind but will be developing that now that a specific massing option has been chosen. The Board suggested limiting the materials palette so the façade composition does not become overly complicated. The Board commented that a restrained materials palette could aid the project in relating to the surrounding small-scale context. One Board member suggested that the applicant could tie materials shown in the neighborhood historic context presented in the packet into a contemporary interpretation with a 5-story building.

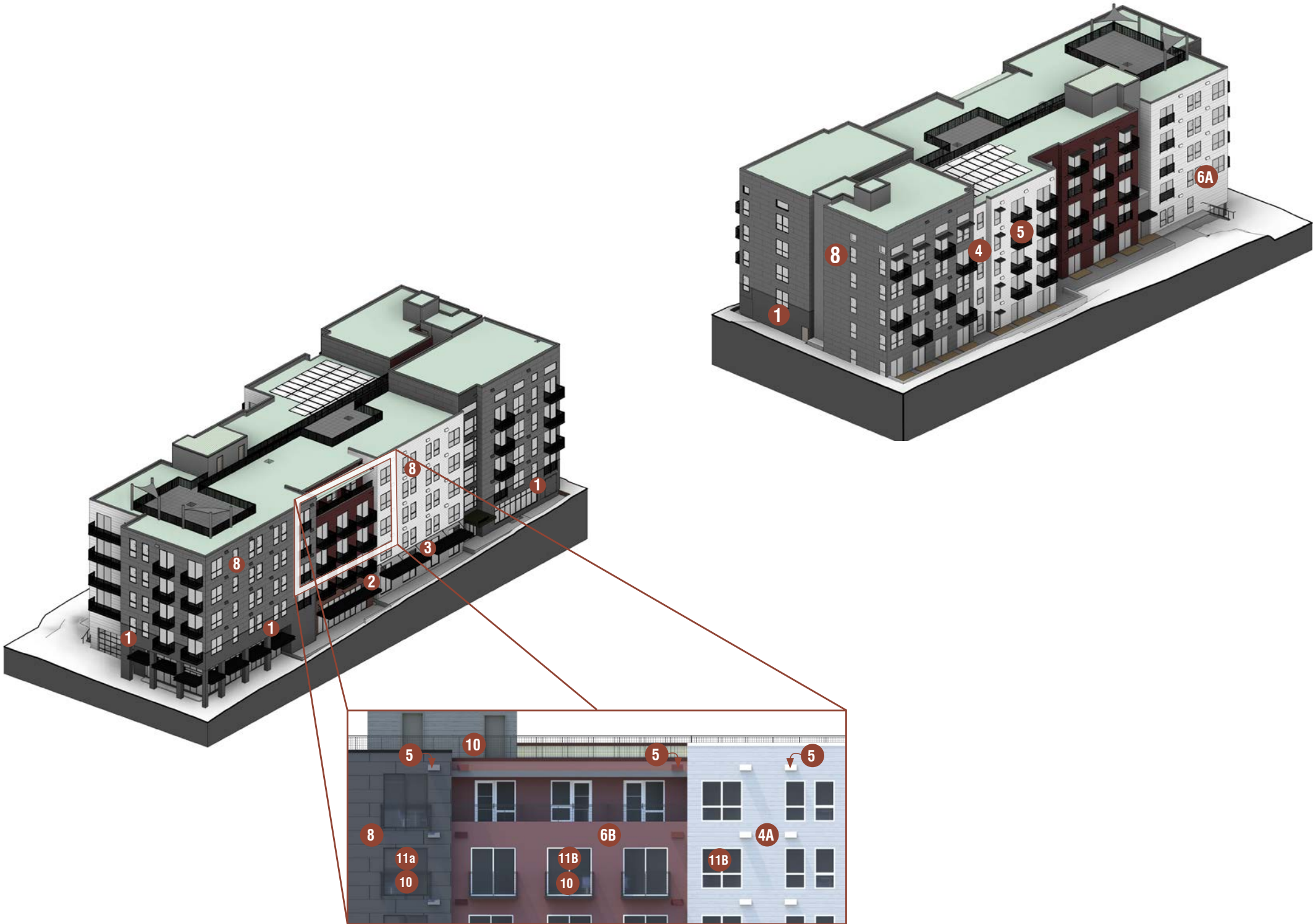
The Board noted the high degree of visibility of all four facades of the building due to the height of the new structure. The Board emphasized the importance of continuing the material concept from the two street-facing facades to the rear of the building where the zone transitions to lower density uses.

DESIGN RESPONSE

The material expression for the project reinforces the vertical modulation and façade composition. Each vertical mass is conceived as a unified form from top to bottom. At the commercial pedestrian frontage, smaller scale bricks are utilized to respond to the human scale and nod to the historic precedent in the neighborhood. Above the bricks, cementitious panels or planks are painted in a similar tone to the brick below in order to contribute to the cohesion of the vertical masses, while offering a residentially scaled material. Three neutral colors are provided – dark grey, white, and brick tone with windows shown in a coordinating finish of either black or white. The vertical expression of the material concepts extend along all four sides, with the brick fronting the pedestrian experience only.

DESIGN GUIDELINES

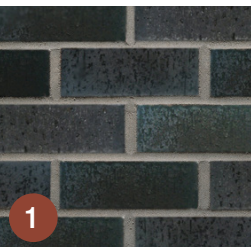
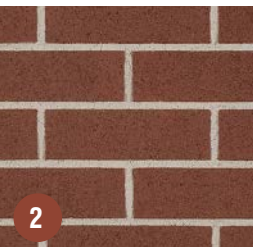
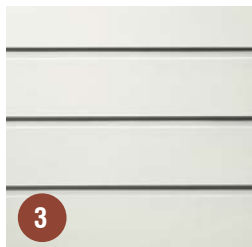
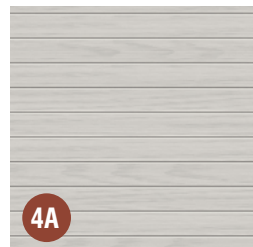
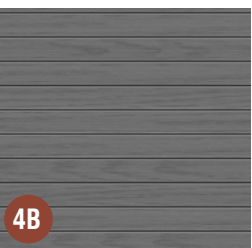
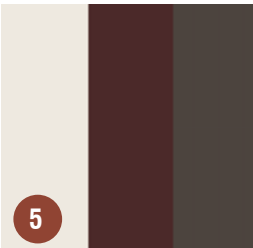
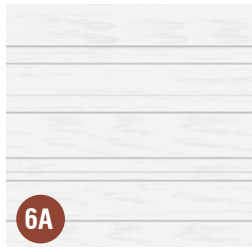
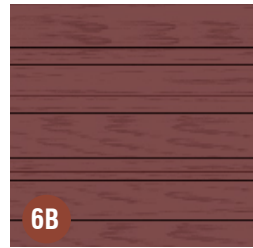
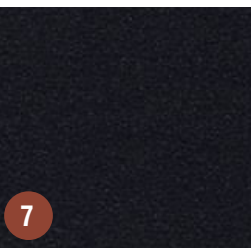

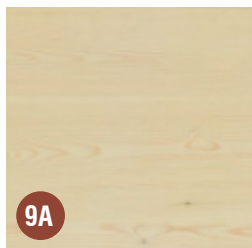


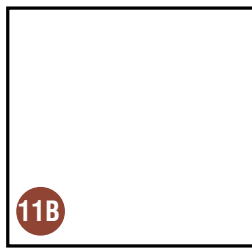
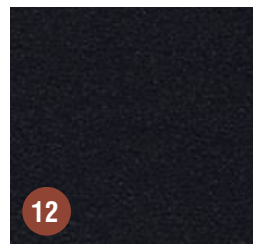
- CS3-A-4. Evolving Neighborhoods
- DC4-A-1. Exterior Finish Materials
- CS2-D-3. Zone Transitions
- DC3-C-2. Amenities/Features



MATERIALS PALETTE



MATERIALS LEGEND

 1 coal creek thin brick	 2 canyon velour red thin brick	 3 alaska velour white thin brick	 4A cementitious board lap siding w/ 8" exposure. siding paint A: Sherwin Williams 7001 Marshmallow
 4B cementitious board lap siding w/ 8" exposure. siding paint B: Sherwin Williams 7023 Requisite Gray	 5 custom metal vent shroud, painted to match	 6A cementitious board lap siding w/ alternating 4" and 8" exposure. siding paint A: Sherwin Williams 7001 Marshmallow	 6B cementitious board lap siding w/ alternating 4" and 8" exposure. siding paint B: Sherwin Williams 2801 Rookwood Dark Red
 7 black steel awnings	 8 cementitious 2' x 8' panels, offset pattern. siding paint: Benjamin Moore 2134-10 Night Horizon	 9A stained wood storefront, fir or similar	 9B storefront, black
 10 standard metal rail w/ wire mesh panels, powder coated black	 11A vinyl windows and doors - black	 11B vinyl windows and doors - white	 12 kawneer black storefront

DEPARTURES			
DEPARTURE REQUEST	LAND USE CODE REQUIREMENT	APPLICANT PROPOSAL	EXPLANATION FOR DEPARTURE

RESIDENTIAL USE AT GROUND FLOOR
SMC 23.47A.005

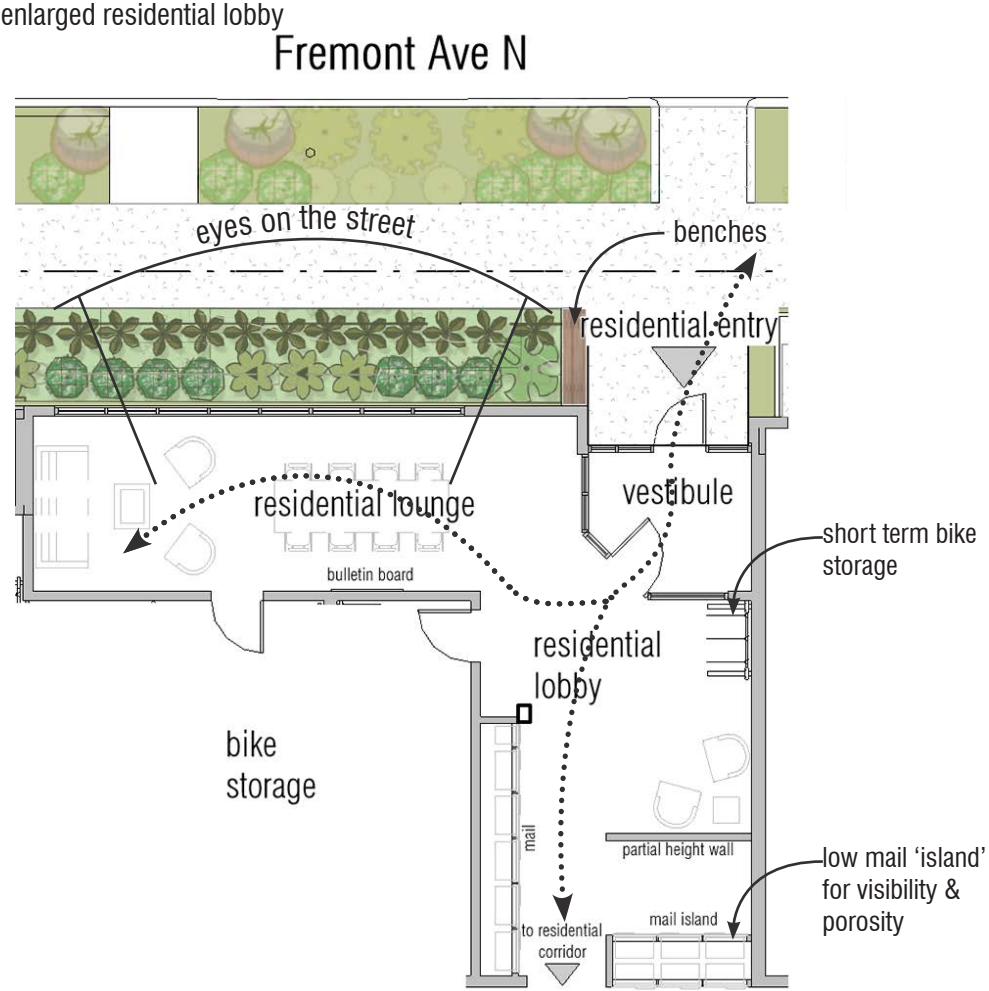
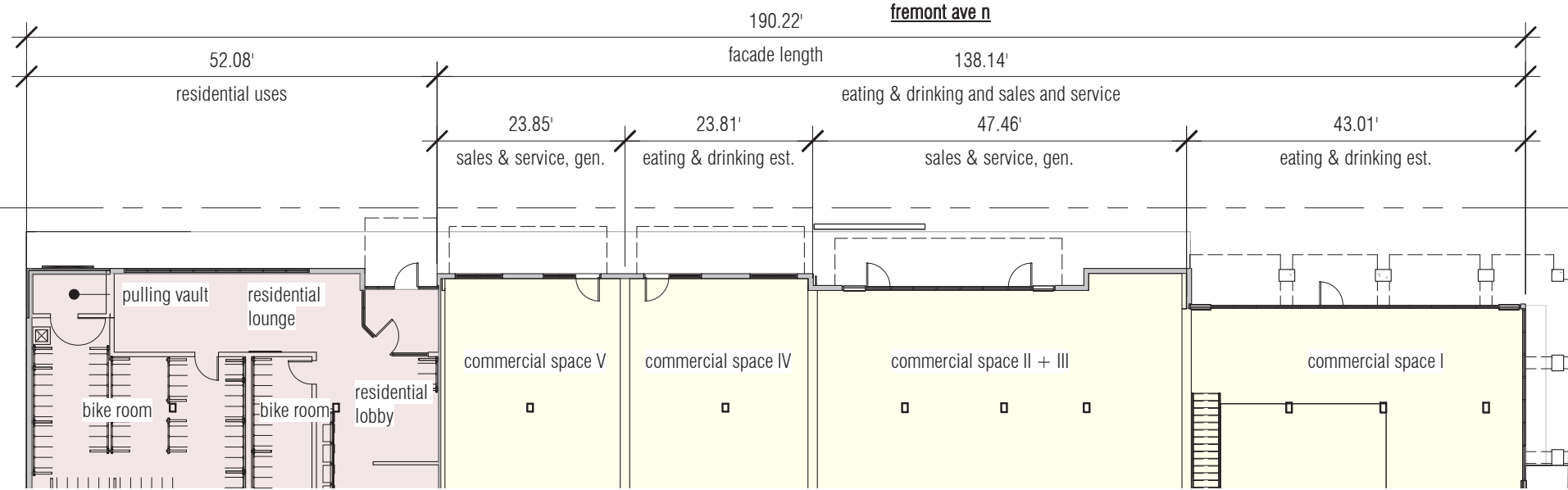
In all nc and c zones, residential uses may occupy, in the aggregate, no more than 20 percent of the street level street facing facade

