

# WHITE GARAGE

1915 THIRD AVENUE, SEATTLE, WA 98101  
PROJECT #3037328-LU  
ADMINISTRATIVE DESIGN REVIEW



*View from Securities Building*



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## Project Information

Residential Units: 112  
 Commercial Area: 3,680 SF  
 Live-Work Units: None Provided  
 Residential Amenity: 4,347 SF  
 Vehicle Parking: None Provided  
 Bike Parking: 153, Basement Level

## Zoning Information

Zoning: DMC 240/290-440  
 Overlay Designation: Belltown Urban Center  
 Pedestrian Zone: None  
 MHA Designation: Yes  
 Historic Landmark: Yes  
 Proposed Building Height: 173' - 3"

## Site Information

Address: 1915 Third Ave, Seattle, WA 98101  
 Parcel Number: 1947720-1055  
 Site Area: 6,480 SF  
 Existing Building Area: 44,729 SF  
 Overall Building Area: 93,972 SF  
 Allowable FAR: 45,360 SF



↑ Aerial photograph with streets and project site labeled





## Revisions

### 1 Architectural Expression

• **SDCI Comment:** EDG guidance 2a-2c discusses the need for a legible architectural expression that establishes a relationship with the Landmark structure below using a compatible structural expression and legibility through detailing and elimination of coplanar materials changes.

The rendering on the first page of the plan set portrays significant depth between windows and framing columns on this facade, which would help to meet this guidance. However, this drawing indicates a minimal amount of depth between the exterior columns and adjacent windows. Please address this issue in the building design. Provide a dimension of this window depth within the Recommendation packet and discussion of how the design addresses EDG guidance.

• **Response:** The updated expression of the new structure maintains the window bay sizing and vertical facade columns established by the landmark structure.

### 2 Roof Top / Penthouse Screening

• **SDCI Comment:** The rooftop screening design appears to show the intent to integrate into the rooftop massing design. However, the attempt to align the rooftop screening panels into the window alignment below does not appear to sufficiently integrate the screening into the overall design. The Recommendation packet should analyze this issue and how the proposed design meets applicable design guidelines. Ideally, the packet will present additional study for roof screening methods (Design Guidelines: A-2 Enhance the Skyline, Belltown A-2. Enhance the Skyline).

• **Response:** The revised structure takes a cleaner approach to integrate into the overall massing by maintaining the rigorous window bay sizing and vertical facade columns; key characteristics of the landmark structure.

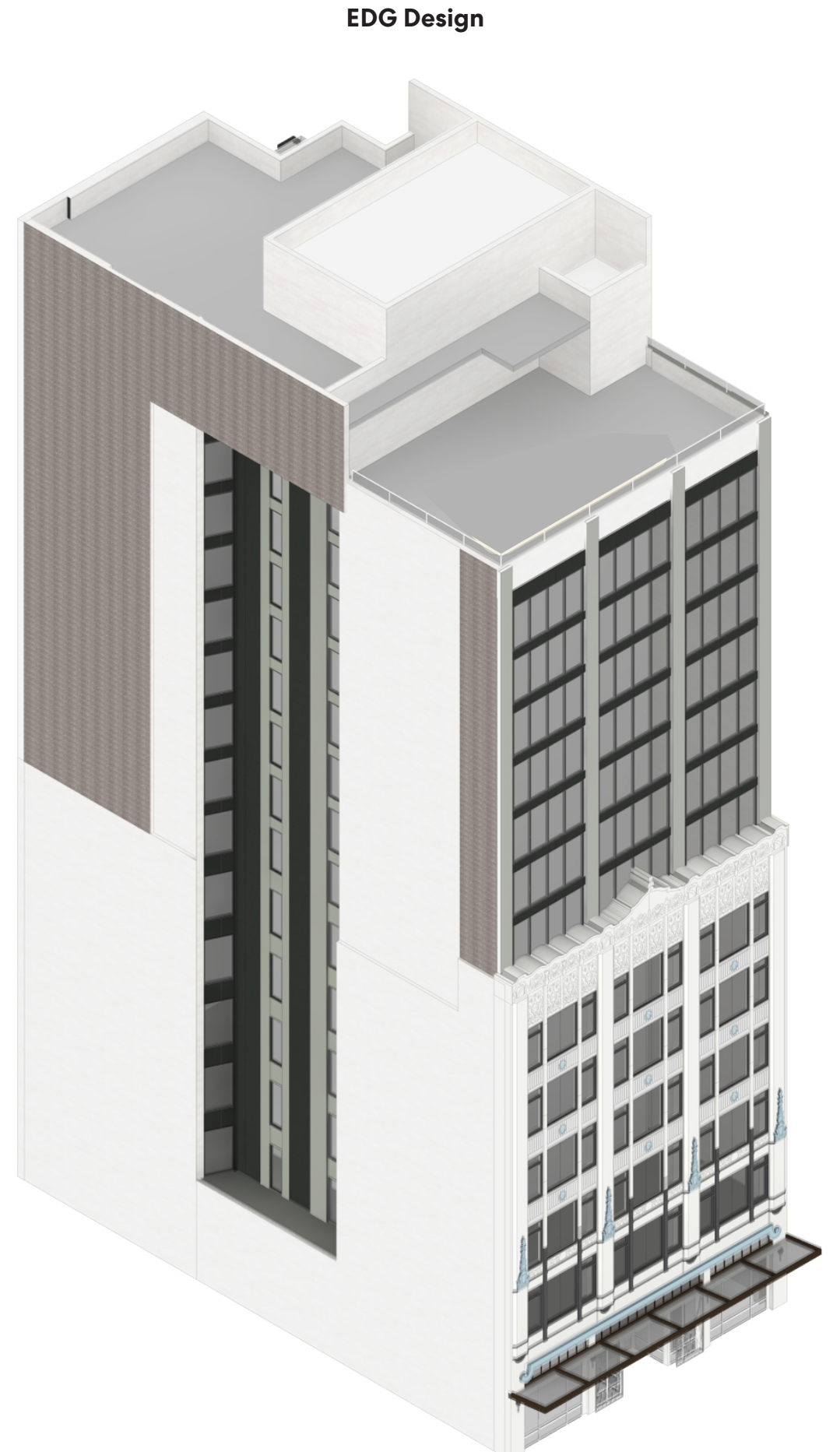
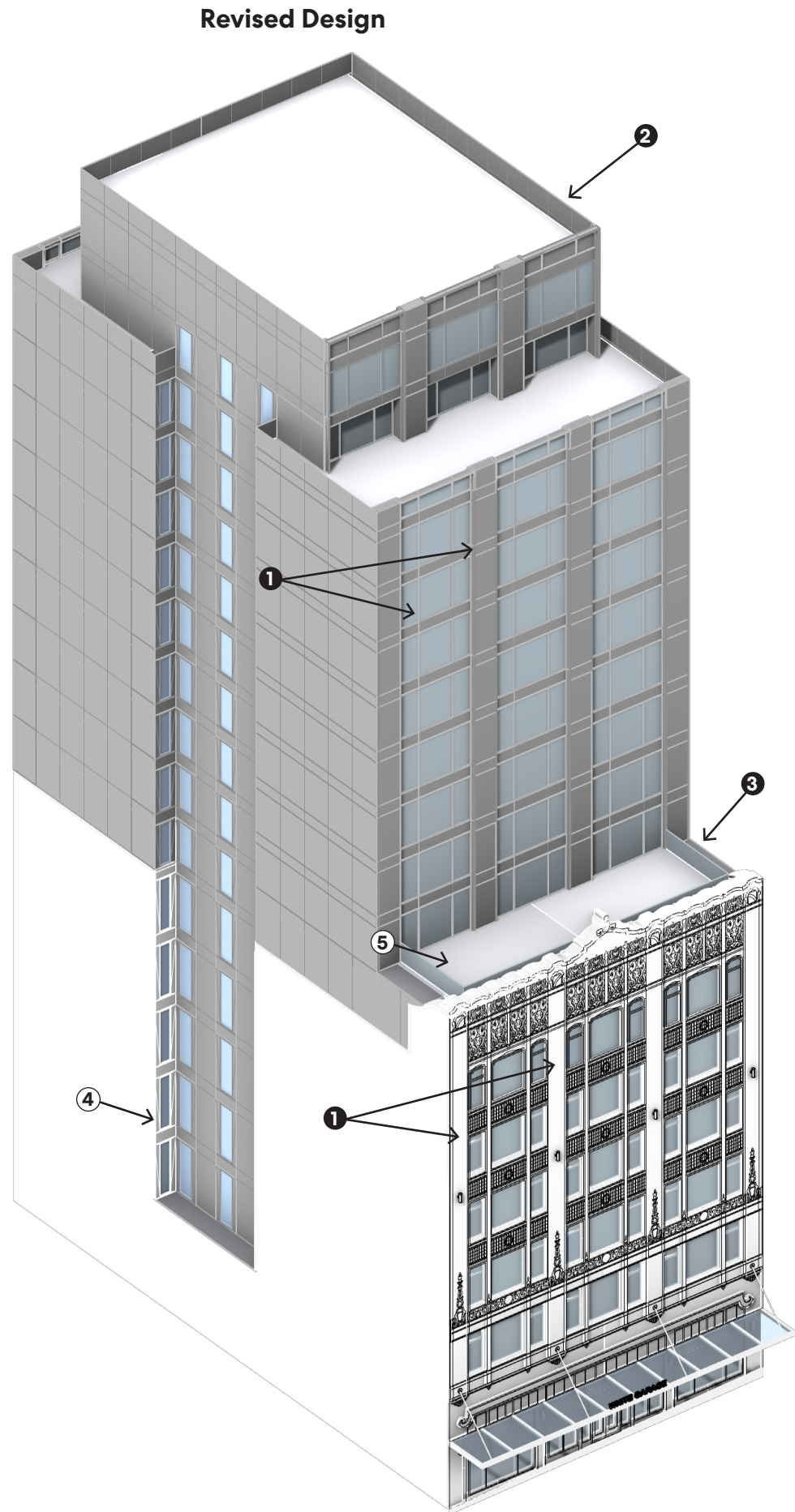
### 3 Material Transitions

