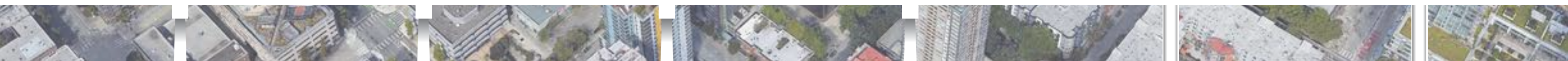


5TH AND JOHN



HKS



HEWITT

112-130 Fifth Avenue Development WA, LLC
SEATTLE | WA
SDCI MUP #3040199-LU | November 15, 2023



PROJECT INFORMATION OVERVIEW

Address: 112 5th Ave N
Seattle, WA 98101

Base Zone: SM-UP 160

Building Height: 9 Stories Above Grade / 124'-6"

Parcels: 1991200245, 1991200250, 1991200255 &
1991200260

Site Area: 27,120 SF

Parking: 86 Stalls

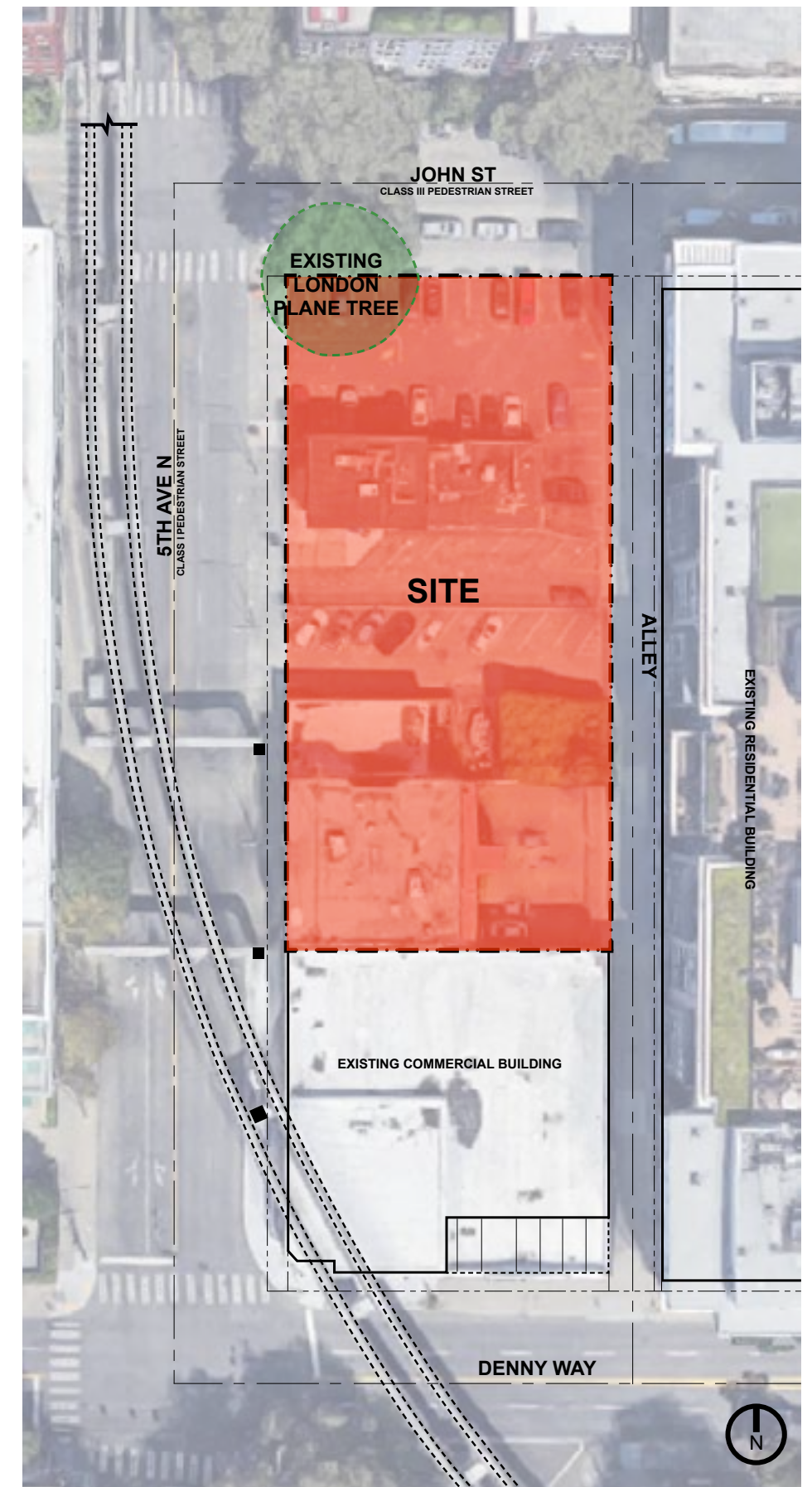
PROJECT DESCRIPTION

The 5th & John Life Science Building is a nine-story, 190,000 SF commercial office tower positioned primarily for lab and office use, with support, amenity and retail tenant spaces located at the ground level. Located in the Uptown neighborhood of Seattle, WA, 5th & John is immediately adjacent to the Seattle Center, the site of the 1962 World's Fair and the home of the iconic Space Needle, as well as the origin point and terminus of the city's monorail line, which runs parallel to the site.

Observing the monorail's elevated path as an implied boundary extending through the district, the concept of the "Datum of Delight" was developed to describe this virtual line between the space of the ground level experience and area of the contextual built environment above. Through this medium, the project responds to the rich culture of spectacle and arts in Uptown and enhances the pedestrian experience along 5th Avenue and John Street.

The Datum provided inspiration for the creation of an at-grade open space with opportunities for public art, focused around the celebration of an exceptional tree, and provides an indelible response to the context of the nearby Seattle Center. The public open space at the northwest corner has direct access to the public lobby and an extensive series of retail spaces arrayed along 5th Avenue and John Street. A rooftop amenity deck with views to both the Space Needle and Lake Union provides an opportunity for outdoor activities and community building for building users.

Further, two key contextual conditions influence the building massing: the presence of the exceptional tree at the northwest corner of the site, and the movement of the adjacent monorail line. With two key points of inflection, the facade moves outward at the northwest corner, gesturing to the tree and its surrounding open courtyard, and inflects inward across the west facade in response to the metro line. The implied motion of these massing gestures is further underscored by the design of the facade curtain wall system, which begins as an organized grid which disassembles as it progresses from north to west, then reorganizes as it transitions to the south, away from the monorail.





BOARD APPROVED DESIGN AT 08.02.2023 RECOMMENDATION

HKS



HEWITT

112-130 Fifth Avenue Development WA, LLC
DESIGN RECOMMENDATION MEETING | SDCI MUP #3040199-LU



BOARD APPROVED DESIGN AT 08.02.2023 RECOMMENDATION

HKS



HEWITT

112-130 Fifth Avenue Development WA, LLC
DESIGN RECOMMENDATION MEETING | SDCI MUP #3040199-LU