Total street level street facing facade: 190.17'
20% Residential street level use allowed: 38.03'
Proposed residential street level use: 52.03' (27%)

AMOUNT OF DEPARTURE
14' (7%) over

This departure request allows for a residential entry and amenity space at the northern edge of the property, directly facing the intersection of Allen Place. This provides a new residential node for the neighborhood, providing clear wayfinding to and from amenities such as the grocery store, while also distinguishing the residential entry from the commercial spaces to the south. The increased width of the residential use allows for an activated residential amenity space, providing eyes on the street throughout the day and night.

- SUPPORTED GUIDELINES:
PL2-D-1 Design as Wayfinding
PL3-C-3 Ancillary Activities
PL3-A-2 Common Entries



DEPARTURES			
DEPARTURE REQUEST	LAND USE CODE REQUIREMENT	APPLICANT PROPOSAL	EXPLANATION FOR DEPARTURE

OVERHEAD WHEATHER PROTECTION
SMC. 23.47A.008.C4

Overhead weather protection is required that is over the sidewalk or within a walking area within 10' immediately adjacent to the sidewalk. When provided adjacent to the sidewalk, the covered walking area must be at the same grade or within 18 inches of sidewalk grade and meet WA state requirements for barrier-free access.

Provide 6' deep minimum overhead weather protection within 10' immediately adjacent to the sidewalk at individual patios to improve use of each space by reducing duplicate circulation.

Due to required powerline setbacks, in order to provide overhead weather protection that is code compliant, a secondary walkway/ramp would need to be provided adjacent to the sidewalk

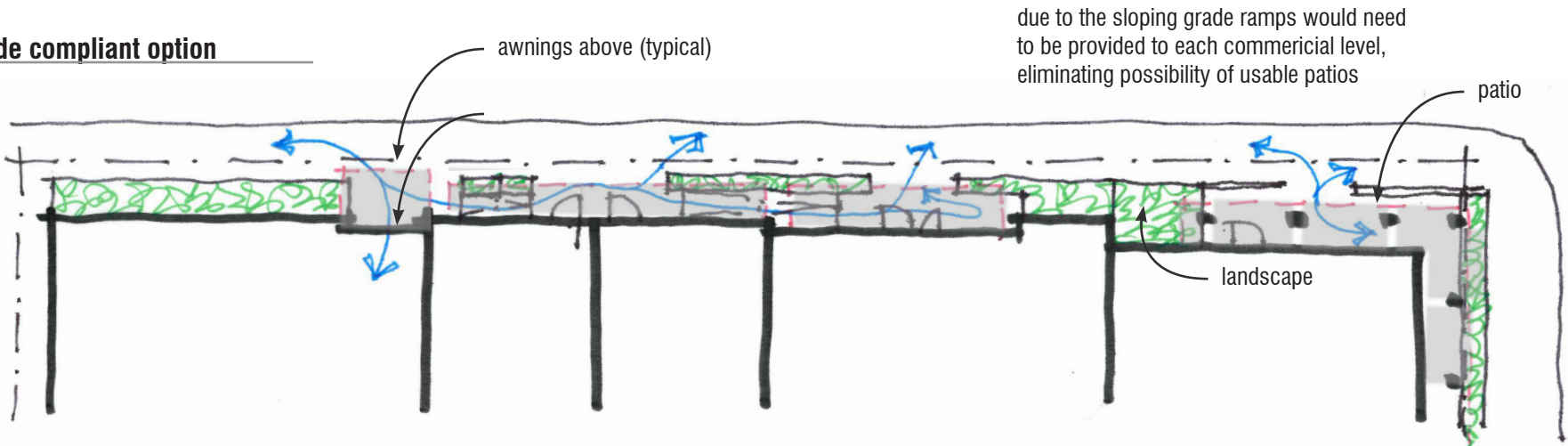
This departure request allows for development of level exterior spaces for use by the retail and commercial spaces along Fremont. The site has a significant grade change and each space is allowed to meet the grade to provide a level, useable and accessible exterior space.

Overhead weather protection is provided in compliance with the intent of the code but is located over the outdoor spaces along Fremont for pedestrians, visitors and shoppers to pause and enjoy the connection between the street and commercial activities.

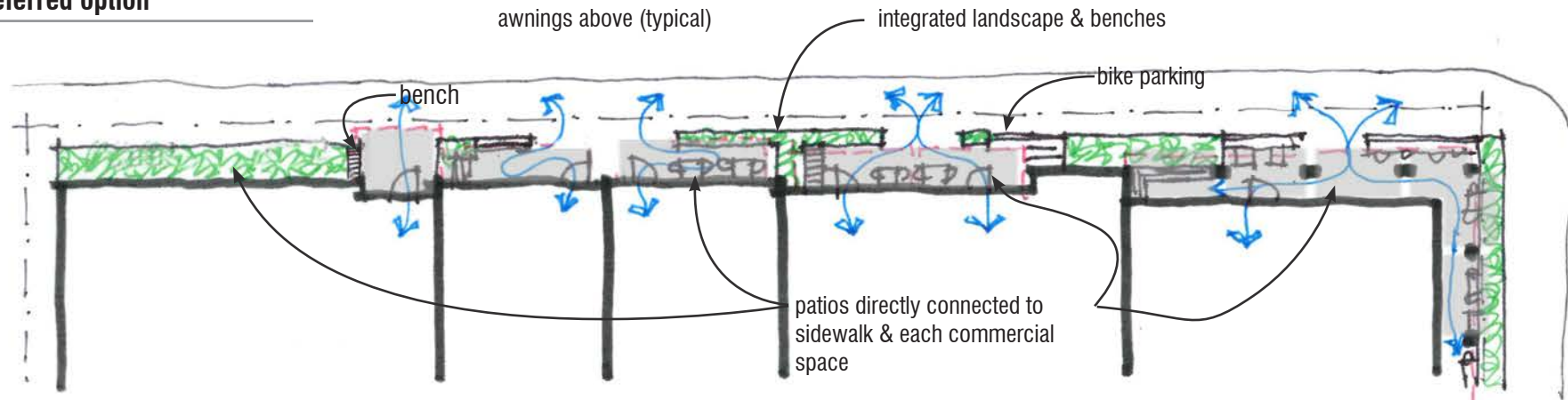
This proposal provides a better connection to the sidewalk edge.

SUPPORTED GUIDELINES:
CS2-B-2 Connection to the Street
CS1-C Topography
PL3-C Retail Edges

code compliant option



preferred option





design guideline key

CS1-C Topography
CS2-C-1 Corner Sites
CS2-B-2 Connection to the Street
CS2-D-3 Zone Transitions
CS2-D-4 Massing Choices
CS2-D-5 Respect for Adjacent Sites
DC2-A-2 Reducing Perceived Mass
DC2-C Secondary Architectural Elements
DC4-A-1 Exterior Finish Materials
PL2-D-1 Design as Wayfinding
PL3-A-2 Ensemble of Elements
PL3 Street-Level Interaction
PL3-C Retail Edges

material legend

1 coal creek thin brick	5 custom metal vent shroud, painted to match	7 black steel awnings	11A vinyl windows and doors - black
2 canyon velour red thin brick	6A cementitious board lap siding w/ alternating 4" and 8" exposure. siding paint A: Sherwin Williams 7001 Marshmallow	8 cementitious 2' x 8' panels, offset pattern siding paint: Benjamin Moore 2134-10 Night Horizon	11B vinyl windows and doors - white
3 alaska velour white thin brick	6B cementitious board lap siding w/ alternating 4" and 8" exposure. siding paint B: Sherwin Williams 2801 Rookwood Dark Red	9A stained wood storefront, fir or similar	12 Kawneer black storefront
4A cementitious board lap siding w/ 8" exposure. siding paint A: Sherwin Williams 7001 Marshmallow		9B storefront, black	
4B cementitious board lap siding w/ 8" exposure. siding paint B: Sherwin Williams 7023 Requisite Gray		10 standard metal rail w/wire mesh panels, powder coated black	



design guideline key CS1-C Topography CS2-C-1 Corner Sites CS2-B-2 Connection to the Street CS2-D-3 Zone Transitions CS2-D-4 Massing Choices CS2-D-5 Respect for Adjacent Sites DC2-A-2 Reducing Perceived Mass DC2-C Secondary Architectural Elements DC4-A-1 Exterior Finish Materials PL2-D-1 Design as Wayfinding PL3-A-2 Ensemble of Elements PL3 Street-Level Interaction PL3-C Retail Edges			
material legend			
1	coal creek thin brick	5	custom metal vent shroud, painted to match
2	canyon velour red thin brick	6A	cementitious board lap siding w/ alternating 4" and 8" exposure. siding paint A: Sherwin Williams 7001 Marshmallow
3	alaska velour white thin brick	6B	cementitious board lap siding w/ alternating 4" and 8" exposure. siding paint B: Sherwin Williams 2801 Rookwood Dark Red
4A	cementitious board lap siding w/ 8" exposure. siding paint A: Sherwin Williams 7001 Marshmallow	7	black steel awnings
4B	cementitious board lap siding w/ 8" exposure. siding paint B: Sherwin Williams 7023 Requisite Gray	8	cementitious 2' x 8' panels, offset pattern siding paint: Benjamin Moore 2134-10 Night Horizon
		9A	stained wood storefront, fir or similar
		9B	storefront, black
		10	standard metal rail w/wire mesh panels, powder coated black
		11A	vinyl windows and doors - black
		11B	vinyl windows and doors - white
		12	Kawneer black storefront



design guideline key

CS2-C-1 Corner Sites
CS2-D-3 Zone Transitions
CS2-D-4 Massing Choices
CS2-D-5 Respect for Adjacent Sites
DC2-A-2 Reducing Perceived Mass
DC2-C Secondary Architectural Elements
DC4-A-1 Exterior Finish Materials
PL2-D-1 Design as Wayfinding
PL3-A-2 Ensemble of Elements
PL3 Street-Level Interaction
PL3-C Retail Edges

material legend

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4B cementitious board lap siding w/ 8" exposure. siding paint B: Sherwin Williams 7023 Requisite Gray		10 standard metal rail w/wire mesh panels, powder coated black	