• **SDCI Comment:** This material transition on the side facades appears to occur within the same plane. Without a change of plane, siding changes appear only as applied patterns, and may weaken the larger composition. Can this transition be simplified or strengthened through a legible change in plane? (Design Guideline DC2).

• **Response:** The updated design establishes clear material transitions that maintain a hierarchy between the landmark structure and the new structure.

### Voluntary Improvements

- ④ Revised window condition at zero lot lines north and south
- ⑤ Increased the setback of the new addition to provide visual relief and modulation that bolsters a clear hierarchy between the two structures







←  
View from 3rd Ave. and  
Virginia St.





←  
View from 3rd Ave.  
and Olive St.





←  
Pedestrian experience  
along 3rd Ave.

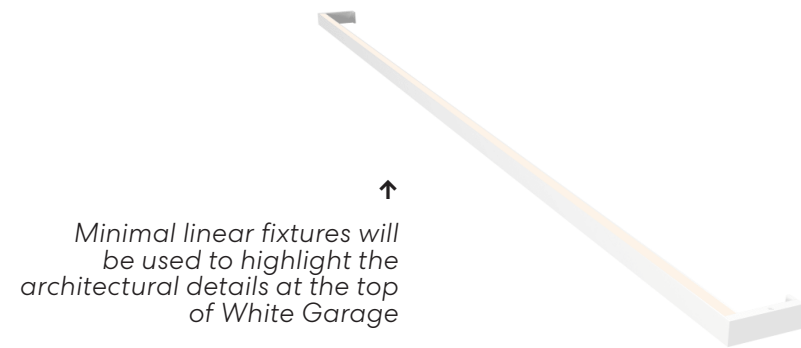


**Exterior Lighting Strategy**

Our lighting strategy is centered around highlighting the ornate details of the landmark structure while also addressing pedestrian security concerns along the street.

LED luminaires will be used to keep energy demand low, while also seamlessly integrating into automated control systems to manage operation.

Luminaire optics will be specified to reduce light trespass into residential units and neighboring buildings. This means narrow wall-washing optics for the cylindrical lamps and low-angle optics for the linear fixtures.

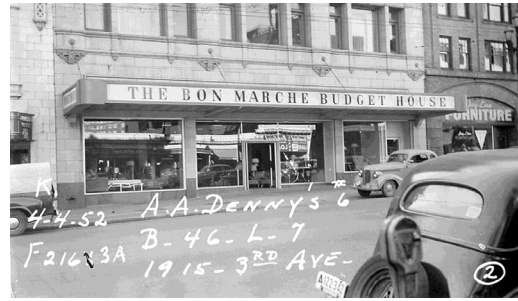




**Exterior Building Signage**

Similar to the historical signage found on the White Garage in the early 1900s, we plan to mount our signage on the ground level canopy.

The current low-profile canopy reduces its visual weight, allowing the existing ornate details to be expressed.