DEPARTURES 1-5 SUMMARY

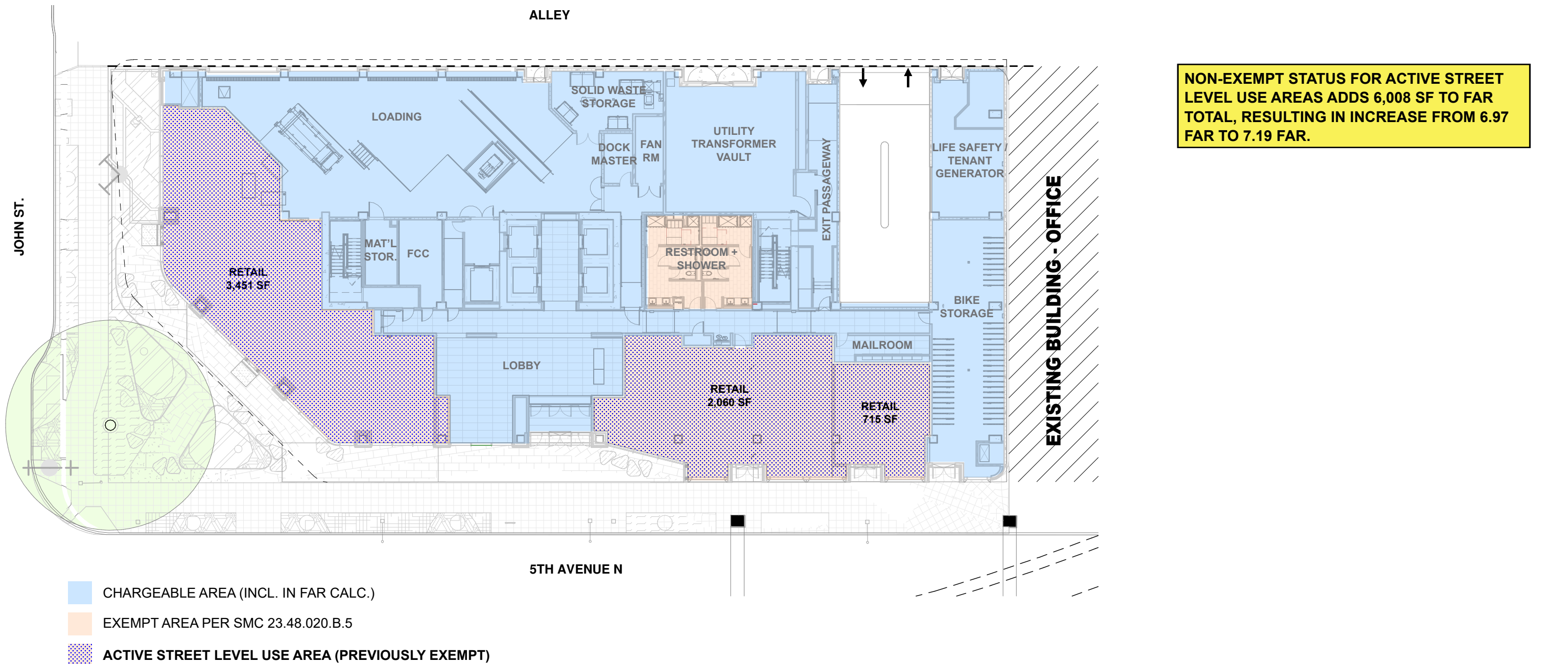
CODE LANGUAGE	REQUEST
<div>DEPARTURE 1: OPEN SPACE COVERAGE</div> <div>REQUIRED USABLE OPEN SPACE IN THE SM-UP 65, SM-UP 85, & SM-UP 160 ZONES (23.48.740.B) 23.48.740.B.1.d Up to a maximum of 20 percent of the required usable open space may be covered, if the open space abuts a street lot line and is open and accessible to pedestrians along the sidewalk;</div>	<div><ul style="list-style-type: none">The project is requesting for a departure to exceed open space allowable coverage from 20% code maximum to 40.8% as designed.</div> <div>APPROVED</div>
<div>DEPARTURE 2: UPPER LEVEL SETBACK</div> <div>UPPER-LEVEL SETBACK REQUIREMENTS IN SM-UP ZONES (23.48.735) 23.48.735.A In all SM-UP zones, any portion of a structure greater than 45 feet in height or 65 feet in height must be set back from a lot line that abuts a designated street shown on Map A for 23.48.735. A setback of an average of 10 feet from the lot line is required for any portion of a structure exceeding the maximum height that is permitted without a setback.</div>	<div><ul style="list-style-type: none">The project is requesting for a departure to comply with intent of upper level setback requirements with proposed “inflection point” facade design along 5th Ave N.</div> <div>APPROVED</div>
<div>DEPARTURE 3: FACADE MODULATION</div> <div>MAXIMUM FACADE WIDTH (45-125 FT) 23.48.745 - Table A For stories above the podium height of 45 FT up to 125 FT facades are limited to a maximum facade width of 150 feet. 23.48.745.D.2 The facade must be modulated at a minimum depth of 15 feet measured from the street lot line for a minimum length of 40 feet. If the required setback is provided, additional portions of the facade may be located within 15 feet of the street lot line.</div>	<div><ul style="list-style-type: none">The project is requesting for a departure to exceed maximum facade width (125FT) along 5th Ave N with a dynamic and integrated facade design that complies with the code intent of facade modulation.</div> <div>APPROVED</div>
<div>DEPARTURE 4: CLASS 1 PEDESTRIAN STREET STANDARDS (NEW)</div> <div>STREET-LEVEL USE PERCENTAGE 23.48.040.C.1 Where street-level uses are required, a minimum of 75 percent of the applicable street-level, street-facing facade shall be occupied by uses listed in subsection 23.48.005.D.1. The remaining street-facing facade may contain other permitted uses or pedestrian or vehicular entrances. 23.48.040.C.3 The space occupied by street-level uses shall have a minimum floor-to-floor height of 13 feet and extend at least 30 feet in depth at street level from the street-facing facade.</div>	<div><ul style="list-style-type: none">The project is requesting for a departure to reduce the code standard 75% acceptable use frontage requirement to 56.8%.</div> <div>APPROVED</div>
<div>DEPARTURE 5: STREET LEVEL DEVELOPMENT STANDARDS IN SM-UP ZONES (NEW)</div> <div>STREET-LEVEL FACADE REQUIREMENTS 23.48.740.A.1 The street-facing facades of structures abutting Class 1 Pedestrian Streets, as shown on Map A for 23.48.740, shall be built to the street lot line for a minimum of 70 Percent of the facade length, provided that the street frontage of any required outdoor amenity area, other required open space, or useable open space provided in accordance with subsections 23.48.740.B and 23.48.740.C is excluded from the total amount of frontage required to be built to the street lot line.</div>	<div><ul style="list-style-type: none">The project requesting for a departure to reduce the code standard 70% of facade length to be built to the street lot line requirement to 48.3%.</div> <div>APPROVED</div>

DEPARTURE 6: ADDITIONAL FAR

CODE LANGUAGE

REQUEST

DEVELOPMENT STANDARD DEPARTURES	REQUEST
<p>23.41.012.B.10</p> <p>Departures may be granted from any Land Use Code standard or requirement, except for the following:</p> <p>10. Floor area ratios (FAR), except that:</p> <p>b. Departures of up to an additional 0.5 FAR may be granted if the applicant demonstrates that:</p> <p>1) The departure is needed to protect a tree that is located on the lot that is either an exceptional tree, as defined in Section 25.11.020, or a tree greater than 2 feet in diameter measured 4.5 feet above the ground; and</p> <p>2) Avoiding development in the tree protection area will reduce the total development capacity of the site.</p>	<ul style="list-style-type: none">• The project is requesting for a departure to add 0.19 FAR to the project to maintain the existing design of the project in light of non-exempt street-level use area increasing the project FAR above the 7.0 maximum threshold.• The approval of street level Departure #4 by the Board resulted in an unforeseen increase from 7.00 FAR to 7.19 FAR due to street level-use areas now being considered non-exempt from FAR and MHA calculations per MUP Zoning review of SMC 23.48.040.C.• While SMC 23.41.012.B.10 allows for an additional 0.5 FAR to be granted, the request is only for an additional 0.19 FAR to preserve the current Board-approved design.• Part of the original design rational was to a) preserve a significant tree, b) create the successful open space around the tree, and c) support viable ground floor space on 5th Avenue.• Absent this departure, the Project is not viable unless the tree is removed and the full ground plan is redesigned, which is not desired by any party.