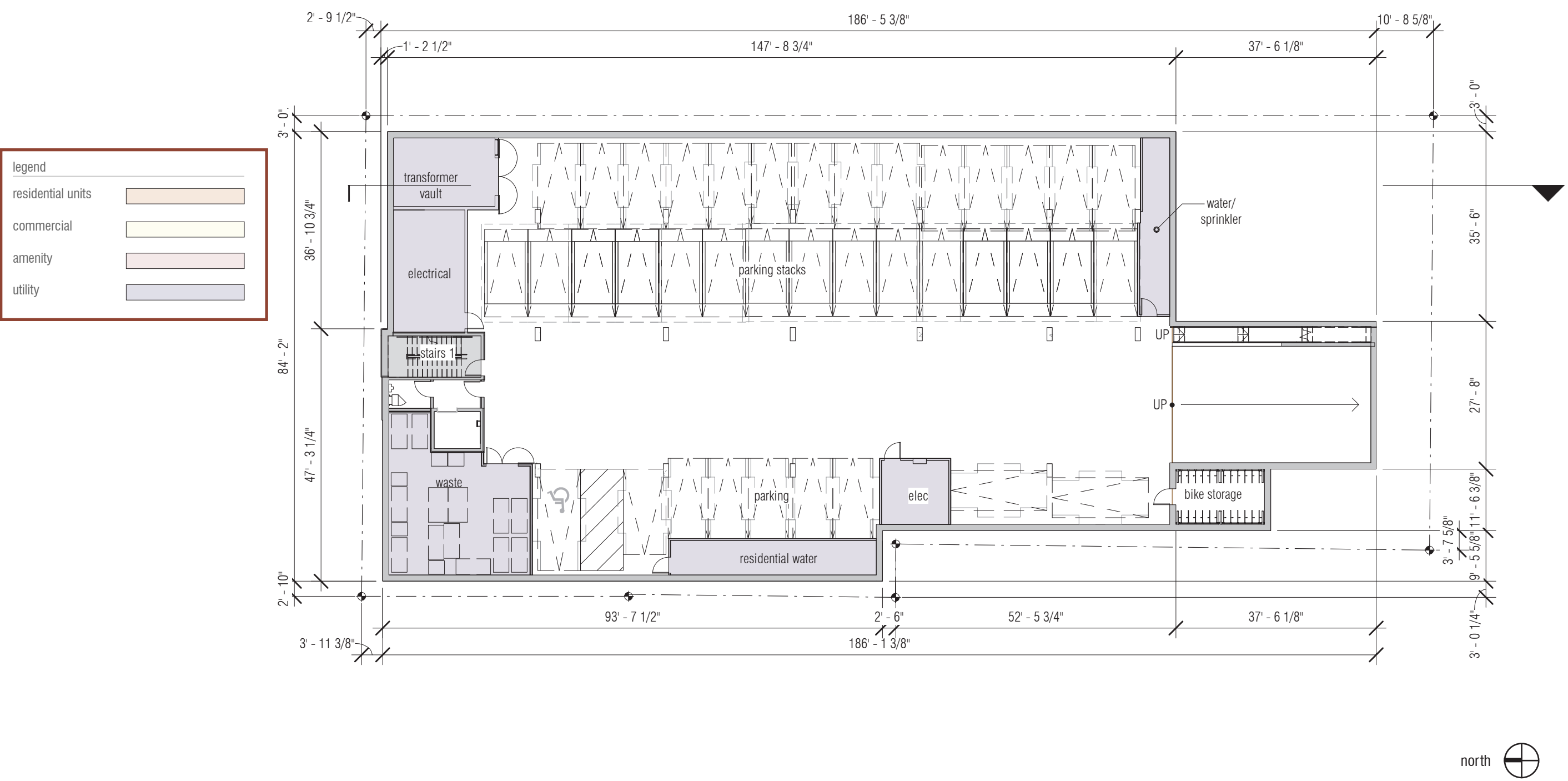


design guideline key

- CS2-C-1 Corner Sites
- CS2-D-3 Zone Transitions
- CS2-D-4 Massing Choices
- CS2-D-5 Respect for Adjacent Sites
- DC2-A-2 Reducing Perceived Mass
- DC2-C Secondary Architectural Elements
- DC4-A-1 Exterior Finish Materials
- PL2-D-1 Design as Wayfinding
- PL3-A-2 Ensemble of Elements
- PL3 Street-Level Interaction
- PL3-C Retail Edges

material legend

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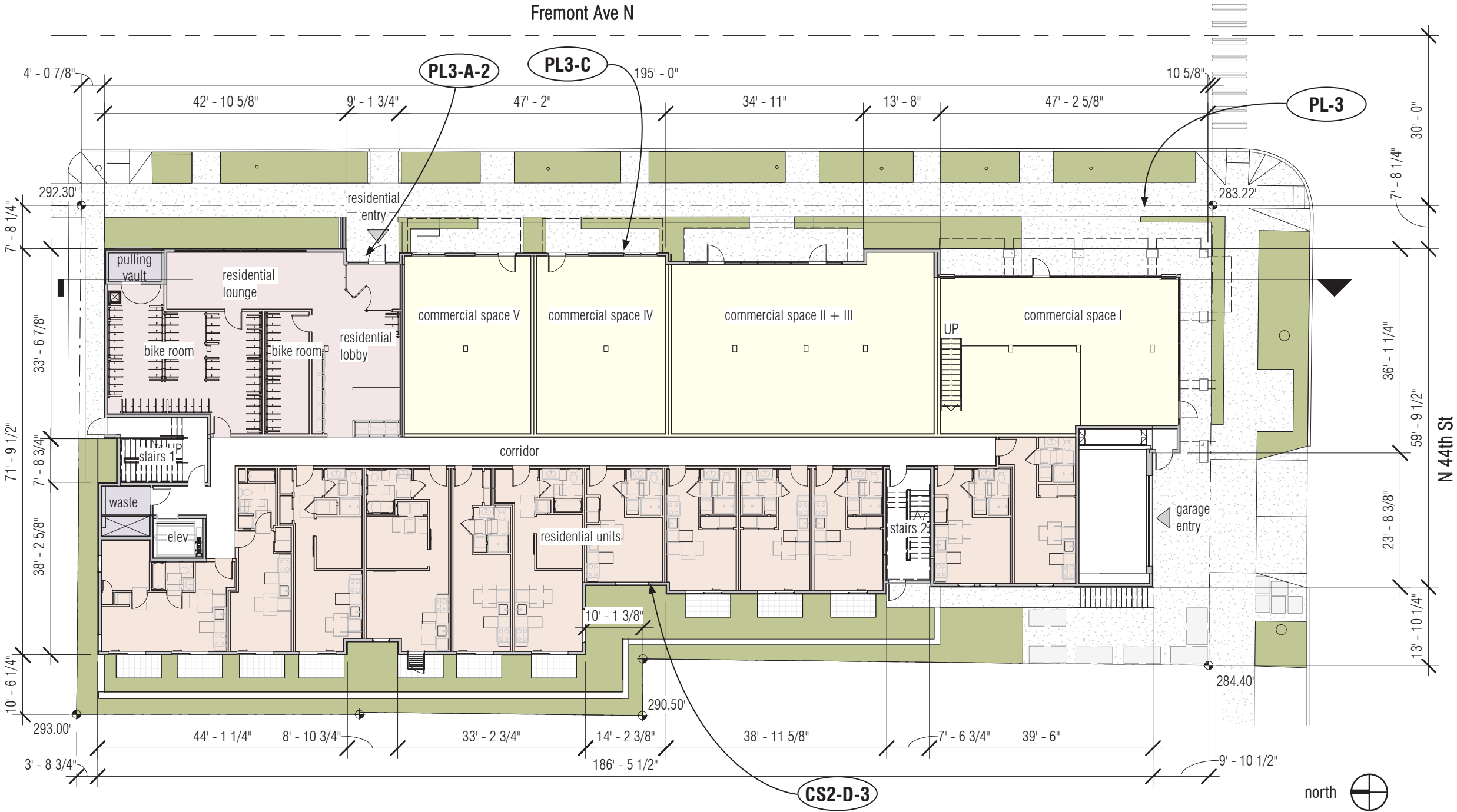


legend

residential units	
commercial	
amenity	
utility	

design guideline key

CS2-C-1 Corner Sites
CS2-D-3 Zone Transitions
CS2-D-4 Massing Choices
CS2-D-5 Respect for Adjacent Sites
DC2-A-2 Reducing Perceived Mass
DC2-C Secondary Architectural Elements
DC4-A-1 Exterior Finish Materials
PL2-D-1 Design as Wayfinding
PL3-A-2 Ensemble of Elements
PL3 Street-Level Interaction
PL3-C Retail Edges