→ Historical building signage of White Garage



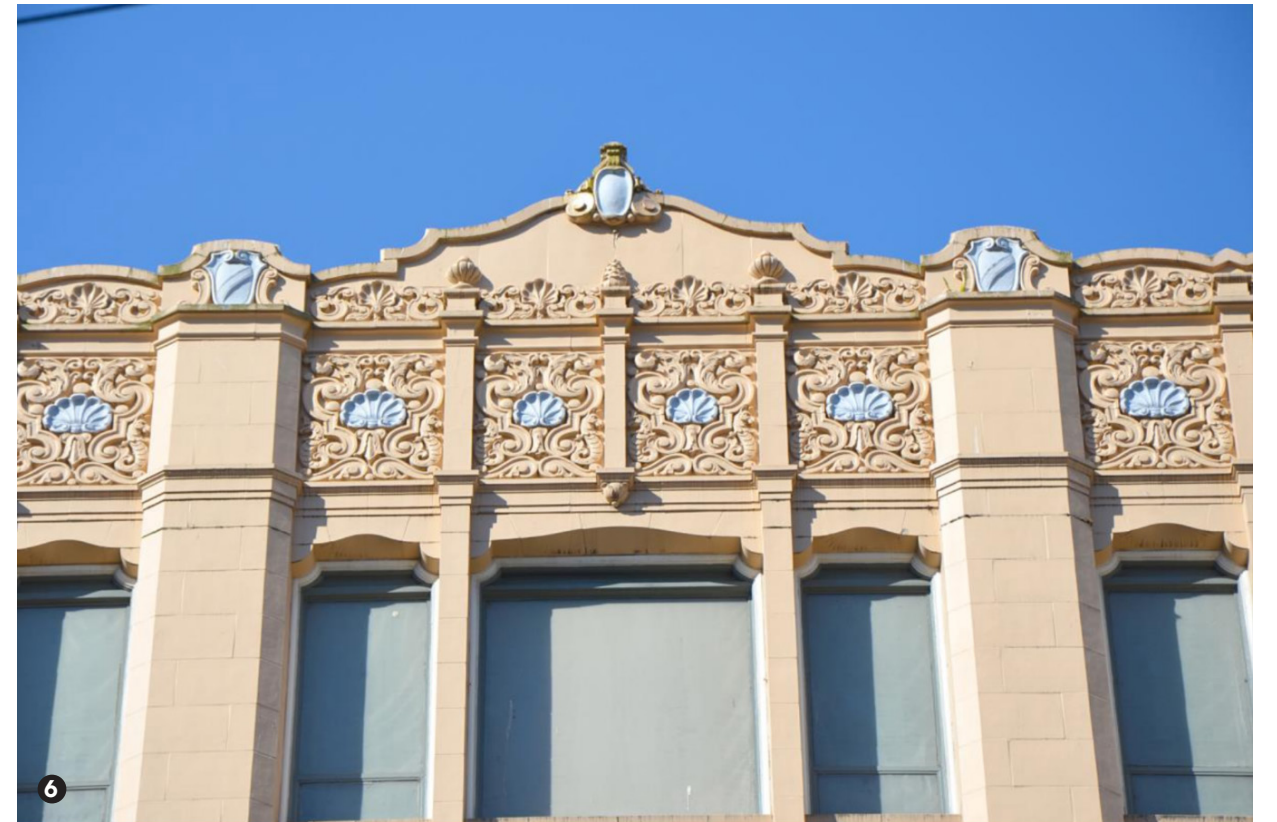
A minimal approach will be taken with the ground level retail signage to not distract from the ornate architecture of the landmark building.