DEPARTURE 6: ADDITIONAL FAR

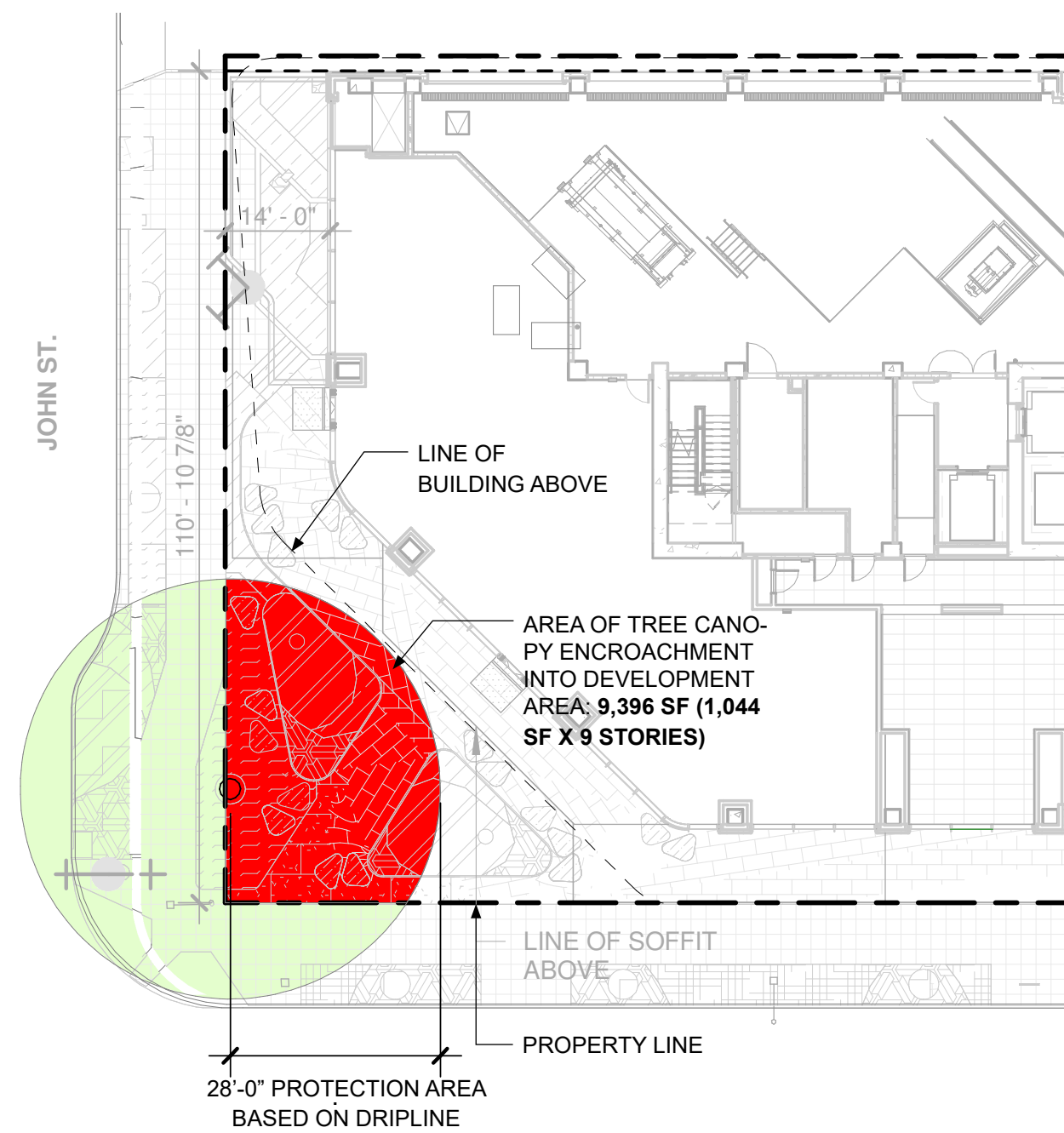
CODE LANGUAGE

REQUEST

DEVELOPMENT STANDARD DEPARTURES

23.41.012.B.10
Departures may be granted from any Land Use Code standard or requirement, except for the following:
10.Floor area ratios (FAR), except that:
b. Departures of up to an additional 0.5 FAR may be granted if the applicant demonstrates that:
1) The departure is needed to protect a tree that is located on the lot that is either an exceptional tree, as defined in Section 25.11.020, or a tree greater than 2 feet in diameter measured 4.5 feet above the ground; and
2) Avoiding development in the tree protection area will reduce the total development capacity of the site.

- The project is requesting for a departure to add 0.19 FAR to the project to maintain the existing design of the project in light of non-exempt street-level use area increasing the project FAR above the 7.0 maximum threshold.
- The approval of street level Departure #4 by the Board resulted in an unforeseen increase from 7.00 FAR to 7.19 FAR due to street level-use areas now being considered non-exempt from FAR and MHA calculations per MUP Zoning review of SMC 23.48.040.C.
- While SMC 23.41.012.B.10 allows for an additional 0.5 FAR to be granted, the request is only for an additional 0.19 FAR to preserve the current Board-approved design.
- Part of the original design rational was to a) preserve a significant tree, b) create the successful open space around the tree, and c) support viable ground floor space on 5th Avenue.
- Absent this departure, the Project is not viable unless the tree is removed and the full ground plan is redesigned, which is not desired by any party.



AREA OF TREE CANOPY ENCROACHMENT: 9,396 SF
MAXIMUM SITE FAR 7.0 = 189,840 SF
 $9,396 \text{ SF} / 189,840 \text{ SF} = 4.95\%$

TREE CANOPY ENCROACHMENT INTO THE DEVELOPMENT AREA RESULTS IN A 4.95% FAR REDUCTION.

THE REQUESTED ADDITIONAL FAR IS 0.19 FAR, or 2.71% OF MAXIMUM FAR



View of London Plane tree, looking north



View of London Plane tree, looking west



DEPARTURE 6: ADDITIONAL FAR: APPENDIX

CODE LANGUAGEREQUEST

DEVELOPMENT STANDARD DEPARTURES

- 23.41.012.B.10**

Departures may be granted from any Land Use Code standard or requirement, except for the following:

 - 10.Floor area ratios (FAR), except that:
 - b. Departures of up to an additional 0.5 FAR may be granted if the applicant demonstrates that:
 - 1) The departure is needed to protect a tree that is located on the lot that is either an exceptional tree, as defined in Section 25.11.020, or a tree greater than 2 feet in diameter measured 4.5 feet aboce the ground; and
 - 2) Avoiding development in the tree protection area will reduce the total development capacity of the site.
- **The project is requesting for a departure to add 0.19 FAR to the project to maintain the existing design of the proj-ect in light of non-exempt street-level use area increasing the project FAR above the 7.0 maximum threshold.**
 - The approval of street level Departure #4 by the Board resulted in an unforeseen increase from 7.00 FAR to 7.19 FAR due to street level-use areas now being considered non-exempt from FAR and MHA calculations per MUP Zoning review of SMC 23.48.040.C.
 - While SMC 23.41.012.B.10 allows for an additional 0.5 FAR to be granted, the request is only for an additional 0.19 FAR to preserve the current Board-approved design.
 - Part of the original design rational was to a) preserve a significant tree, b) create the successful open space around the tree, and c) support viable ground floor space on 5th Avenue.
 - Absent this departure, the Project is not viable unless the tree is removed and the full ground plan is redesigned, which is not desired by any party.