10.0 ARCHITECTURAL DESIGN | second-fifth floor plan (typ.)

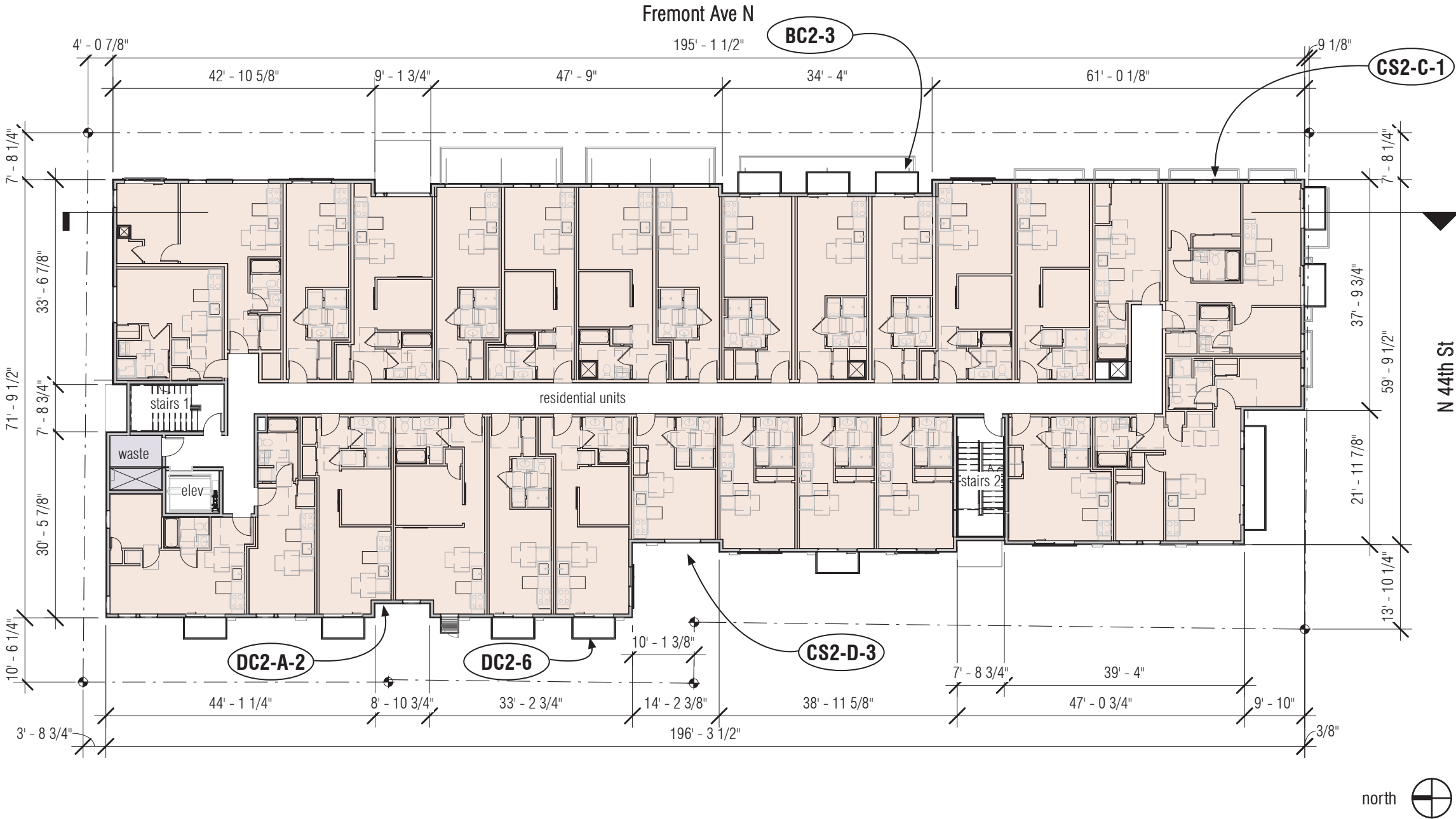
legend

residential units

commercial

amenity

utility



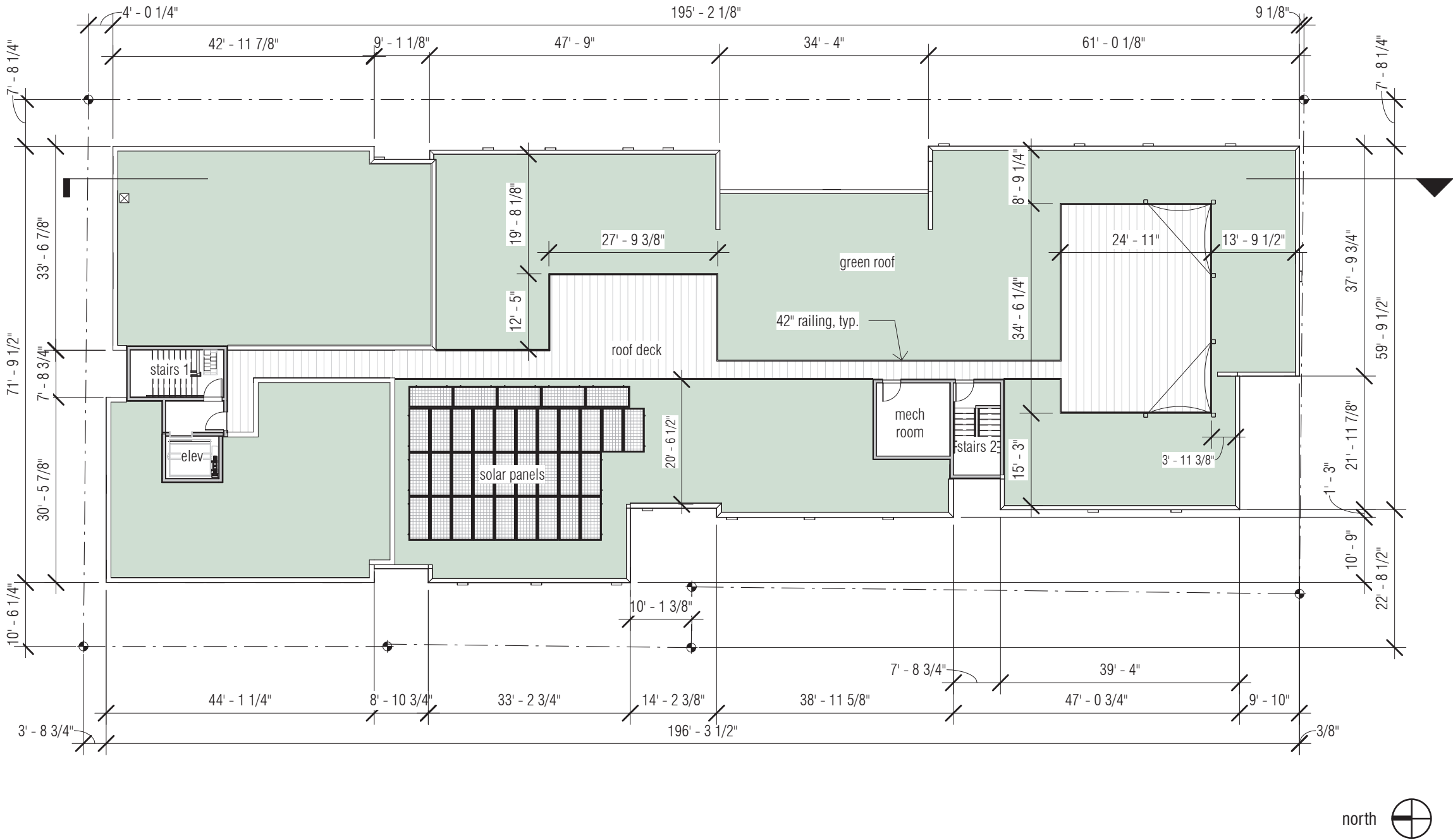
legend

residential units

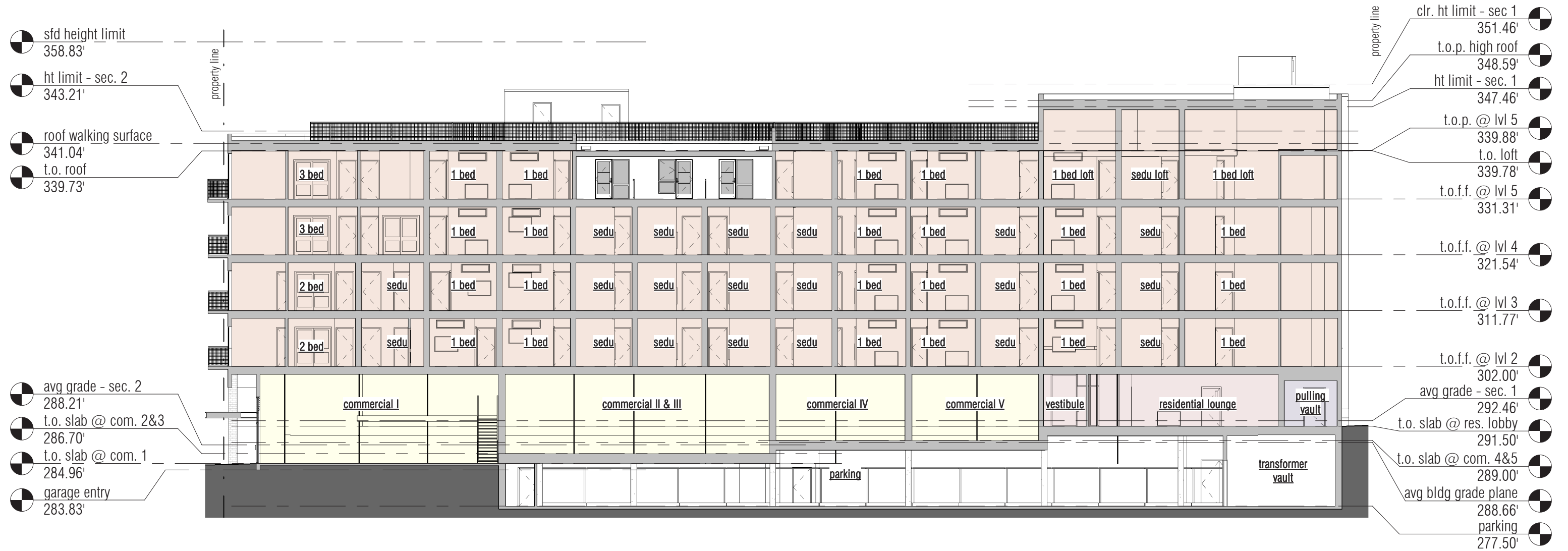
commercial

amenity

utility



10.0 ARCHITECTURAL DESIGN | section at residential lobby







GARDEN APARTMENT LANDSCAPE CONCEPT:

We are exploring providing private patios for the residents on the north-west corner of the building, which will receive good sun exposure from the south and west. We are looking at providing them with green vertical trellis separation, using natural cedar to add warmth and texture.

ROOFTOP LANDSCAPE CONCEPT AND AMENITIES:

The rooftop will be over 80% landscaped, with areas for residents to sit, relax and work from home. We will provide areas for individual and group seating, with an emphasis on the southern roof area, which has views to the Olympics, Downtown, and the Cascades. This will also direct the most activity towards Fremont Ave with its higher intensity zone, providing more privacy for the lowrise zone to the west.



44th



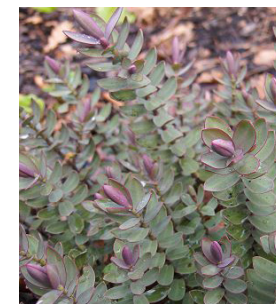
Fremont Ave



Roof



Shade Gardens





LANDSCAPE PLAN - GROUND PLANE





LANDSCAPE PLAN - ROOFTOP



lighting strategy

The lighting strategy seeks to provide illumination for wayfinding and security while being mindful of the residential neighbors. The types of fixtures selected focus light downward, targeting specific areas and highlighting landscape, rather than spilling light upwards.



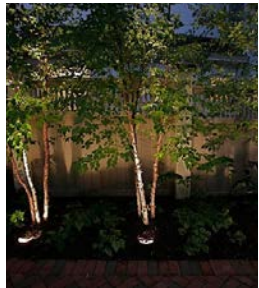
1 linear downlight



2 downlight



3 wall sconce



4 tree lighting



5 down & up sconces

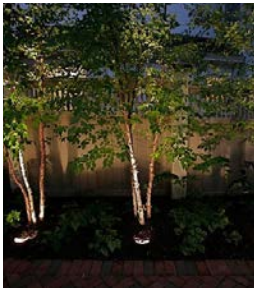


lighting strategy

The lighting strategy seeks to provide illumination for wayfinding and security while being mindful of the residential neighbors. The types of fixtures selected focus light downward, targeting specific areas and highlighting landscape and activity spaces rather than spilling light upwards.



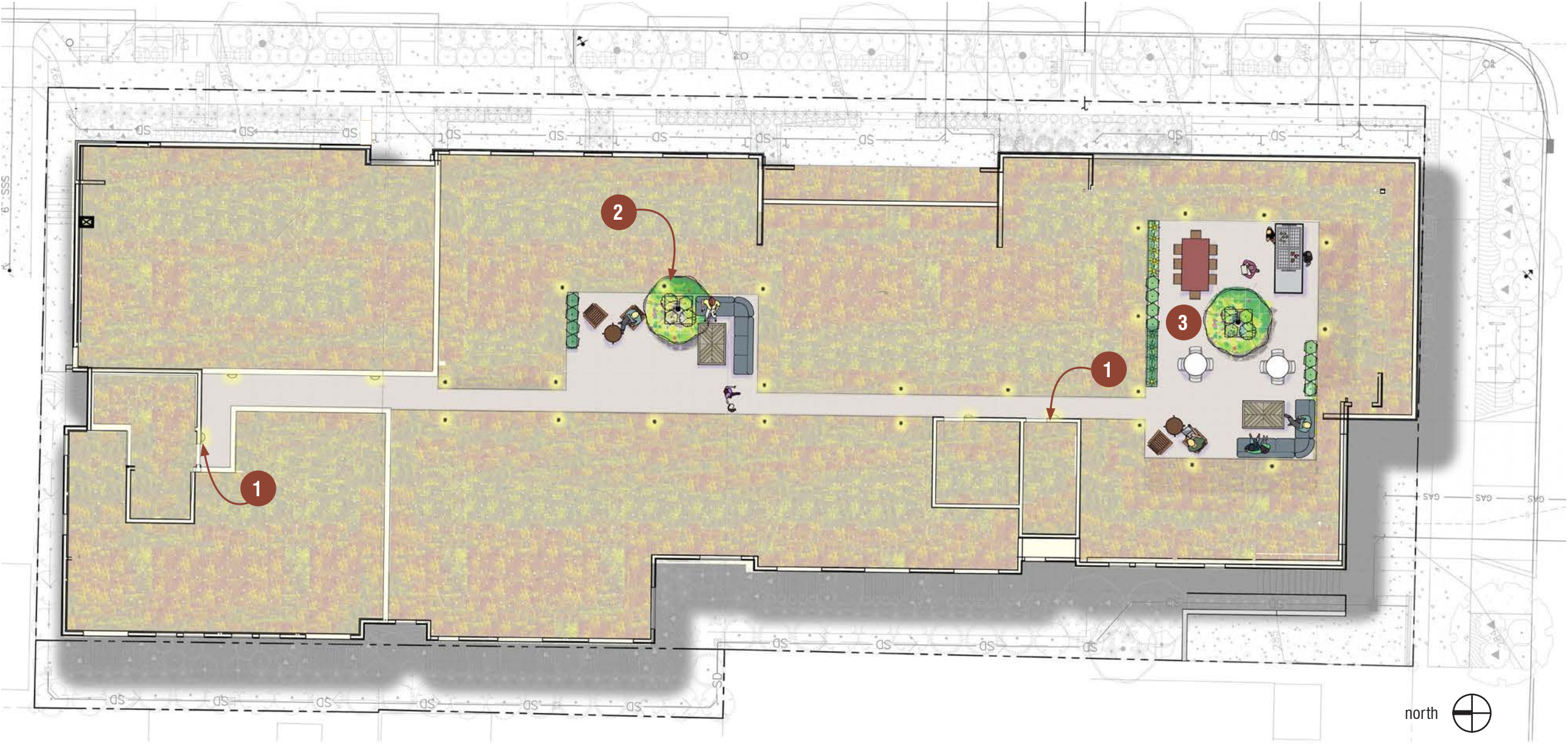
1 wall sconce



2 tree lighting



3 string lighting





APPENDIX



August 22, 2023

Molly O'Donnell
Atelier Drome

Dear Molly,

Thank you for submitting to SPU the solid waste service plans for **4401 Fremont Ave N.**, subject to review by the Seattle Department of Construction and Inspections (SDCI) as Permit # **3039107-LU**.

SPU has reviewed your solid waste plans and approves the following conditions:

118 units (66 SEDUs + 52 apartments) + 4,200 ft² retail

- Building staff will stage dumpsters in designated solid waste staging area off N. 44th St.
- Building staff will stage food+yard waste carts in the planting strip of N. 44th St.
- SPU drivers will come on-site to collect all dumpsters from the designated staging area.
- Residents will have on-floor access to garbage and recycle via chutes and food+yard waste via container.

Multifamily Services (66 SEDUs + 52 apartments)

- Garbage: 5 – 1.5yd dumpsters – serviced 2x/week
- Recycle: 4 – 1.5yd dumpsters – serviced 2x/week
- Food waste: 3 – 96g carts – serviced 1x/week

Commercial Services (retail)

- Garbage: 1 – 2yd dumpsters – serviced 2x/week
- Recycle: 1 – 2yd dumpsters – serviced 2x/week
- Food waste: 2 – 96g carts – serviced 1x/week

Please work with the assigned SDCI zoning reviewer to adopt this plan. **If the attached drawings differ from the Construction drawings, you will need to update your permit application to consistently reflect the contents of this letter or seek re-review and re-approval from SPU Solid Waste Development Review of the proposed changes to solid waste storage and access.**

Call SPU Customer Service at 206.684.7665 one month prior to occupancy to request residential solid waste services.

Call WM at 855.894.9447 one month prior to occupancy to request commercial solid waste services.