→ Example signage to be hung from canopy or wall-mounted





5.0 Existing Site Conditions / Exterior



1. East Elevation along 3rd Ave.

2. Details of the upper level terra cotta

3. Details of window terra cotta

4. Details of ground level terra cotta

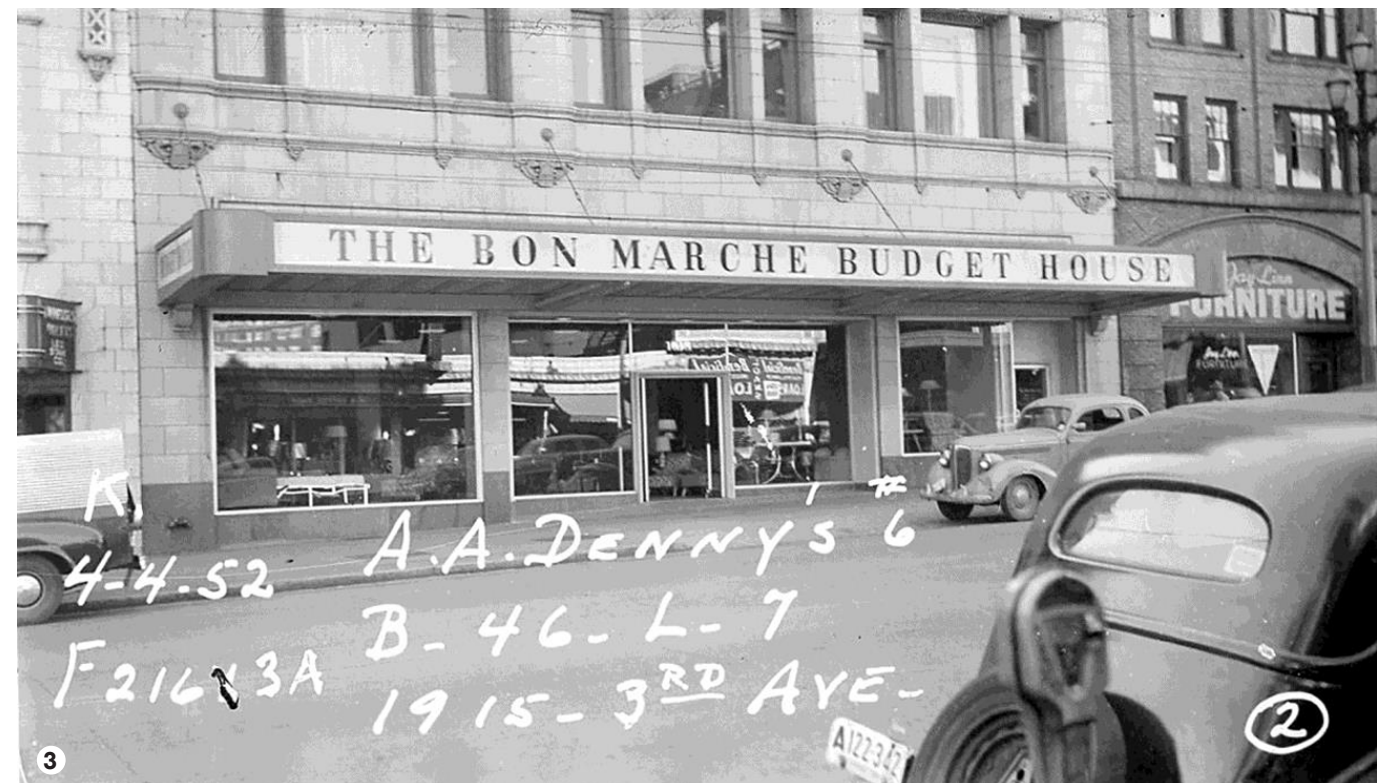