Arborist Report
Lincoln Property Company: 5th & John

August 23, 2023

codominant trunks starting at approximately 5 feet off the ground. It appears to have been in good health and good structural condition. This tree did not qualify as exceptional under the vested code.

The London plane tree is growing at the corner of 5th Avenue and John Street. It is on private property but its trunk is growing into the SDOT right-of-way. The tree is 33.5 inches diameter at standard height (DSH) and qualifies as exceptional per SMC 25.11 and the Seattle Director’s Rule 16-2008. This tree is proposed to be retained as part of the development project.

The London plane tree is in good health and fair structural condition. The tree has been impacted by heavy pruning on the south side of the canopy (Photo 3, 4) and pruning of 3 large structural roots on the north side of the trunk for new sidewalk panels (Photo 5) that were installed since my last site visit in February of 2019. In my professional experience, this species is tolerant of root and canopy pruning. However, further impacts to this tree should be limited by careful construction practices.

The London plane tree has been growing at the edge of a parking lot with very little infrastructure limiting its root system. It is likely that the root system of the tree has grown below the surface of the parking lot. The root flare of the tree has enveloped a portion of concrete from the previous sidewalk surface (Photo 5).

Construction Impacts

The Crimson King Norway maple is no longer present and would have been proposed for removal. It was growing in the center of the property behind the existing building at 112 5th Ave North, where the new building would be constructed. It was neither an exceptional tree nor was it 24 inches or greater in DSH and therefore it would not have received the protection or required replacement under the code and would likely have been approved for removal under 25.11.030.A.

The London plane tree is proposed to be retained as part of the development project. The tree protection area which is typically based on dripline should be applied on an average of 28 feet from the center of the trunk on all sides of the tree – even to the south where a large portion of the canopy is no longer present.

Greater than 1/3 of the outer dripline would be disturbed as a result of the location of the tree in an urban setting. The tree has been largely growing under paved surfaces with only a planter around the basal trunk of the tree since at least 1998 when King County images show that the site was a parking lot. In my professional opinion, the tree is likely to survive construction if it is conducted according to the plans and the following recommendations.

All construction should follow the attached General Tree Protection Specifications that are also included in the plans. Arborist monitoring is required for all surface disturbance activities including demolition and excavation within the tree protection area. This is necessary to avoid damage and also so that the arborist can assess any impacts to the tree that occurred prior to and during the project which could affect its structural stability.

Shoring

The building foundation would be approximately 28 feet to the southeast of the tree. The foundation wall would be installed with shoring using soldier piles with wood lagging resulting in a vertical cut at the back of the pile. Soil nails would be installed in the wall on a 13-to-20-degree angle toward the tree. Due

Tree Solutions Inc., Consulting Arborists

Page 2

ORIGINAL FAR CALCULATION		
LEVEL	EXEMPT AREA	CHARGEABLE AREA
LEVEL B3	17,797	0
LEVEL B2	23,936	0
LEVEL B1	16,142	0
LEVEL 01	697 6,226	13,113
LEVEL 02	0	21,949
LEVEL 03	0	24,408
LEVEL 04	0	22,214
LEVEL 05	0	22,332
LEVEL 06	0	22,449
LEVEL 07	0	22,580
LEVEL 08	0	22,702
LEVEL 09	0	22,829
LOW ROOF	0	1,319
SUBTOTAL	64,798	195,895
3.5% MECH. EXEMPT	6,856.33	-6,856.33
TOTAL	71,654.4	189,038.7
189,038.7 SF / 27,120 SF SITE AREA = 6.97 FAR		

CURRENT FAR CALCULATION
3,451 SF + 2,060 SF + 715 SF = 6,226 SF 6,226 SF - 3.5% = 6,008 SF ADD'L NON-EXEMPT AREA 189,038.7 SF + 6,008 SF = 195,046.7 SF TOTAL AREA
195,046.7 SF / 27,120 SF SITE AREA = 7.19 FAR

DEPARTURE 1: OPEN SPACE COVERAGE

CODE LANGUAGE

REQUIRED USABLE OPEN SPACE IN THE SM-UP 65, SM-UP 85, AND SM-UP 160 ZONES (23.48.740.B)

23.48.740.B.1.d
Up to a maximum of 20 percent of the required usable open space may be covered, if the open space abuts a street lot line and is open and accessible to pedestrians along the sidewalk;

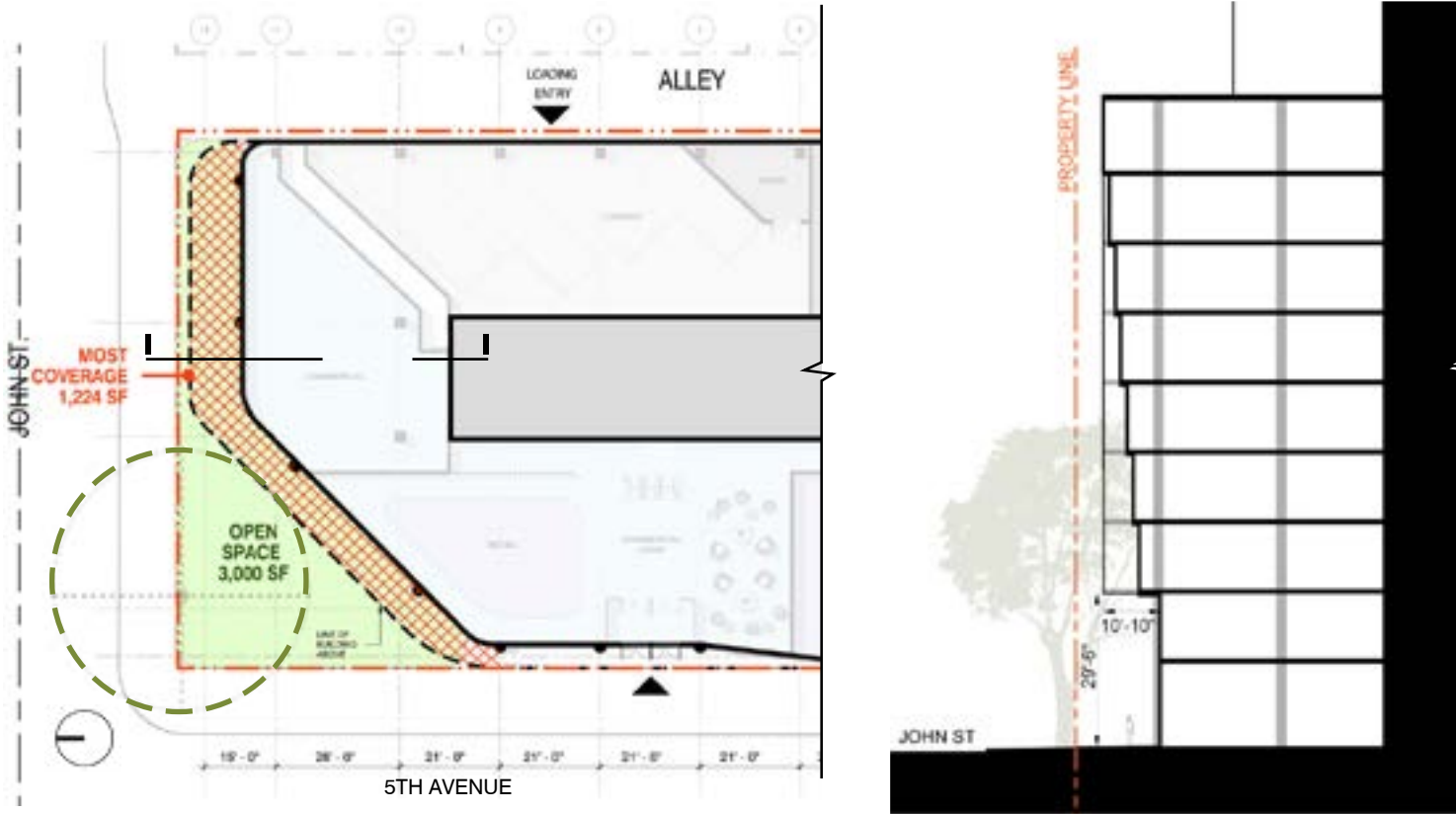
Open Space: 3,000 SF
Allowable Coverage: 20%
3000 x .2 = 600 SF