Sincerely,

Adam Maurer
Seattle Public Utilities Solid Waste Division
206.300.9613
adam.maurer@seattle.gov

700 Fifth Avenue | PO Box 34018 | Seattle, WA 98124-4018 | 206-684-3000 | seattle.gov/utilities

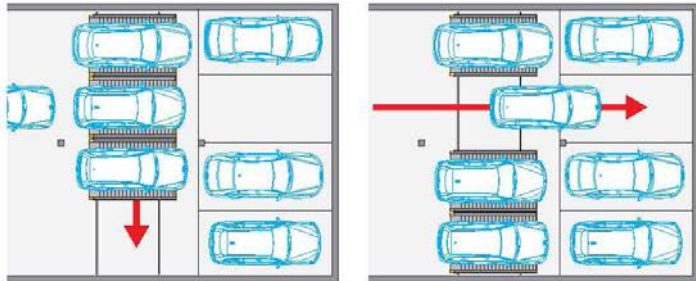
Data Sheet

WÖHR PARKING PLATFORM 501

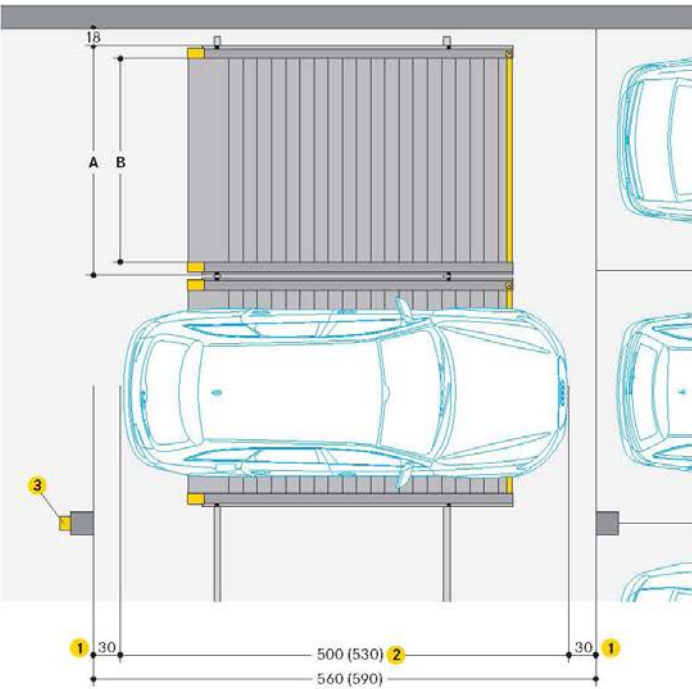


Electric rail on the ceiling

- Laterally shifting parking platforms
- Platform load options:
 - max. 2000 kg, load per wheel 500 kg
 - max. 2600 kg, load per wheel 650 kg
- Every parking platform is given its own electric rail



Length and width measurements



Parking Platform A	Clear platform width B
226	197
236	207
246	217
256	227
266	237
276	247
286	257

Platform width of at least 217 cm is recommended.

- 30 cm safety clearance as per DIN EN 14010:
 - between the front or rear bumper of a car parked on the parking platform
 - between fixed parts of the surroundings or another car
- Car length 500 cm = length measurement 560 cm
Car length 530 cm = length measurement 590 cm
The length measurement can only be reduced if the max. car parking space is shortened or a light barrier is used
- Operation device

Note

The entire system and its movement sequences must be visible from the operation device.

Dimensions

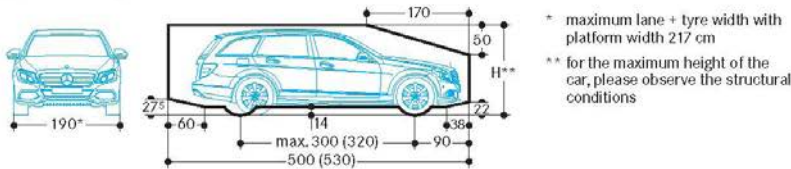
- all dimensions specified are the minimum, finished dimensions
- tolerances must be taken into consideration
- the evenness of the floor has priority as per DIN 18202
- all dimensions are given in cm

WÖHR Autoparksysteme GmbH | Ölgrabenstr. 14 | 71292 Friolzheim | Germany
+49 [0] 7044 46-0 | +49 [0] 7044 46-149 | info@woehr.de | woehr.de

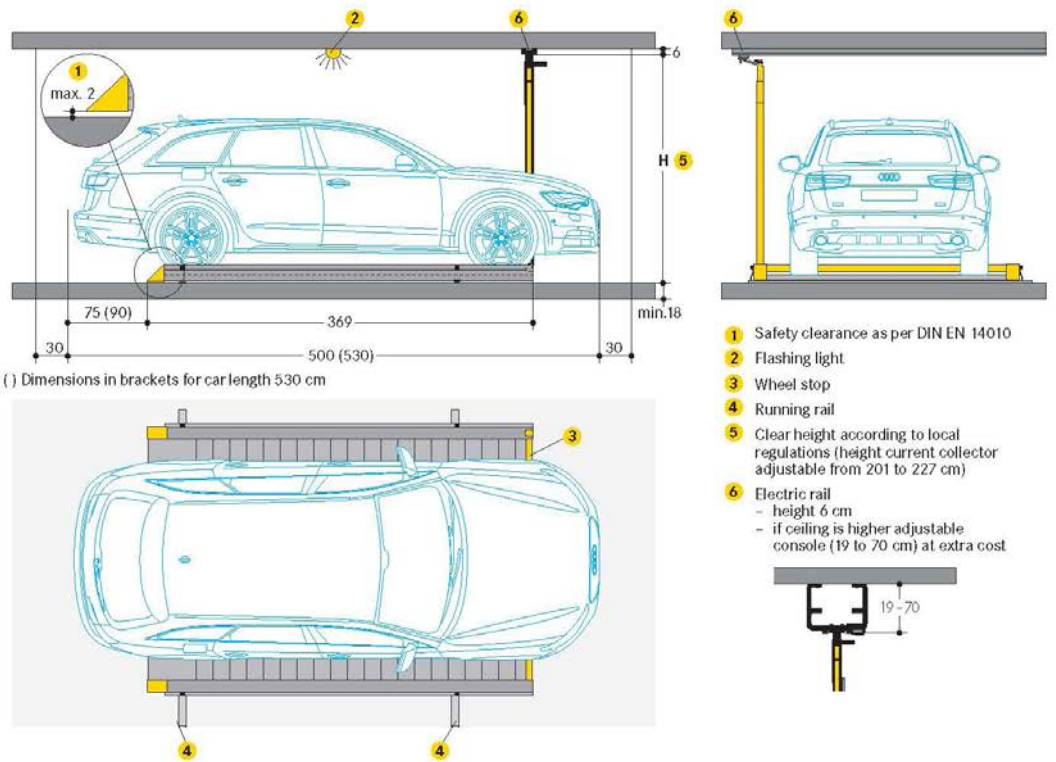
WÖHR PARKING PLATFORM 501 (Electric rail on the ceiling) | 12.2022 | C027-5285

Page 2 of 7

Clearance profile



Electric rail on the ceiling



Track and floor details (electric rail on the ceiling)

Track loading due to a moving traffic load:

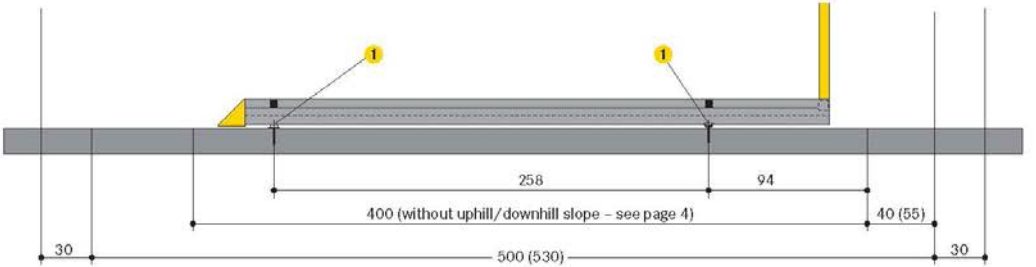
- PARKING PLATFORM 501-2.0:
 - max. 7,5 kN per track roller
- PARKING PLATFORM 501-2.6:
 - max. 7,6 kN per track roller

Installation of the running rails:

- meter markers are to be permanently attached by the customer
- do not use cast asphalt!
- after bringing in the screed, the track rails are secured with wooden screws and plastic dowels
- level as per DIN 18202, table 3, line 3
- no expansion gaps or building separation gaps are permitted in the area of the track system

For any subsequent installation of the parking platforms, in the parking area, an additional screed is to be taken into account by the customer, depending on the floor evenness. The measures are decided after a levelling.

Running rail



Evenness and tolerances

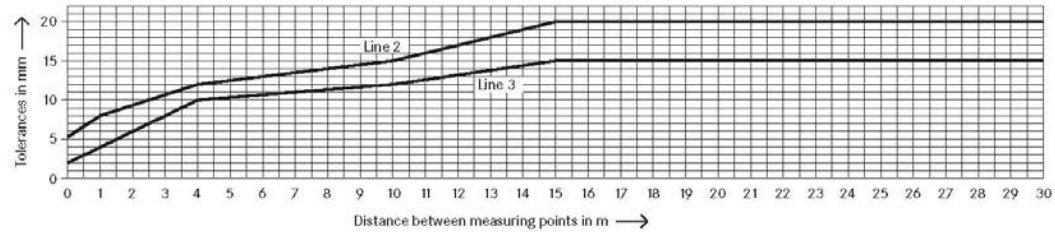
According to DIN EN 14010, the safety clearance between the outer lower edges of the parking platforms and the garage floor must not exceed 2 cm.

In order to comply with the requirement and to obtain the necessary floor level, the tolerances of the evenness of the finished floor cannot be exceeded according to DIN 18202, table 3, line 3. Therefore, exact levelling of the floor by the customer is essential.

Abstract from DIN 18202, table 3

Column	1	2	3	4	5	6
Line	Reference	Vertical measurements as limits in mm with measuring points distances in m to*				
		0,1	1	4	10	15
2	Unfinished to surface of covers, subconcrete and subsoils for higher demands, e.g. as foundation for cast plaster floor, industrial soils, paving tiles and slabstone paving, compound floor paving. Finished surfaces for minor purposes, e.g. warehouses, cellars.	5	8	12	15	20
3	Finished grounds, e.g. floor pavement serving as foundation for coverings. Coverings, tile coverings, PVC flooring and glued coverings.	2	4	10	12	15

* Intermediate values are to be taken out the diagram and must be rounded-off to mm.



Measuring points: finished floor (electric rail on the ceiling)

The evenness of a surface is tested by gauge measurements between two measuring points on the surface irrespective of its position and inclination.

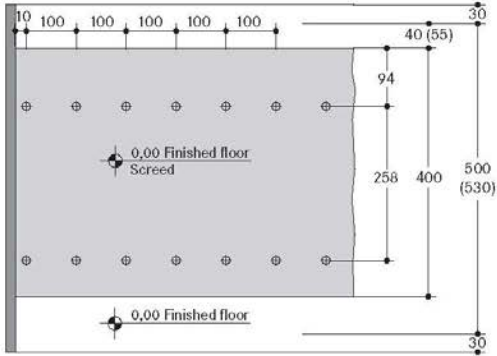
When inspected by WÖHR only a random test is made in areas which appear to be uneven.

For the uniform verification of the evenness of the floor surface, the measuring points are defined as surveying and control points.

For the finished floor:

- Measuring points in the longitudinal distance of 100 cm for checking the unevenness as per DIN 18202, table 3, line 3 and / or as per diagram

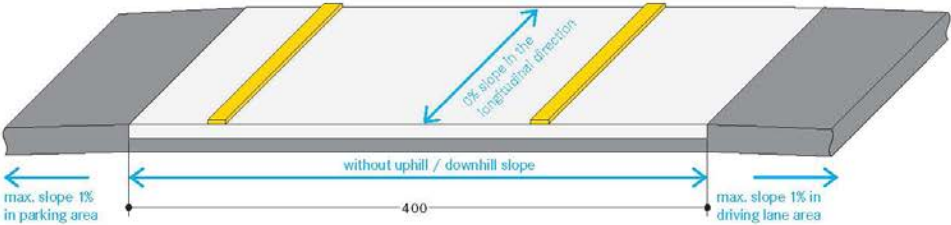
() Dimensions in brackets for car length 530 cm



Drainage

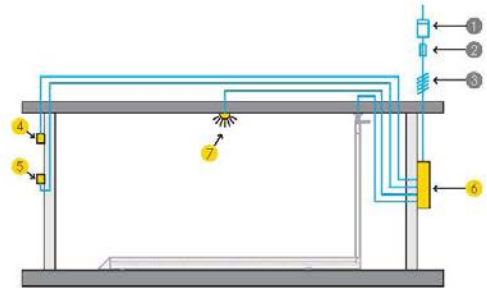
As per the requirements of DIN EN 14010 and the accident prevention regulations, the distance between the lower edge of the parking platform and finished floor is max. 2 cm.

Sloping in the parking area is prohibited. For water drainage, the areas outside the parking platform area are to be provided with a slope of max. 1%.