5. West Elevation along the alley

6. Details of parapet terra cotta



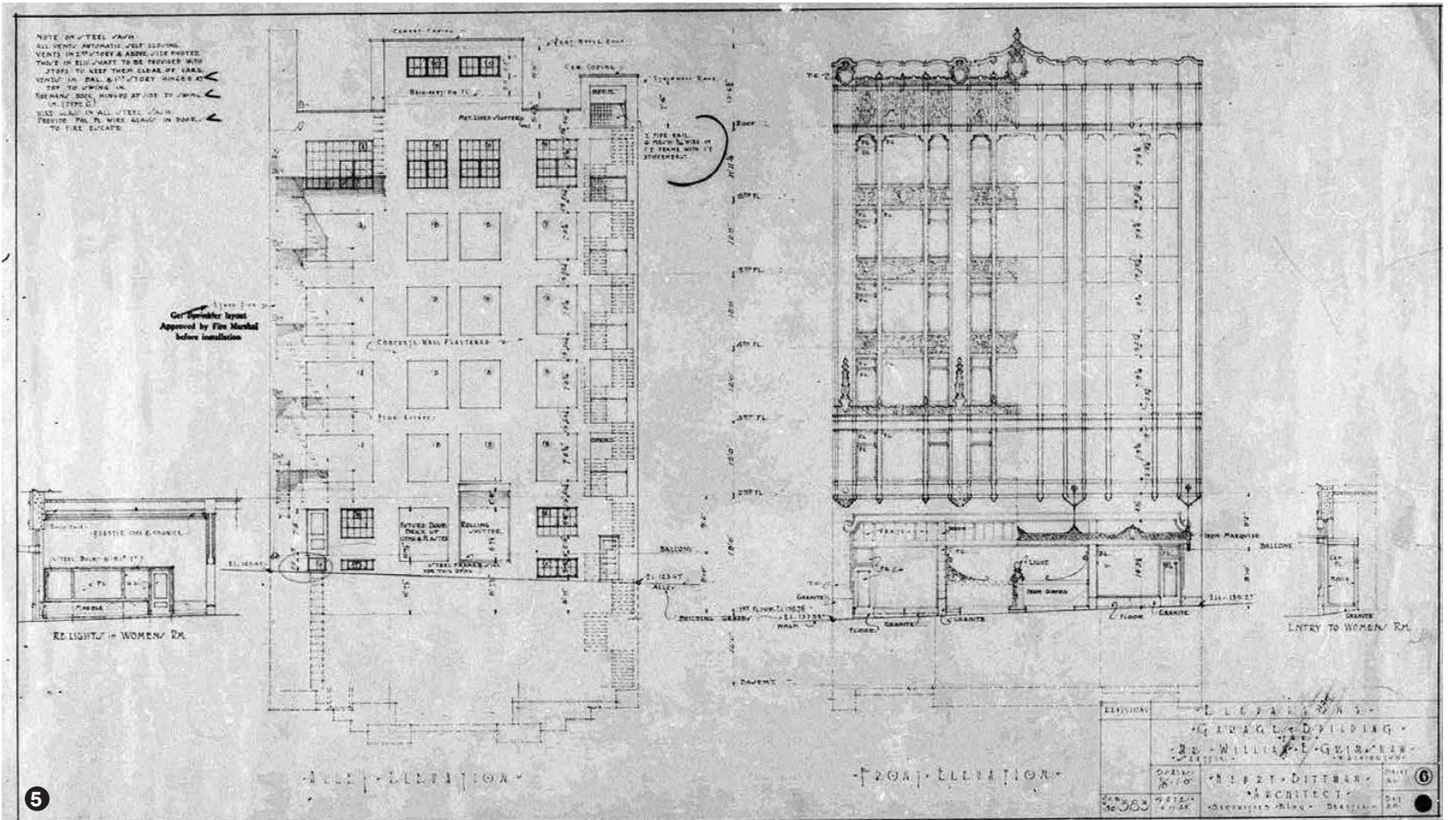
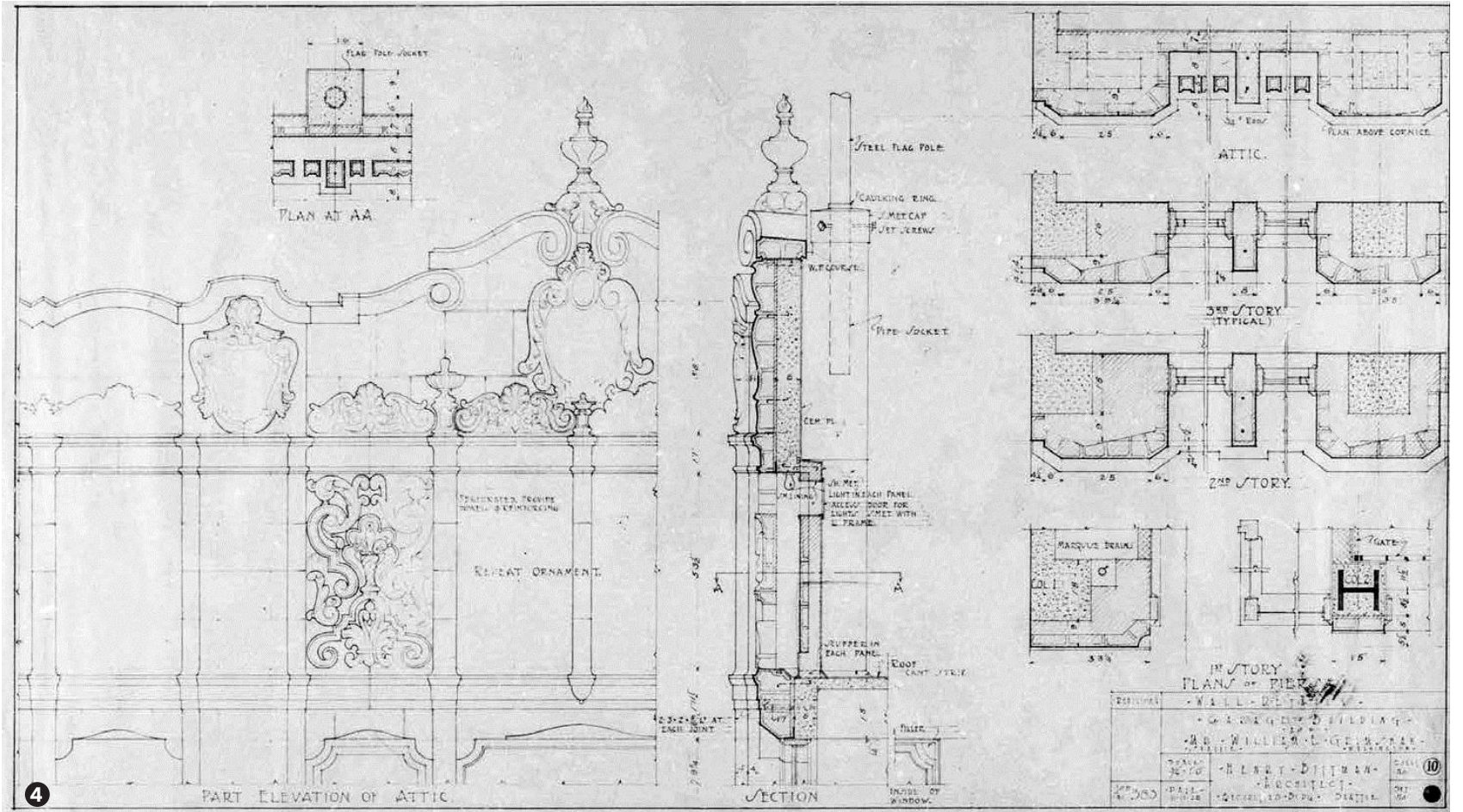
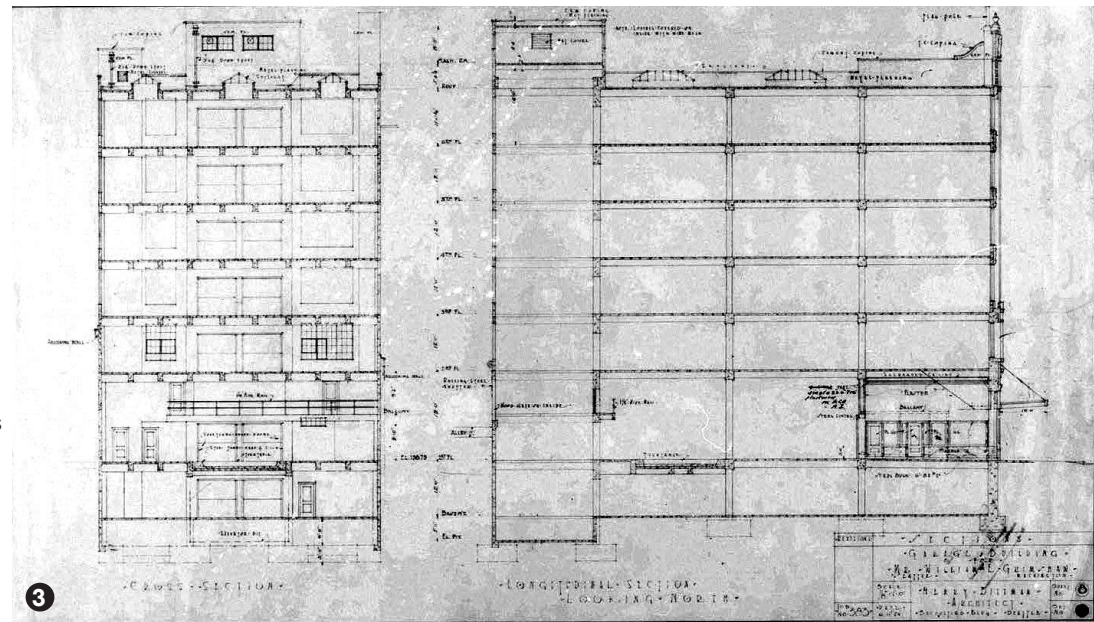
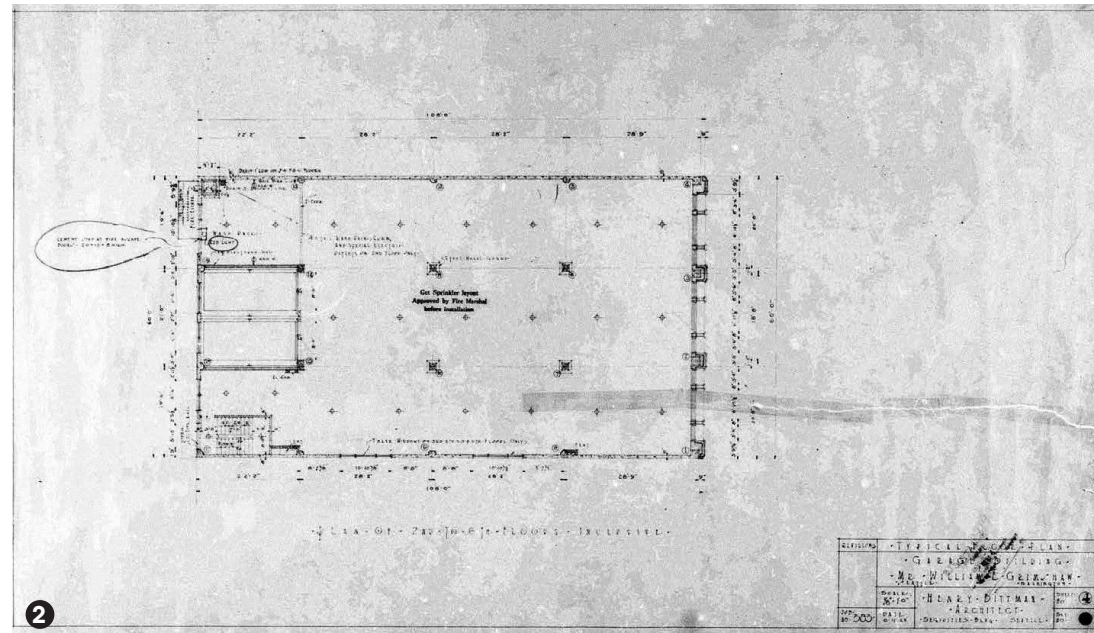
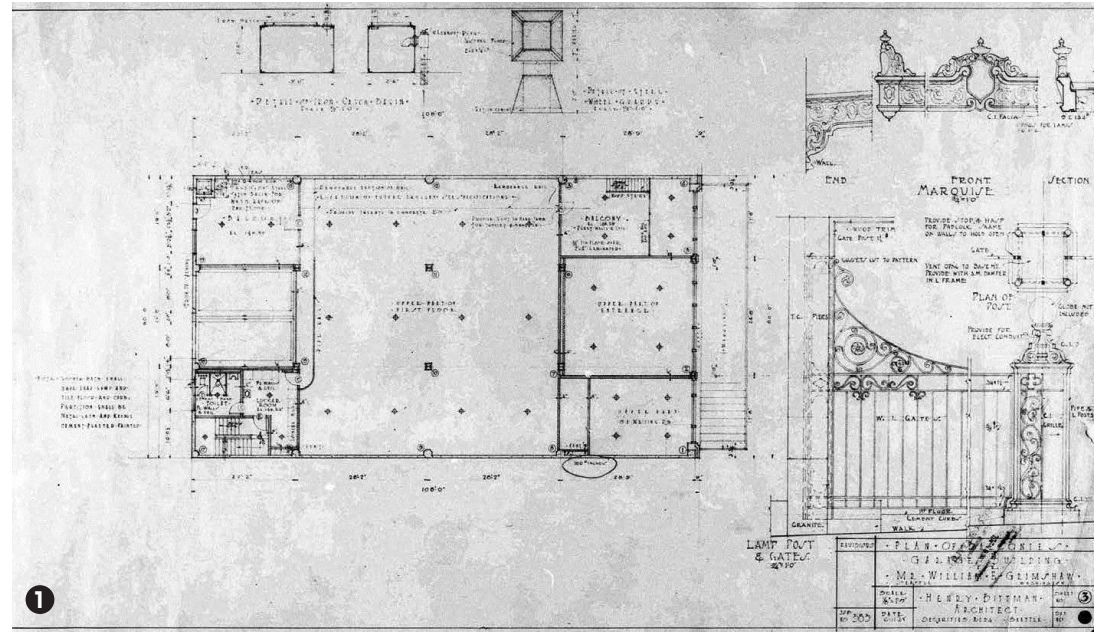