Preferred Concept
Designed Coverage: 1,224 SF
1,224 / 3000 = 40.8%

REQUEST

- To avoid a long blank facade along the south side of the building and provide well-lit working areas, the upper levels above the podium have been pulled north 10' from the south property line. This distance permits 40% clear glazing on these facades and allows the skin of the building to remain continuous around all four facades and provides a better day-one condition for the neighborhood. This aligns with requirements of DC4.1.b and c, as well as DC2.5.e. The result is a shift of the upper levels over the open space.
- In CS2.3 the design guidelines request for open spaces to address the corner and promote activity, and that they are desirable locations for art and special features. Retaining the exceptional tree on the site made locating the required open space on the north side of the property an optimal choice for both the project and in response to the design guidelines.
- Additionally, PL3.1 and DC2.1 request that the design of entries be pedestrian-friendly and scaled to create entries that are discernible to the pedestrian. Although 29'-6" high, the overhang lowers the scale of the pedestrian experience and provides a clearly expressed pedestrian zone which begins at the main lobby entry and continues around and through the open space. The continuity of this datum creates legibility of the project from afar and up close that the cantilever supports. At its deepest point, the top floor of the building overhangs the open space 10'-10" and provides twice as much coverage as the code allows. To offset the effect of this, level three up to level nine incrementally peel outward, increasing the amount of perceived sky from the ground level and gives additional breathing room to the exceptional tree
- The project is requesting for a departure to exceed open space allowable coverage from 20% code maximum to 40.8% as designed.