Electrical specifications (electric rail on the ceiling)

Installation diagram



Cabling preparation to be performed by the customer:

- up to the main switch to be in place prior to starting the installation operations
- connection to the main switch during installation
- system functional check testing can be performed by WÖHR together with the electrician provided by the customer
- if requested at a later date, functional check testing can be performed by WÖHR at extra-cost

To be performed by the customer

Item	Quantity	Description	Position	Recurrence
1	1 piece	power meter	in the feed cable	1 x per control
2	1 piece	fuse protection or automatic circuit breaker compliant to DIN VDE 0100 part 430: 3 x 16 A slow blow	in the feed cable	1 x per control
3	based on site conditions	compliant to local power supply regulations 3 phases + N + PE* 230/400 V, 50 Hz	feed cables to main switch	1 x per control

* DIN VDE 0100 sections 410 and 430 (no permanent load) 3 phases + N+ PE (three phase current)

Scope of delivery by WÖHR (unless otherwise specified)

Item	Description
4	Lockable main switch
5	Operation device
6	Control unit (position must be defined object-dependently) Space demand (height x width x depth): - 1 to 8 parking platforms: 800 x 1000 x 210 mm - per 8 further parking platforms: 600 x 400 x 210 mm
7	Flashing light

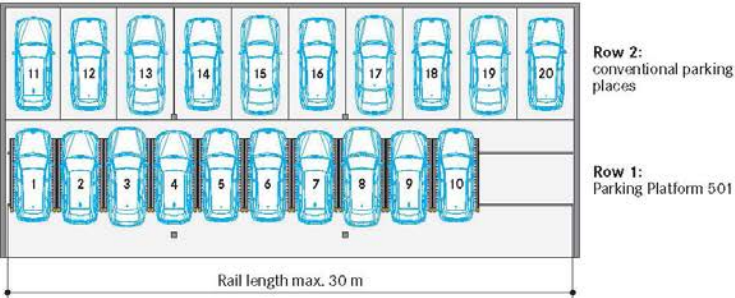
Without position numbers: total cabling of the system

Arrangement possibilities and parking place numbering (with 2 rows)

Different numbering of parking places is possible at extra cost (software changes are necessary).

2 rows one behind the other (up to 30 m rail length)

Planning example with 10 parking platforms with a platform width of 217 cm:



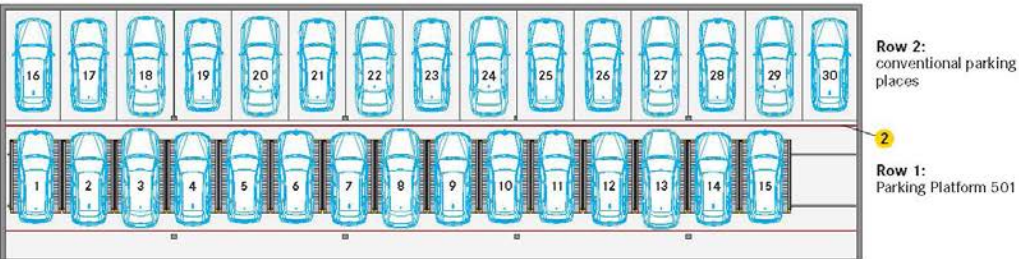
No light barriers are required for rails up to a length of 30 m.
The number of possible parking platforms depends on the platform width (see table).
The final number of parking platforms may vary depending on the requirements and the design of the building (e.g. width of the pillars, etc.).

Parking Platform A 1	Clear platform width B 1	Number of Parking Platforms
226	197	11
236	207	11
246	217	10
256	227	9
266	237	9
276	247	8
286	257	8

1 See graphic on page 1

2 rows one behind the other (over 30 m rail length)

WÖHR recommends: max. 15 parking platforms per row



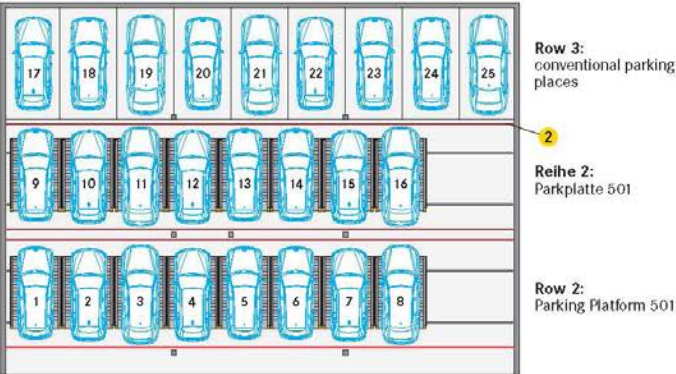
2 If the rail length is more than 30 m, light barriers compliant to DIN EN 14010 must be provided.

Arrangement possibilities and parking place numbering (with 3 and 4 rows)

Different numbering of parking places is possible at extra cost (software changes are necessary).

3 rows one behind the other

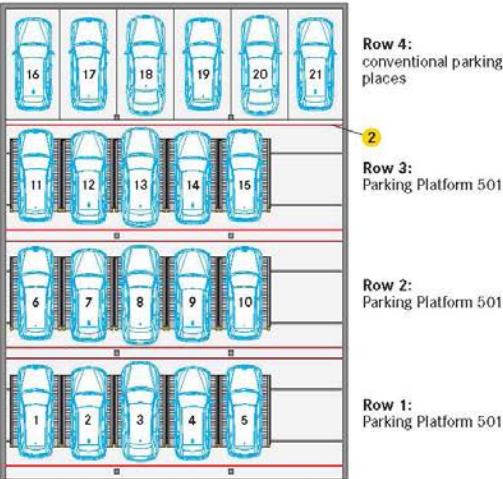
WÖHR recommends: max. 8 parking platforms per row



2 Light barriers must be provided for multi-row arrangements compliant to DIN EN 14010.

4 rows one behind the other

WÖHR recommends: max. 5 parking platforms per row



2 Light barriers must be provided for multi-row arrangements compliant to DIN EN 14010.

WÖHR PARKING PLATFORM 501 (Electric rail on the ceiling) | 12.2022 | C027-5285

Page 7 of 7

Notes and directions

Scope of application

- suitable for residential buildings, office buildings and business premises
- only for long-term users that have been instructed on how to use the system

Operation

- number of parking platforms through RFID
- parking platforms are moved in such a way that access to the selected parking space is freely accessible
- on the left side of the parking space there is about 60–80 cm space for getting in and out
- getting in and out area can also be provided on the right side

Temperature

- system operating range: +5° to +40°C
- humidity: 50% at +40°C
- in the event of changes to system conditions please consult with WÖHR accordingly

Lighting

- sufficient lighting of the driving aisle and of the parking places must be performed by the customer

Noise emission

- low running noise due to ball bearing rollers

Fire safety

- all fire safety requirements and all mandatory equipment (fire extinguisher and fire alarm systems, etc.) must be performed by the customer



Maintenance

- WÖHR and all the WÖHR partners abroad provide an installation and customer service network
- regular, annual maintenance is provided subject to the stipulation of a maintenance agreement

Prevention of corrosion damage

- all operations listed in the WÖHR Cleaning and Maintenance Instructions are to be performed regularly (independently of maintenance operations)
- zinc-plated parts, components and platforms are to be kept clean of dirt, road-salt and any other debris (due to corrosion hazards)
- always keep the garage well ventilated and deaerated

Conformity examination (TÜV)



- voluntary conformity assessment by the TÜV SÜD

The parking systems are compliant to:

- EC Machinery Directive 2006/42/EC
- DIN EN 140 10

Surface protection

- please consider the information on surface protection!

Tender specification

- please consider the specifications!

Parking Place-Profile

- please consider the product information Parking Place-Profile!

Electromobility

- please consider the product information E-charging!
- depending on the position of the charging point on the electric vehicle, collision points with protruding plugs and charging cables can occur

Product liability

WÖHR assumes no liability for any damage resulting from crushing hazards if the floor implementation notes and the flatness tolerances are exceeded. The distance between the lower edge of the parking platform and the floor must be max. 2 cm.

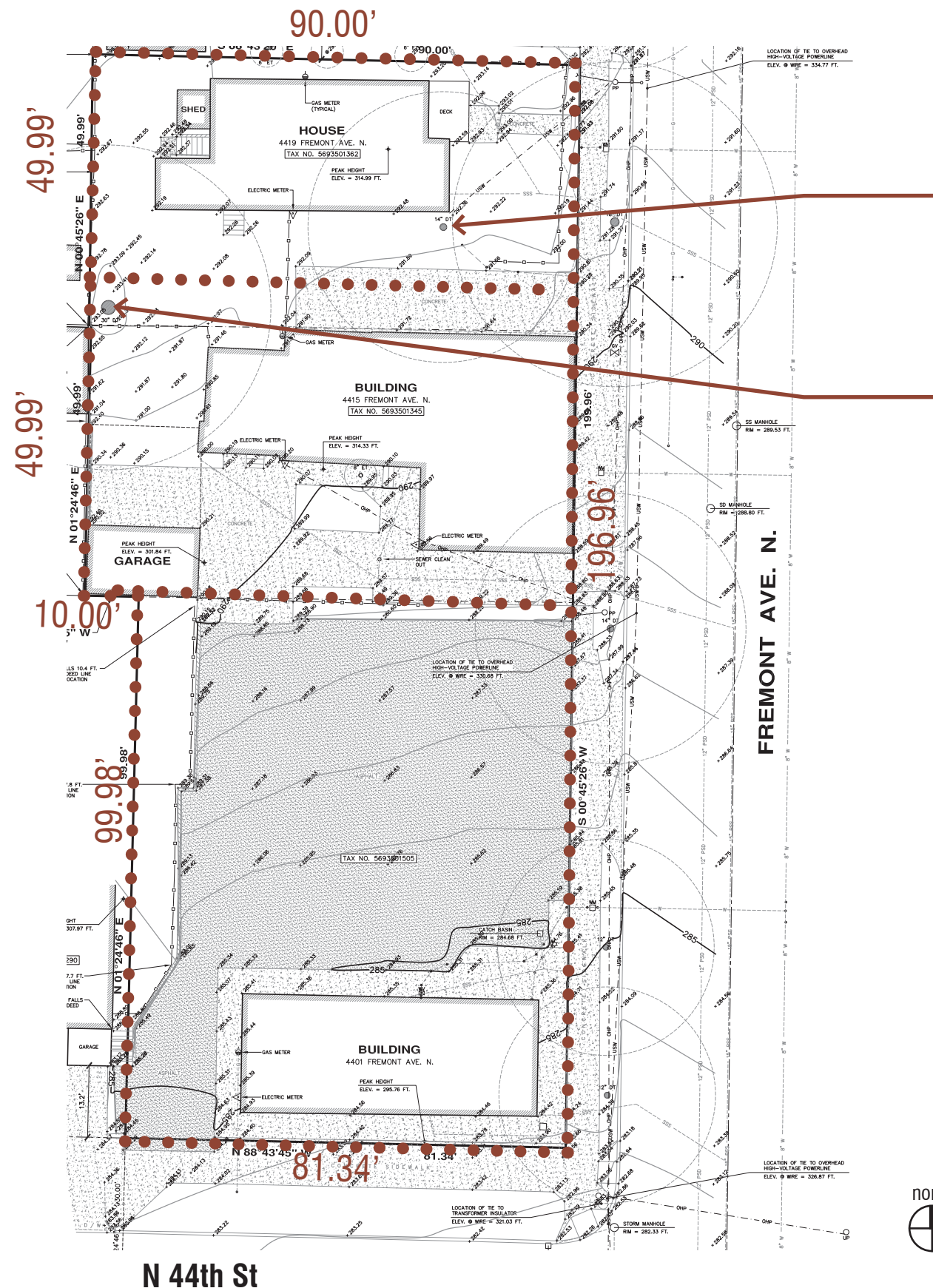
Construction formalities

- the documentation necessary for construction permit applications is provided by WÖHR on demand

Construction alterations and/or modifications

- the right to construction or model modifications and/or variations is hereby reserved
- the right to any subsequent part modification and/or variation and amendments in procedures and standards due to technical and engineering progresses or due to environmental regulation changes is also hereby reserved

© WÖHR Autoparksysteme GmbH | Vehicle drawings © creativ collection Verlag GmbH | www.cvision.de

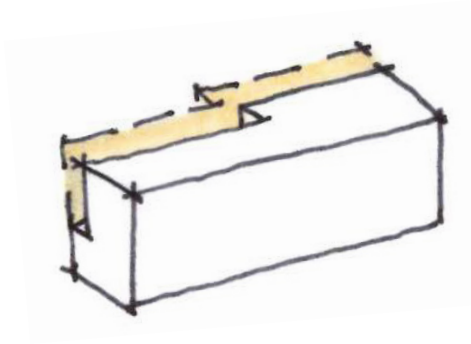


LEGAL DESCRIPTION

TAX PARCEL NO. 5693501505
4401, 4411, 4419 FREMONT AVE N
LOT 29, 30, 31, AND 32, BLOCK 9, MOTOR LINE ADDITION
TO THE CITY OF SEATTLE, ACCORDING TO THE PLAT
THEREOF RECORDED IN VOLUME 2 OF PLATS, PAGE 164,
RECORDS OF KING COUNTY, WA.
EXCEPT THAT PORTION CONDEMNED FOR FREMONT AVE. IN
KING COUNTY SUPERIOR COURT CAUSE NO. 52566, AS
PROVIDED BY ORDINANCE NO. 13677 OF THE CITY OF
SEATTLE.