1. White Garage, completed in 1928  
 2. White Garage & Haddon Hall (right)  
 3. White Garage converts to Bon Marche Budget House Furniture, 1951



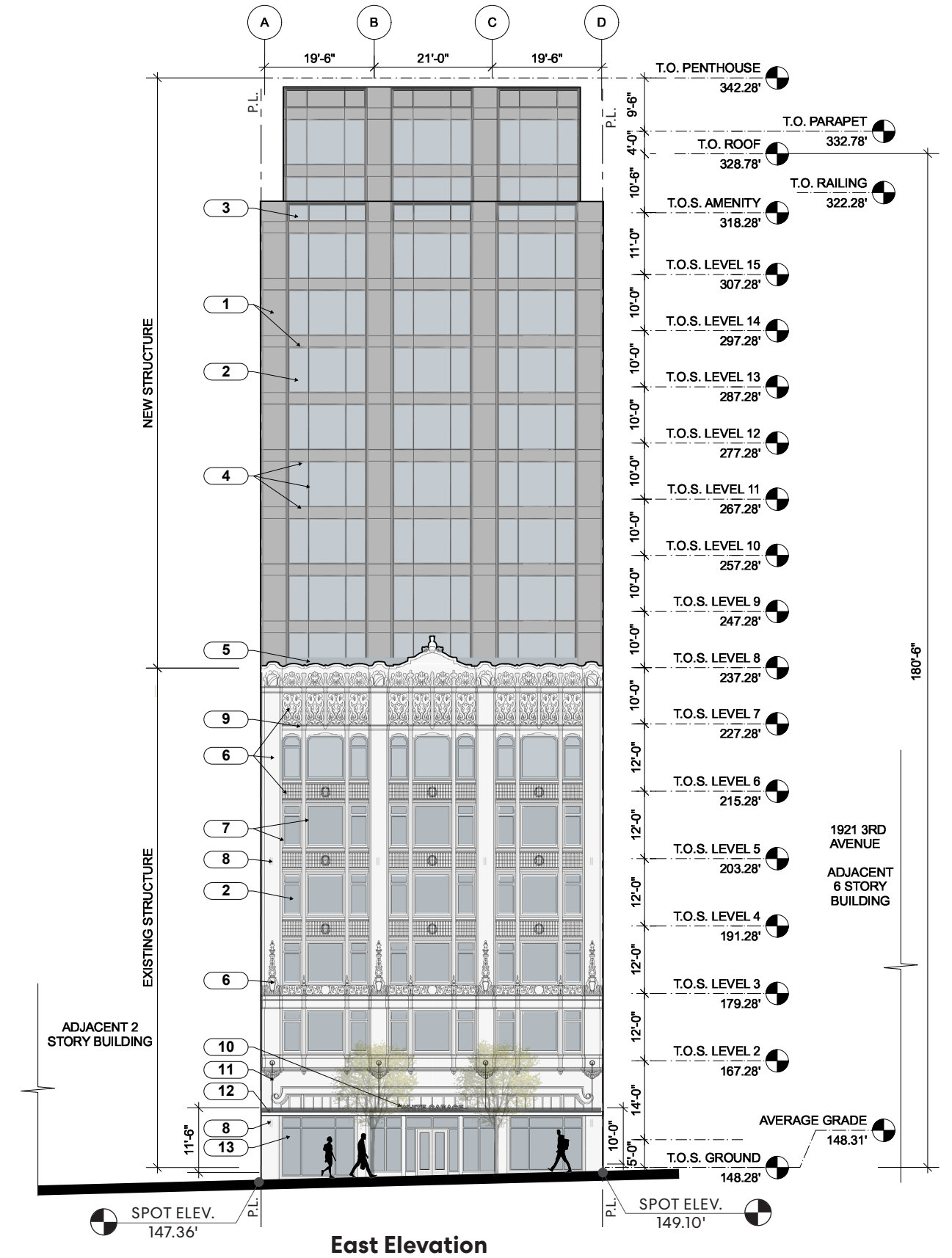
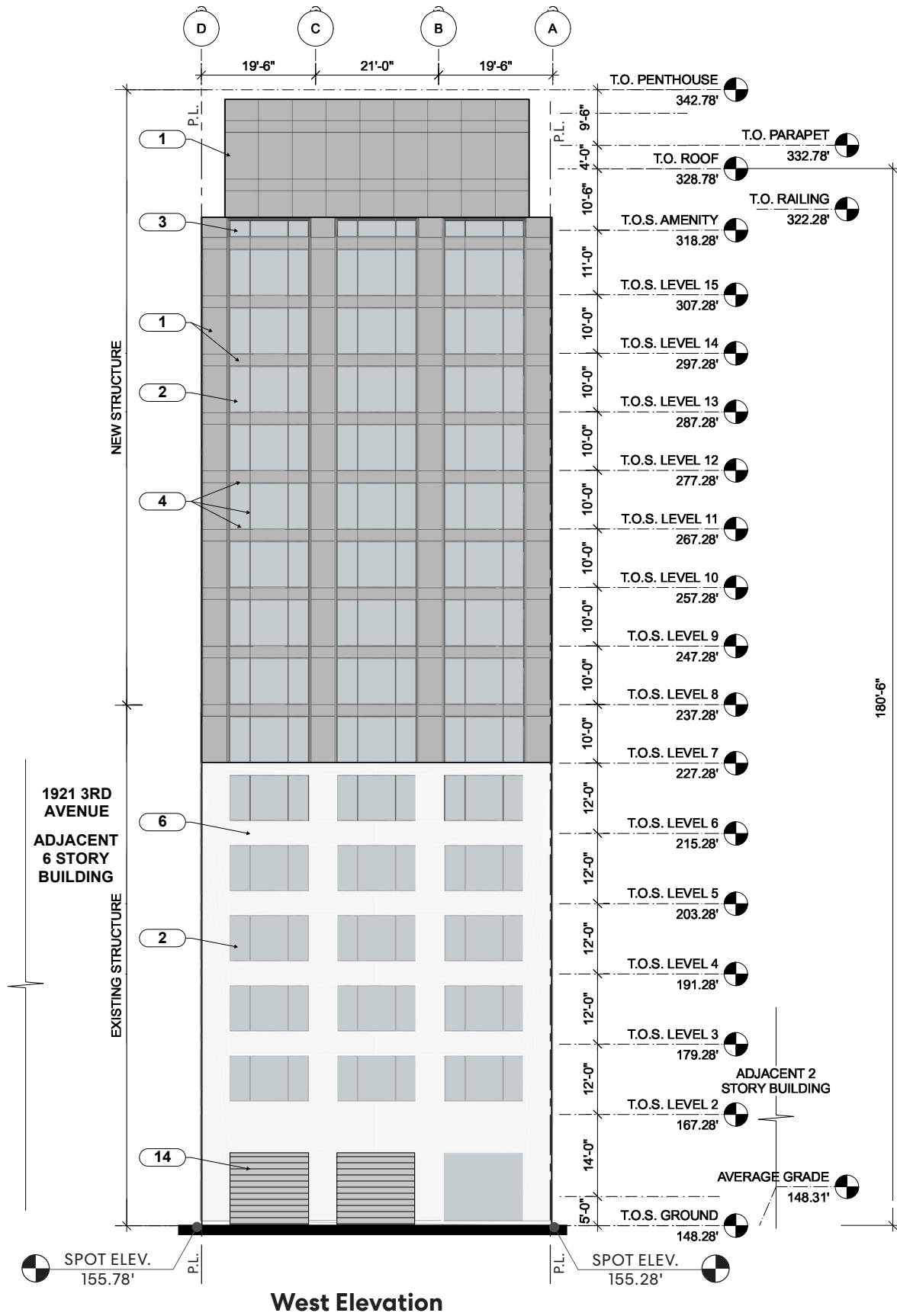