PREFERRED CONCEPT - DEPARTURE REQUIRED

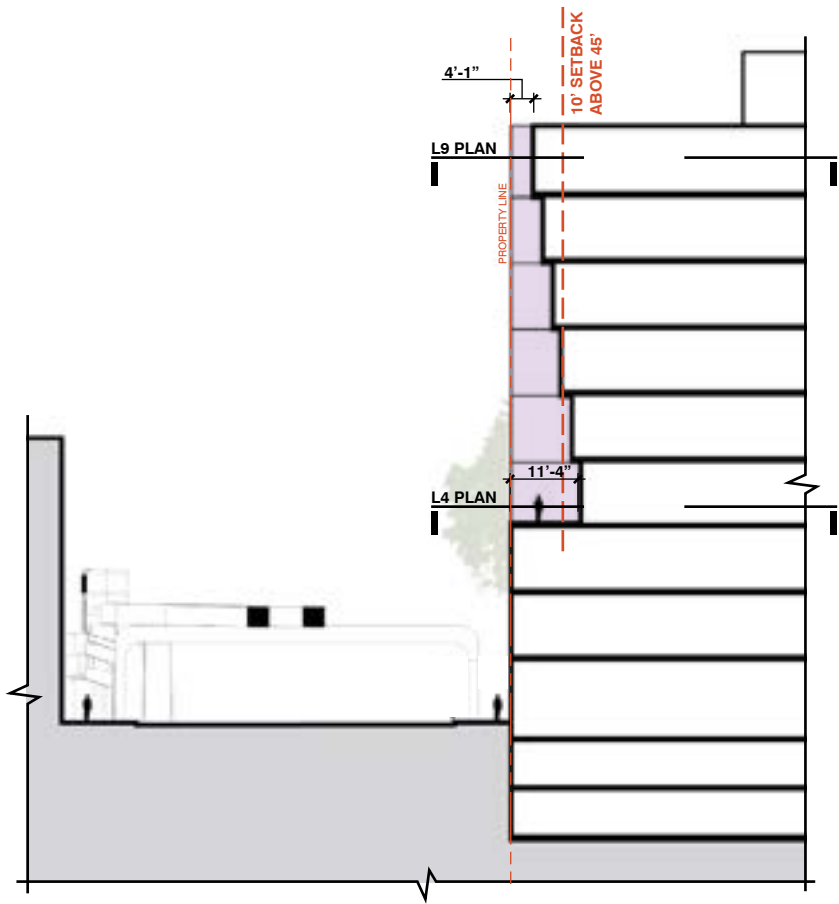


DRB PACKAGE REFERENCE | PAGE #74

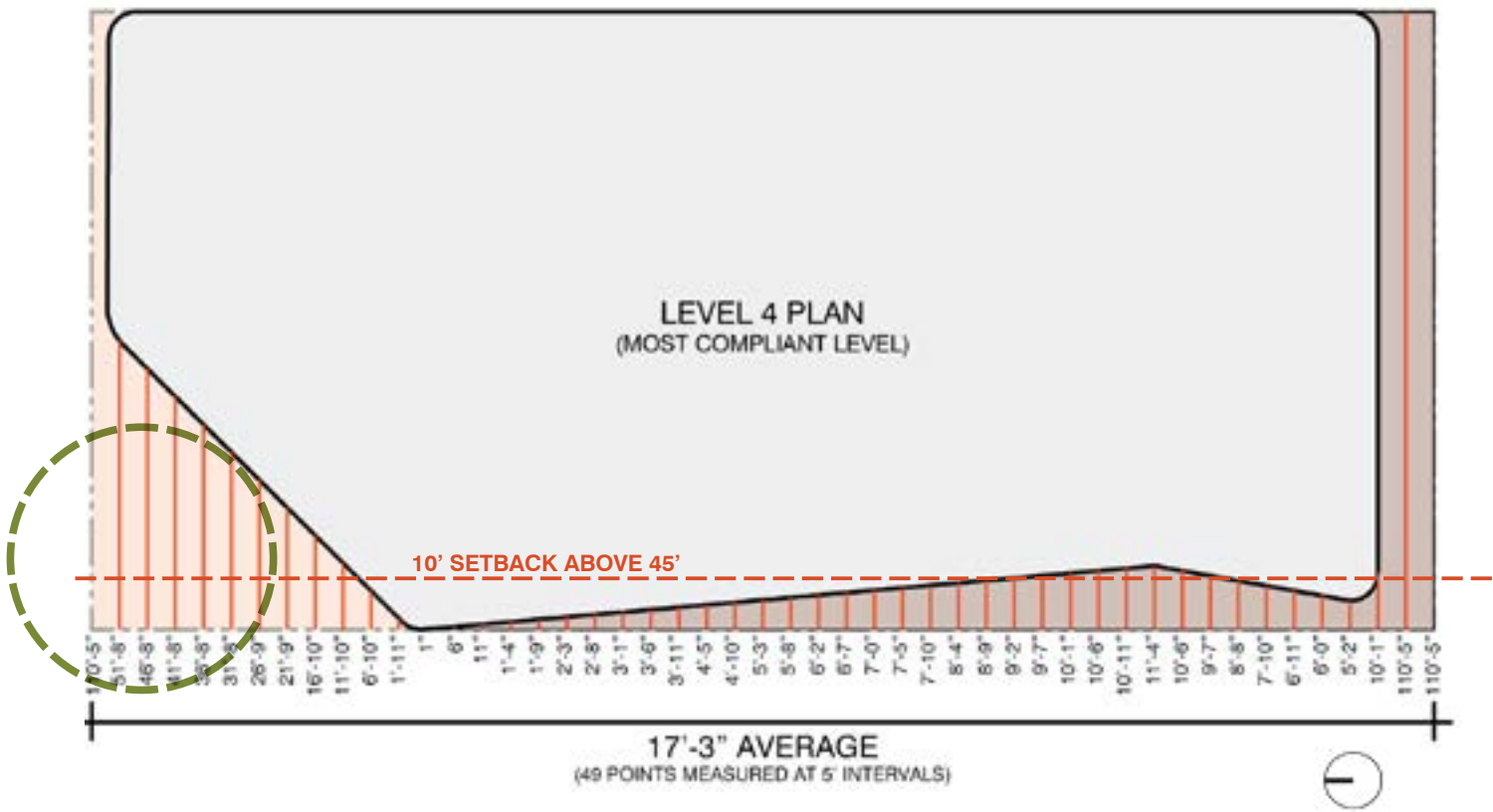
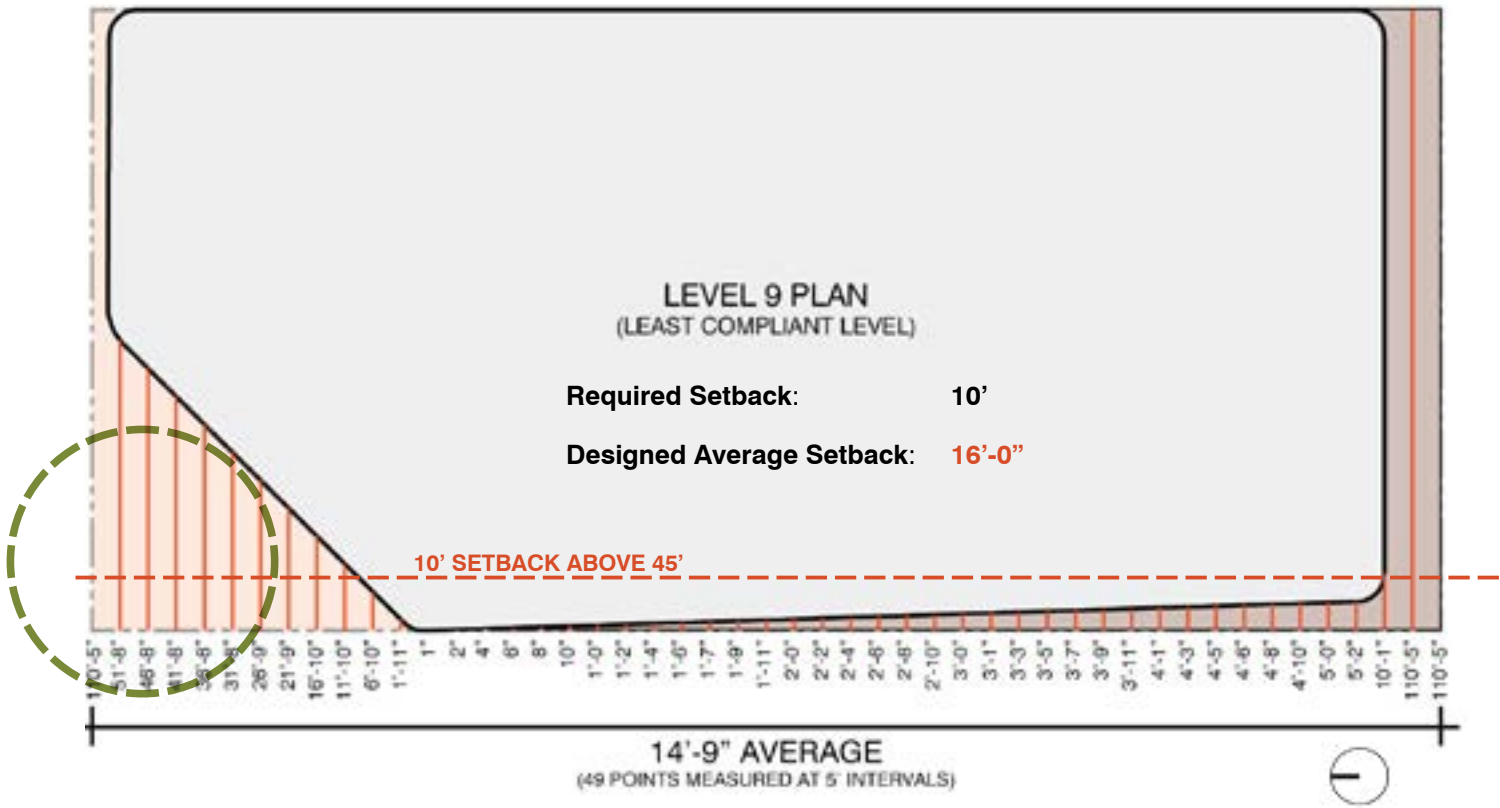
DEPARTURE 2: UPPER LEVEL SETBACK

CODE LANGUAGE	REQUEST
<p>UPPER-LEVEL SETBACK REQUIREMENTS IN SM-UP ZONES (23.48.735)</p> <p>23.48.735.A In all SM-UP zones, any portion of a structure greater than 45 feet in height or 65 feet in height must be set back from a lot line that abuts a designated street shown on Map A for 23.48.735. A setback of an average of 10 feet from the lot line is required for any portion of a structure exceeding the maximum height that is permitted without a setback.</p>	<ul style="list-style-type: none">DC2.5.C asks to avoid big unmodulated boxy forms, and consider curved, angled, shifting, and/or carved yet coherent forms. It also asks that modulation should be up-sized to match longer, taller view distances. The massing of the project has setbacks above the podium ranging from 0' up to 110'. Acknowledging the code language allowing an average of 10-feet from the lot line, the project uses continually modulated and carved forms to create a setback average of 16-feet above 45-feet in the Preferred Concept. These forms create a unified, coherent, massing which responds to site constraints, increase solar access, and offers visual interest from the ground level and while riding the monorail. The modulated facades are also located along the streets with the longest view distances, ensuring that the movement in the building massing supports the prominence of its location.The project is requesting for a departure to comply with intent of upper level setback requirements with proposed "inflection point" facade design along 5th Ave N.

PREFERRED CONCEPT - DEPARTURE REQUIRED



PREFERRED CONCEPT - DEPARTURE REQUIRED



DEPARTURE 3: FACADE MODULATION

CODE LANGUAGE

MAXIMUM FACADE WIDTH (45 -125 FT)

23.48.745 - Table A
For stories above the podium height of 45 FT up to 125 FT facades are limited to a **maximum facade width of 150 feet**.

23.48.745.D.2
The facade must be modulated at a minimum depth of 15 feet measured from the street lot line for a minimum length of 40 feet. If the required setback is provided, additional portions of the facade may be located within 15 feet of the street lot line.