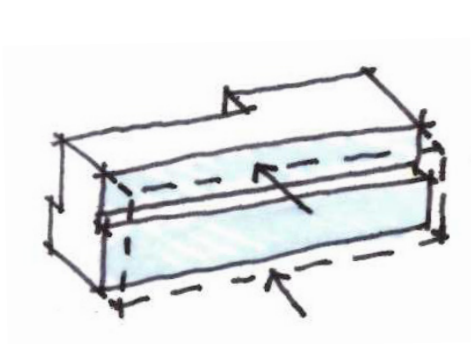
TAX PARCEL NO. 5693501362
BEGINNING AT THE INTERSECTION OF THE NORTH LINE OF LOT 25, MOTOR LINE ADDITION TO THE CITY OF SEATTLE, ACCORDING TO THE PLAT THEREOF RECORDED IN VOLUME 2 OF PLATS, PAGE 164, RECORDS OF KING COUNTY, WA., WITH THE WEST MARGINAL LINE OF FREMONT AVE. N., AS THE SAME WAS ESTABLISHED BY PROCEEDINGS IN KING COUNTY SUPERIOR COURT CAUSE NO. 52566 FOR WIDENING FREMONT AVE., AS PROVIDED FOR IN ORDINANCE NO. 13677 OF THE CITY OF SEATTLE; THENCE WEST ALONG THE NORTH LINE OF LOTS 25 AND 8, SAID BLOCK 9, A DISTANCE OF 90 FEET; THENCE SOUTHWARD PARALLEL WITH SAID WEST MARGINAL LINE TO THE SOUTH LINE OF LOT 7, SAID BLOCK 9; THENCE EAST ALONG THE SOUTH LINES OF LOTS 7 AND 26, SAID BLOCK 9, TO THE WEST MARGINAL LINE OF FREMONT AVE. N.; THENCE NORTHWARD ALONG SAID WEST MARGINAL LINE OF FREMONT AVE. N. TO THE POINT OF BEGINNING.

TAX PARCEL NO. 5693501345
THE EAST 10 FEET OF LOTS 5 AND 6, AND ALL OF LOTS
27 AND 28, BLOCK 9, MOTOR LINE ADDITION TO THE CITY
OF SEATTLE, ACCORDING TO THE PLAT THEREOF
RECORDED IN VOLUME 2 OF PLATS, PAGE 164, RECORDS
OF KING COUNTY, WA.
EXCEPT THAT PORTION CONDEMNED FOR FREMONT AVE. IN
KING COUNTY SUPERIOR COURT CAUSE NO. 52566, AS
PROVIDED BY ORDINANCE NO. 13677 OF THE CITY OF
SEATTLE.



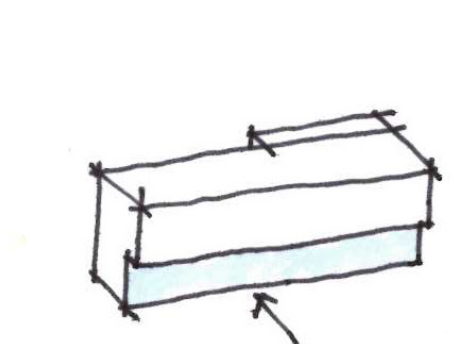
ZONING ENVELOPE

- The building envelope is delineated by the zoning code, with the 55' zone height limit, and setbacks for the abutting LR1 zone on the west facade.



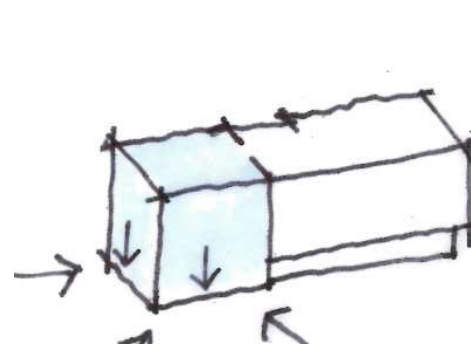
SITE-SPECIFIC SETBACKS

- The overall mass is further carved by site-specific requirements on Fremont Ave N. A 3' setback is provided as required by SDOT for the sidewalk and frontage zone. The upper floors are pushed back for the SCL powerline setback.



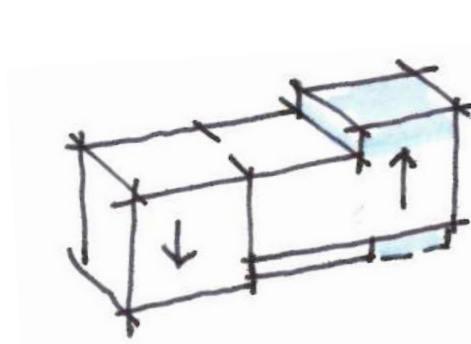
OPTION 1 (CODE-COMPLIANT)

- The mass is refined and responds to the importance of pedestrian experience by setting back on Fremont.
- The urban edge is held above grade creating weather protection for street scape.



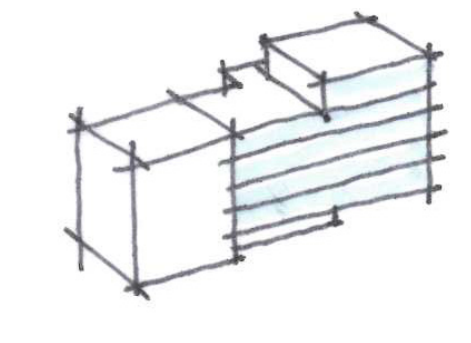
ENHANCED CORNER

- The corner of 44th & Fremont is a highly visible and established focal point of the upper Fremont neighborhood. The building corner is grounded and holds the edge of the sidewalk.



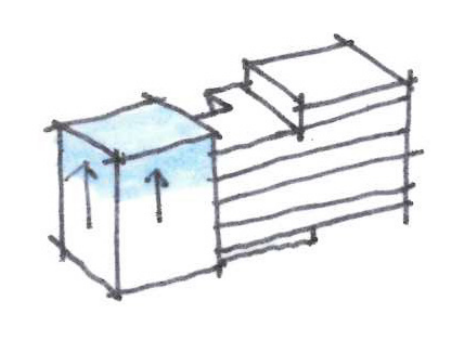
TOPOGRAPHY REFINEMENT

- The building terraces to take advantage of the topography and allow the ground floor spaces to meet the ground plane, allowing for each commercial space to be accessible.



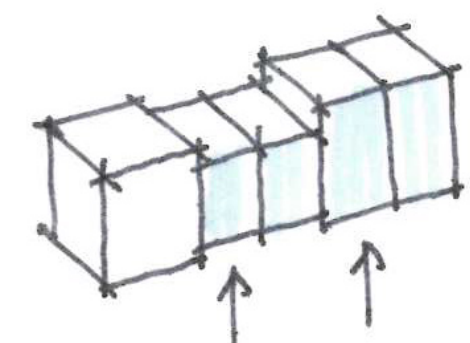
OPTION 2 (DEPARTURES)

- The secondary mass is broken down horizontally to provide relief from the long facade on Fremont.



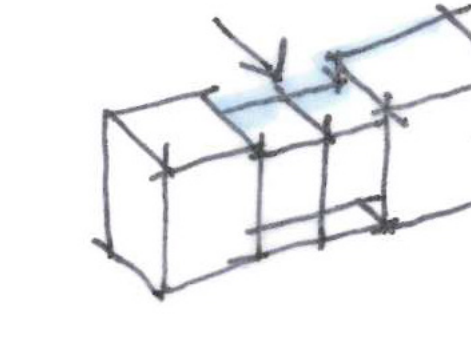
RE-AFFIRM CORNER

- The corner is raised to maintain it's significance, which was diminished by the north mass in option 2.



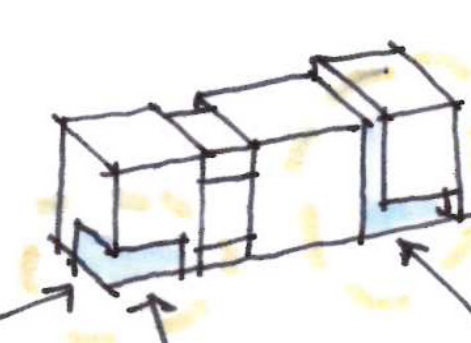
VERTICAL MODULATION

- Study of existing and historic buildings in the neighborhood provides additional direction and clarity of how the north mass should be modulated.
- Expression of breaking down secondary mass explored vertically, to reduce the long horizontal.



ZONE TRANSITION

- Recess the west facade and provide modulation to respond to the LR zone and existing single-family houses.



OPTION 3 (CODE COMPLIANT)

- Vertical modulation of massing on Fremont accentuates the corner and allows individual response and connection to the street.
- Corner element is carved away at the ground floor to create pedestrian open space that can be vibrant and used day & night by a cafe, and enhance the pedestrian experience.
- Establish pedestrian node to reinforce connection to N Allen Place.

HISTORIC CONTEXT AND FRAMEWORK

In the early 1900s, Upper Fremont was served by a private streetcar system and then later by the Seattle Municipal Railway, extending public transportation into Ballard, Phinney Ridge, Wallingford and Green Lake. The Phinney line (Route 21) was a double track, and ran on Fremont Ave N. The line turned west at N 43rd and continued north to Woodland Park, Phinney Ridge, and Greenwood. According to the City of Seattle’s Historic Resources Survey Report, Fremont’s “most intense period of residential development occurred after 1902 and was spurred in part by the addition of additional streetcar routes.”

The Phinney line was one of the last lines to stop running in 1941. The advent of the automobile reduced ridership and clogged the shared roadways, slowing down the public transit option. One of the biggest impacts to Fremont was the “expansion of Aurora Street, which had been an historic residential street in the neighborhood, to a multi-lane highway served to divide off the eastern portion of the neighborhood” and bisecting Woodland Park.

Today, the streetcars are gone, but several of the buildings from that early boom still exist and contribute to Upper Fremont's historic and cultural character.

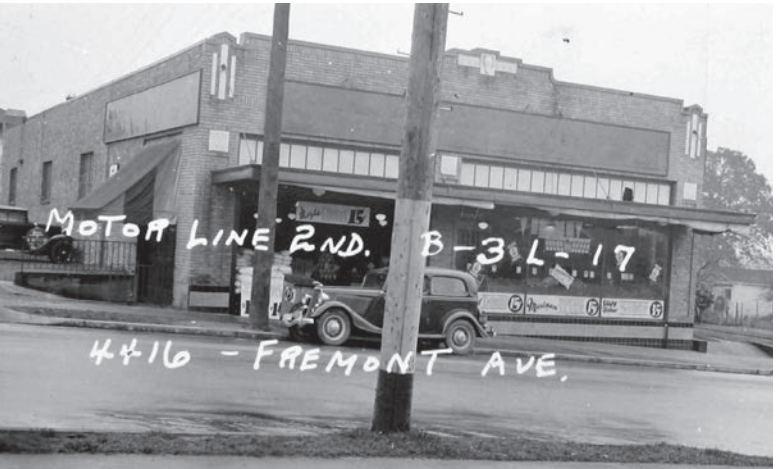


View east from N 43rd and Evanston Ave N, with tracks of Seattle Municipal Railway, dated 6 Jan 1921. SMR271, Seattle Photograph Collection, University of Washington Libraries, Special Collections