5.0 Existing Site Conditions / Historic Drawings



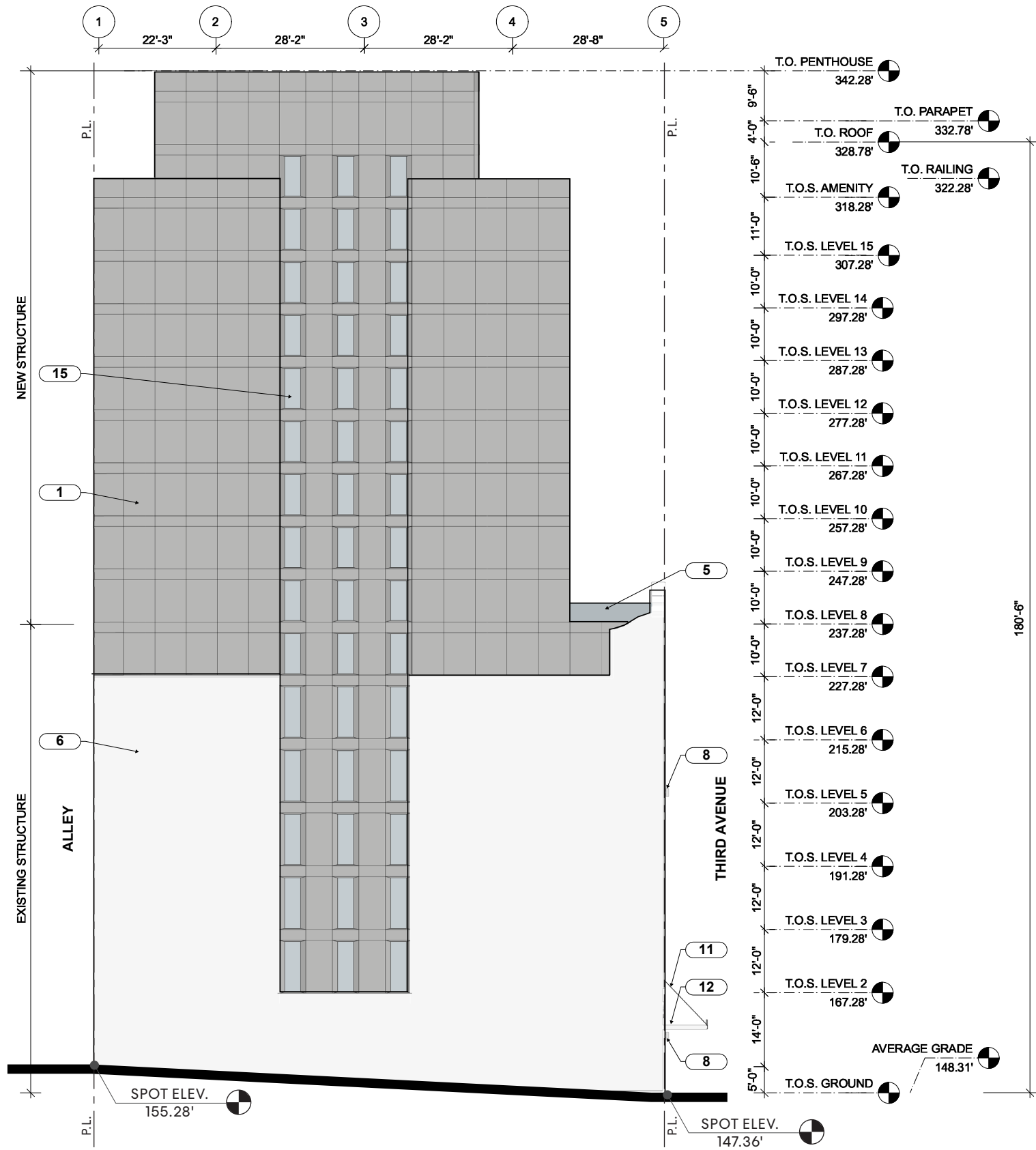
- 1. L1 Plan & Gate Details
- 2. L2 Plan
- 3. Sections
- 4. Ornamental details
- 5. Elevations