Code Req'd Modulation (15' x 40' Notch)

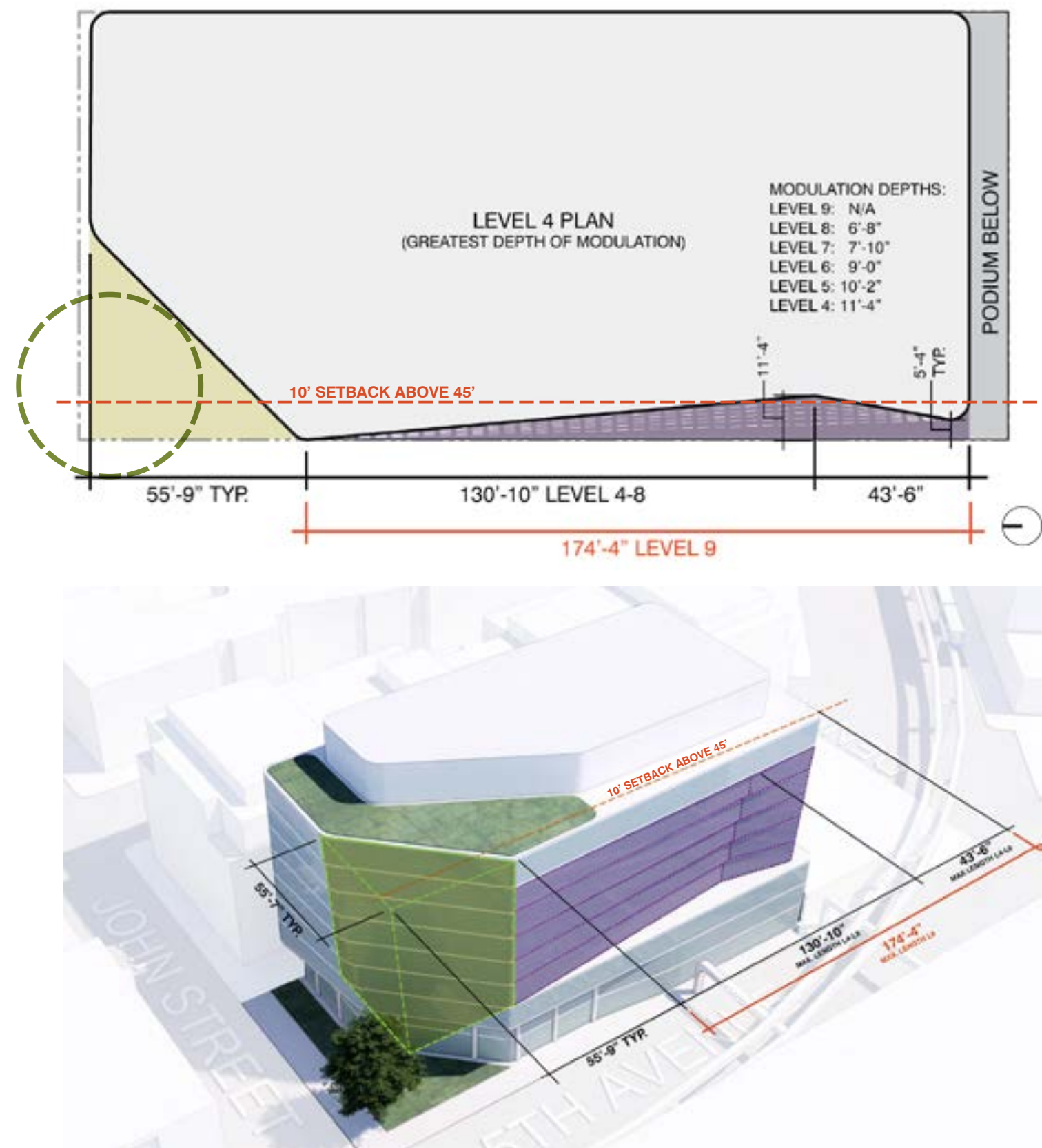
5th Avenue Designed Modulation (Continuous)

Modulation for Tree (Accommodate Drip Line)

REQUEST

- Related to the other two departures, saving the exceptional tree resulted in a loss of development capacity. In order to recoup the FAR, while providing glazing on the south facade, the project is asking for a departure of the code standard 15'x40' step in the facade in lieu of a continuously carved approach as supported by design guideline DC2.5.c, and a reduction in its depth. Across the length of 5th Avenue, the volume of modulation is increased with the Preferred Concept and Concept 2.
- Additionally, DC2.5.d requests that the project mediates extra height through legible, multistory elements like floor groupings, gaskets, projections, and layering to avoid a single repeated extrusion from base to top. The concept of the "Datum of Delight" was formed around the contextual relationship of the site to the monorail alignment and the effect it has in creating clearly defined zones in the vertical axis. The ground level facade recesses to maintain the pedestrian scale, while the upper level modulation provides continuously folding forms which are unified, but not uniform in depth. This folding creates a continually modulated massing on all street frontages with only the top floor exceeding 150' in length along 5th Avenue. This top level facade was left intentionally straight to highlight top of the building, and augment the sense of movement happening in the levels below it.
- **The project is requesting for a departure to exceed maximum facade width (125FT) along 5th Ave N with a dynamic and integrated facade design that complies with the code intent of facade modulation.**

PREFERRED CONCEPT - POTENTIAL DEPARTURE REQUIRED



DEPARTURE 4: CLASS 1 PEDESTRIAN STREET STANDARDS (NEW)

CODE LANGUAGE

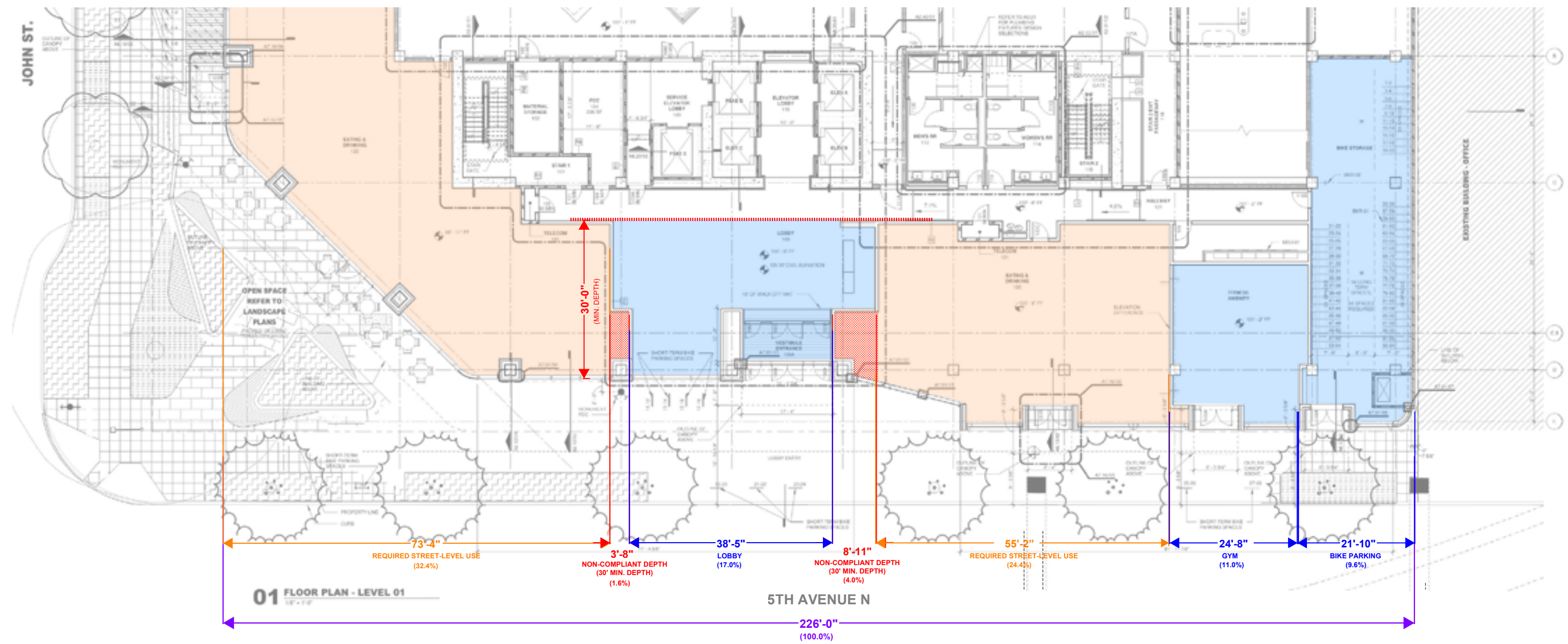
REQUEST

STREET-LEVEL USE PERCENTAGE

23.48.040.C.1
Where street-level uses are required, a **minimum of 75 percent** of the applicable street-level, street-facing facade shall be occupied by uses listed in subsection 23.48.005.D.1. The remaining street-facing facade may contain other permitted uses or pedestrian or vehicular entrances.