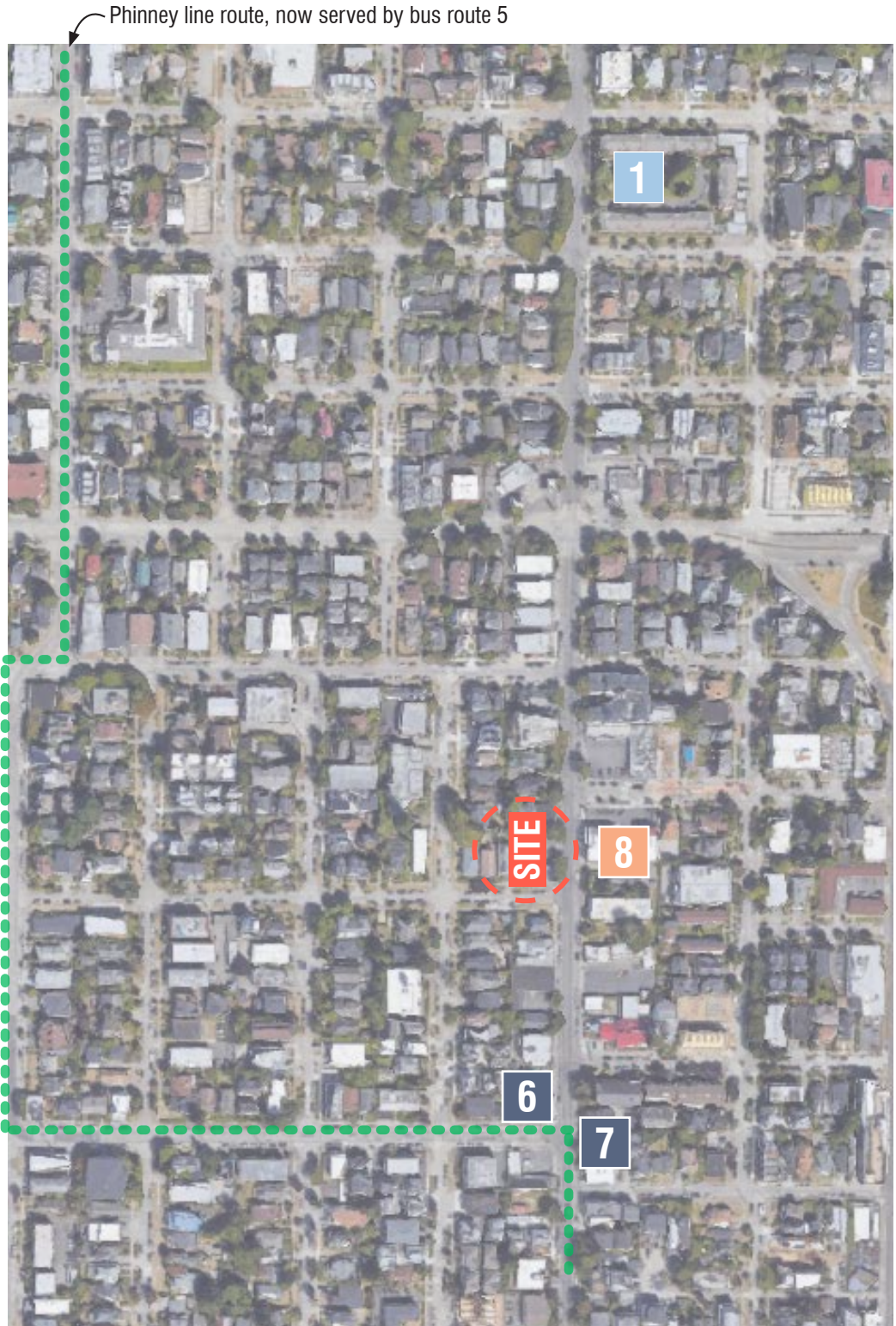
6 Herzing Building
4301 Fremont Ave N
year built: 1927

- Minor modulation with clear verticality
- Horizontal datum between 1st & 2nd story
- Repetitive windows with additional detail at corner and ends only



8 Marketime Foods
4416 Fremont Ave N
year built: 1931

- Large horizontal awning/datum
- Clear hierarchy of structure, void, and secondary features



note: site numbering corresponds to urban context key map



1 Hawthorne Square Condos
4800 Fremont Ave N
year built: 1920

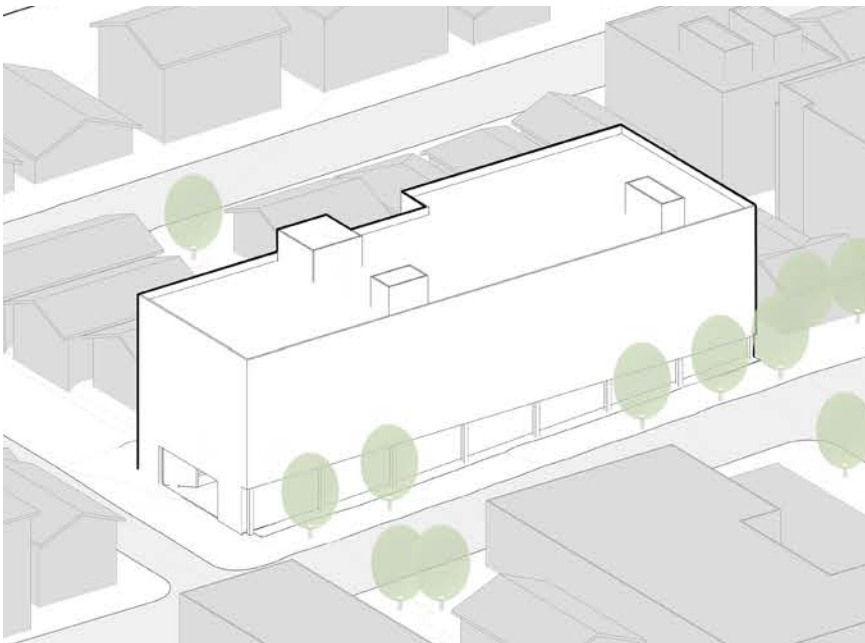
- Clear vertical planes
- Minor modulation over long facade
- Repetitive windows



7 Northfield Block
4250 Fremont Ave N
year built: 1909

- Minor modulation over large facade
- Individualization at storefronts
- Strong horizontal cornice at parapet
- Repetitive windows

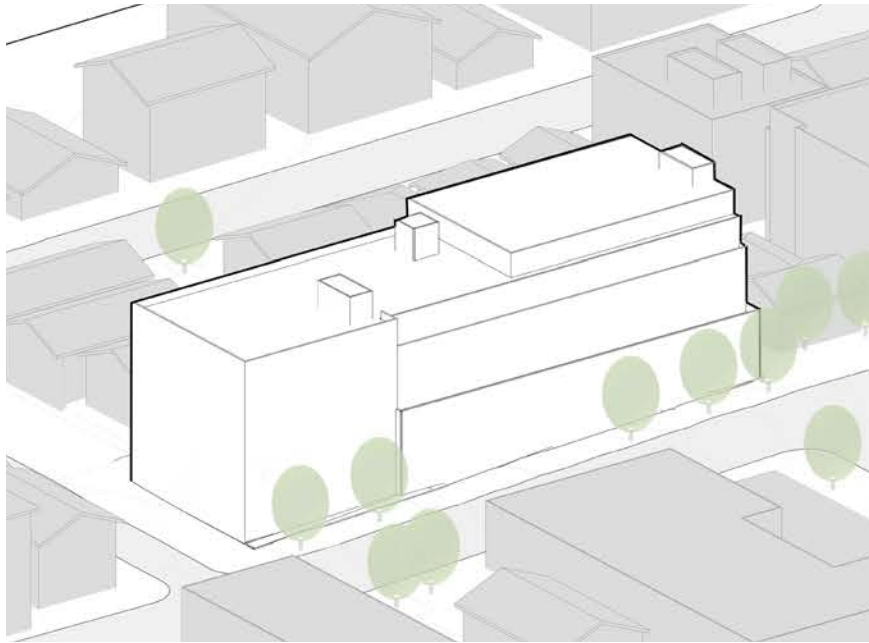
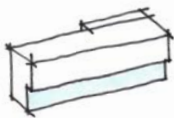




OPTION 1

COMMERCIAL USE: 2 SPACES, 2,658 SF TOTAL
RESIDENTIAL USE: 115 UNITS
PARKING: 44 STALLS

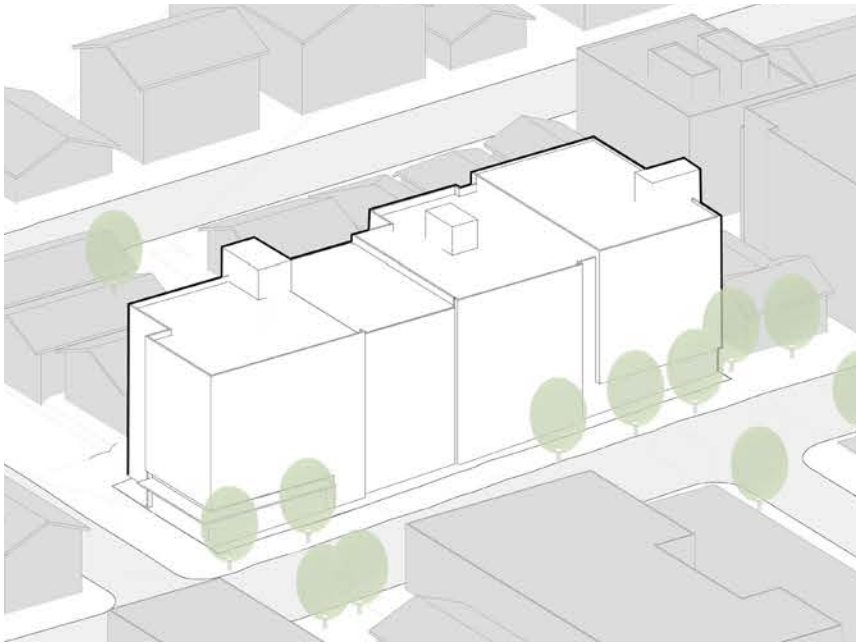
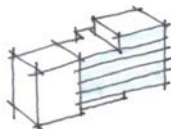
DEPARTURES: NONE (CODE COMPLIANT)



OPTION 2

COMMERCIAL USE: 5 SPACES, 5,068 SF TOTAL
RESIDENTIAL USE: 112 UNITS
PARKING: 46 STALLS

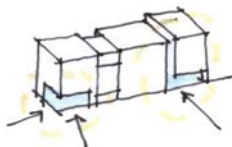
DEPARTURES: - DRIVEWAY SIGHT TRIANGLE (23.54.030.G.1)
- STREET LEVEL NON-RESIDENTIAL USES HEIGHT (23.47a.008.B.4)

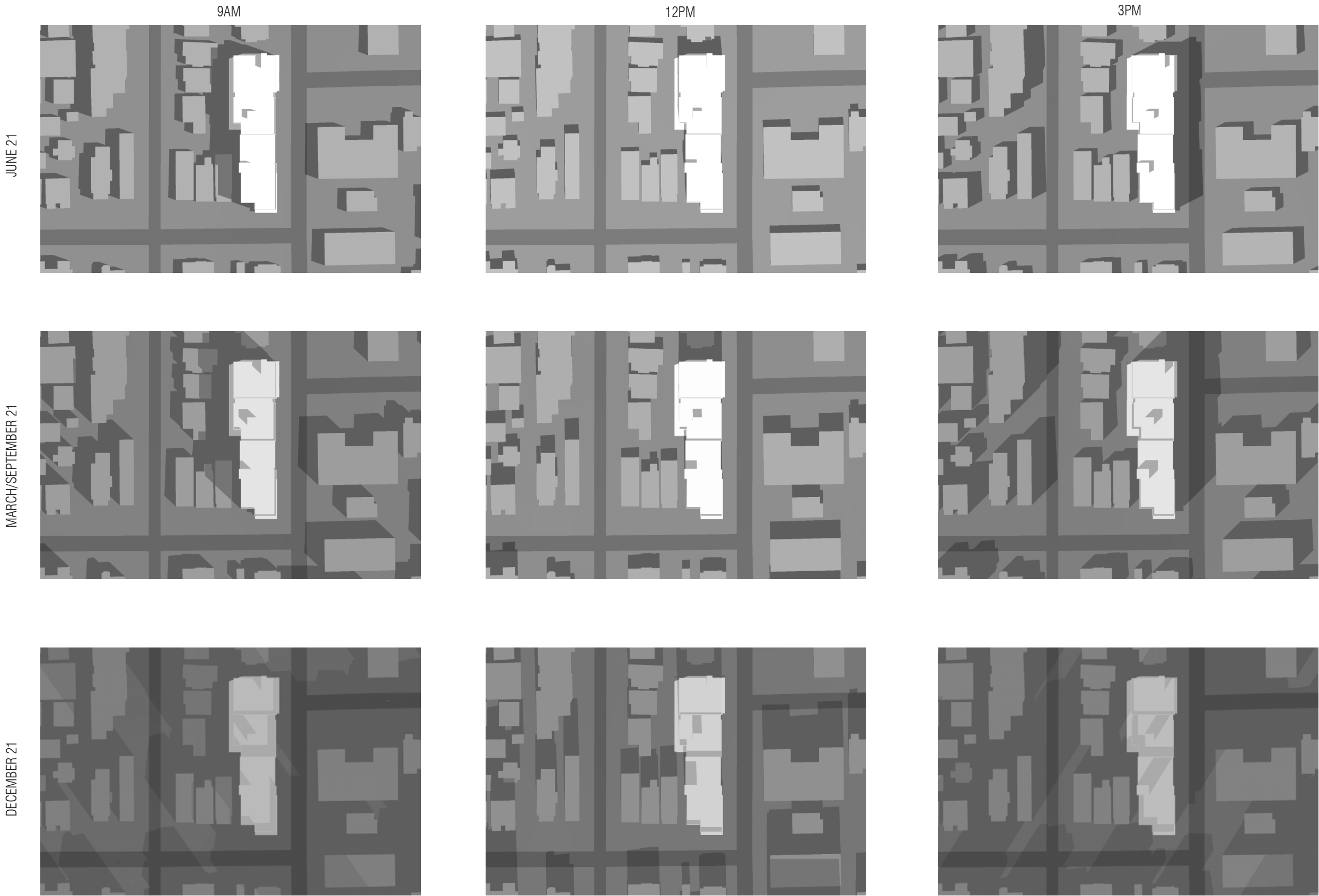


OPTION 3 (PREFERRED)

COMMERCIAL USE: 4 SPACES, 4,204 SF TOTAL
RESIDENTIAL USE: 119 UNITS
PARKING: 49 STALLS

DEPARTURES: NONE (CODE COMPLIANT)





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