23.48.040.C.3
The space occupied by street-level uses shall have a minimum floor-to-floor height of 13 feet and **extend at least 30 feet in depth at street level from the street-facing facade.**

- The project is requesting for a departure to reduce the code standard 75% acceptable use frontage requirement to 56.8%.** This departure is the result of a desire to create continuous retail access that wraps the corner from 5th Avenue around to John Street, provide inviting lobby that is physically and visually connected to the sidewalk along 5th, and to provide a gracious corner plaza which will support pedestrian connectivity through the neighborhood. PL1-2a requests additional opportunities to add to public life and PL1-4 encourages outdoor dining in Uptown. PL3-1a asks for entries to be designed in a pedestrian-friendly way and to make them clearly discernible to users.
- This departure will also allow the project to place long-term bike parking for the building mid-block (south end of the property), further activating 5th Avenue throughout the day. Placing the long-term parking in this location instead of the alley or John Street is supported by PL4-2a’s goals for safety and ease of access and PL1-2’s opportunities to add to public life.

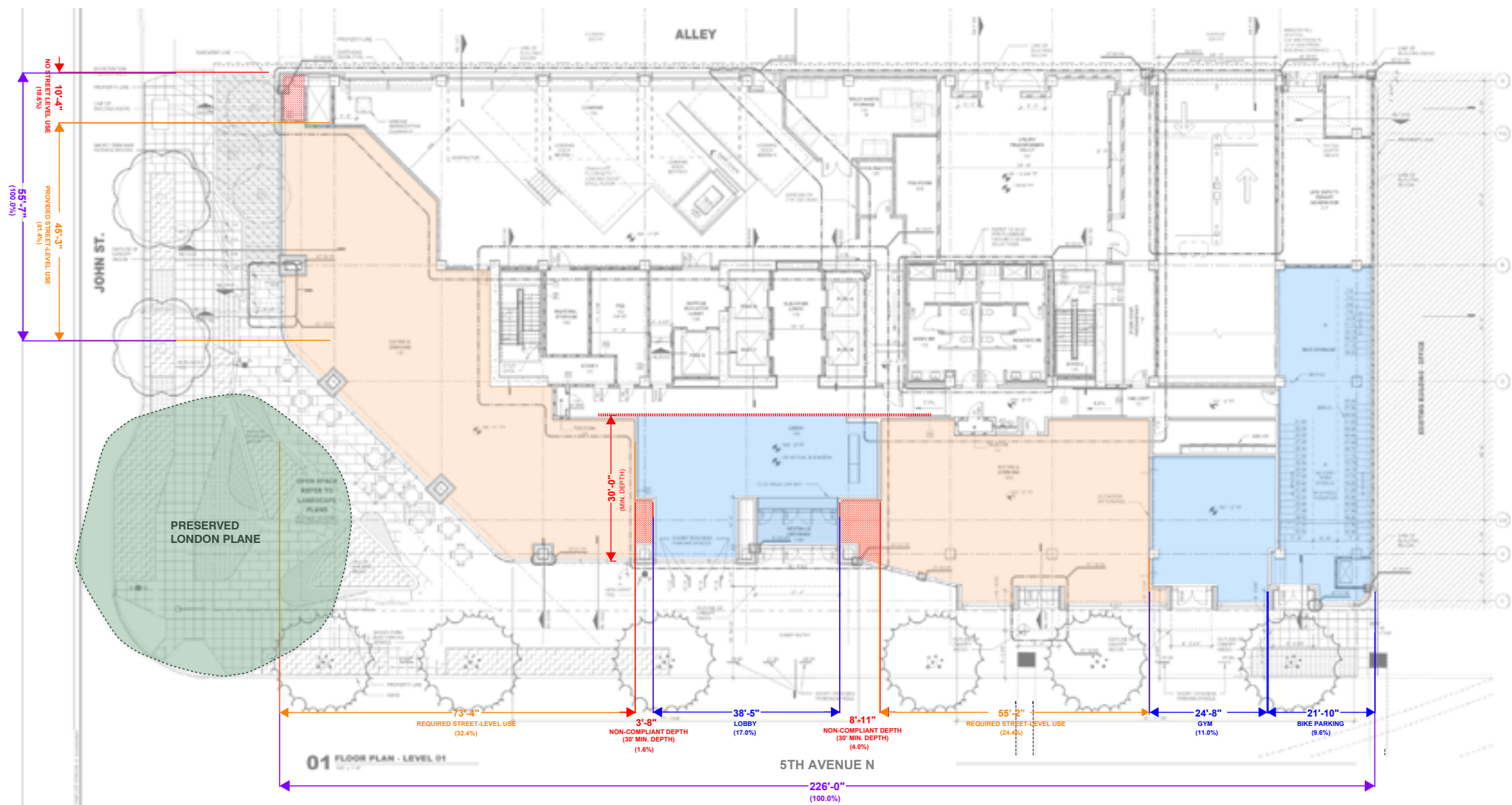


DEPARTURE 4: CLASS 1 PEDESTRIAN STREET STANDARDS (NEW)

FOR CONSIDERATION TO SUPPORT DEPARTURE #4

POINTS FOR CONSIDERATION FOR GRANTING OF DEPARTURE #4

- The ground level experience and frontage program allocation has been carefully considered by the design team to prioritize **honoring of the existing London Plane** and its integration into **a gracious corner plaza space**.
- The project team had taken the boards EDG recommendation to heart and wrapped retail around corner along John Street frontage to optimize pedestrian experience. **John Street is a class 3 pedestrian street with no street level use requirement. The project is providing an additional 45'-3" of (non-required) street level use along John Street.**
- **The design team has opted to place ground level building amenity space along 5th Avenue in exchange for a larger retail space at the corner of John Street.** This placement further activates the public plaza and celebrates the larger London Plane tree. The street level amenity spaces on 5th Avenue will be designed to allow for retail space in the future, offering doors directly to sidewalk and maximum transparency in the storefront system.



DRB PACKAGE REFERENCE | PAGE #79

DEPARTURE 5: STREET LEVEL DEVELOPMENT STANDARDS IN SM-UP ZONES (NEW)

CODE LANGUAGE	REQUEST
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STREET-LEVEL FACADE REQUIREMENTS

23.48.740.A.1
The street-facing facades of structures abutting Class 1 Pedestrian Streets, as shown on Map A for 23.48.740, shall be built to the street lot line for a **minimum of 70 Percent of the facade length**, provided that the street frontage of any required outdoor amenity area, other required open space, or useable open space provided in accordance with subsections 23.48.740.B and 23.48.740.C is excluded from the total amount of frontage required to be built to the street lot line.

- The project requesting for a departure to reduce the code standard 70% of facade length to be built to the street lot line requirement to 48.3%. This departure is the result of a desire to maintain better pedestrian flow and connectivity between 5th Avenue, the building entry and the transition of movement into corner plaza. The setback along 5th Avenue also provide additional pedestrian weather protection and assist in wayfinding by announcing the area of main entry for the building.
- PL1-1 supports the enhancement of open spaces by promoting physical and visual connection to the street. We believe this departure would significantly enhance the connection of the open space to 5th Ave N.
- PL1-3c supports outdoor uses, activities and widening of sidewalks through additional building setback at street level. Our request for departure is a direct response to this design guideline with the intention of supporting and promoting enhanced pedestrian activity at street level.
- PL3-1a supports pedestrian-friendly building entries with appropriate scale and architectural detailing. We believe this setback is a great gesture that announce and celebrate the building entry through building massing, form and sightline connectivity for the public.