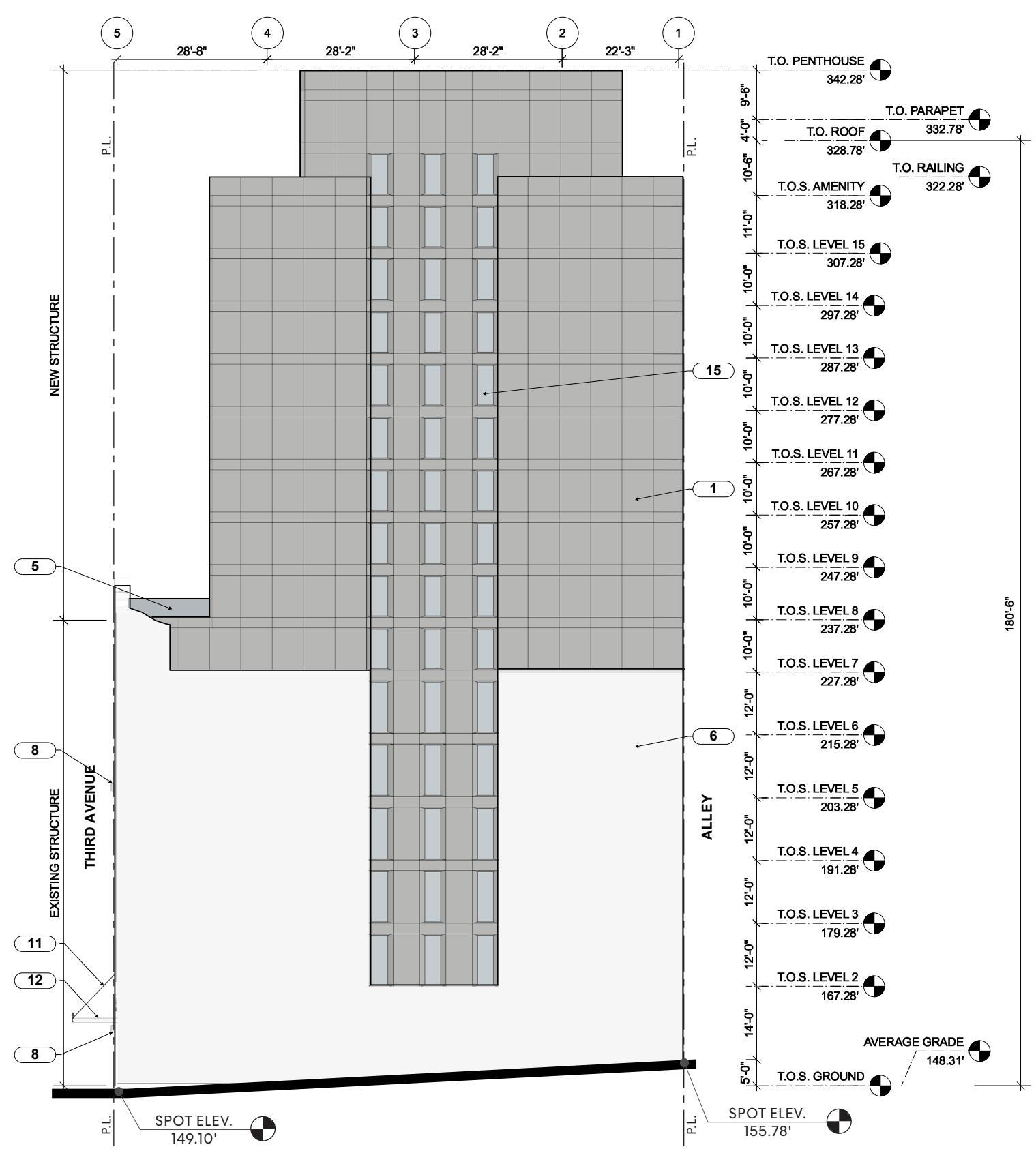




11.0 Elevations / North and South

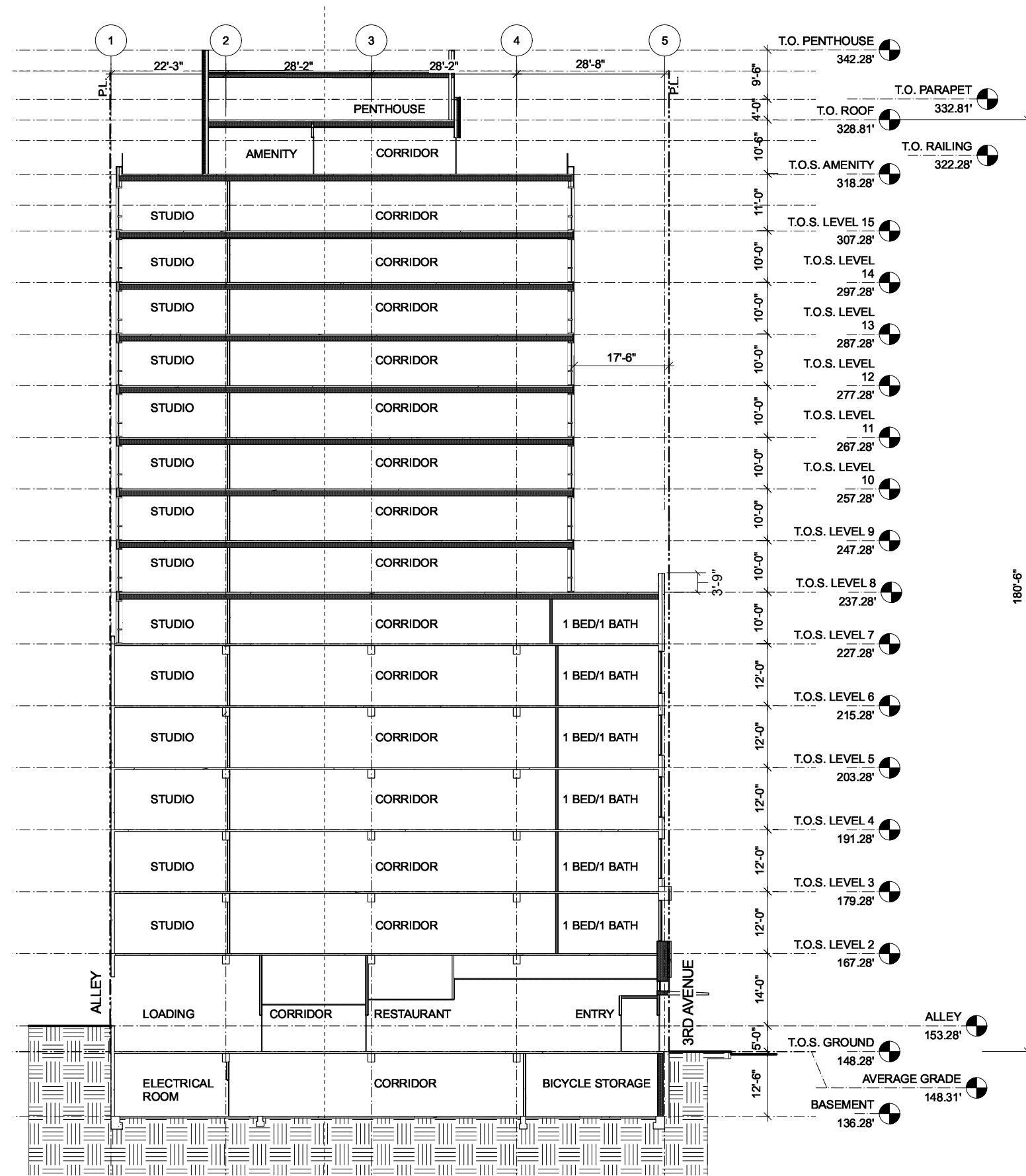


South Elevation

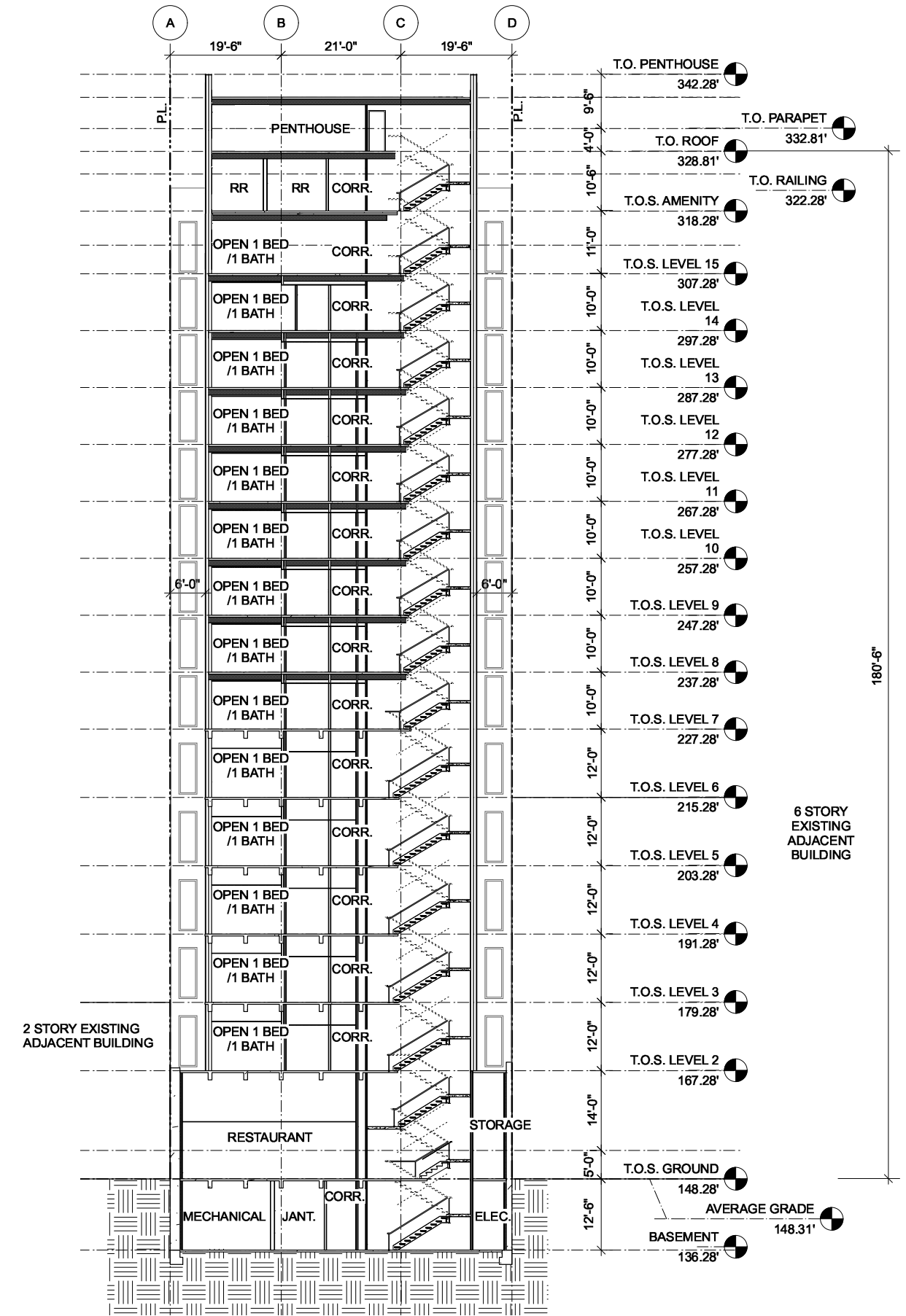


North Elevation





Longitudinal Section

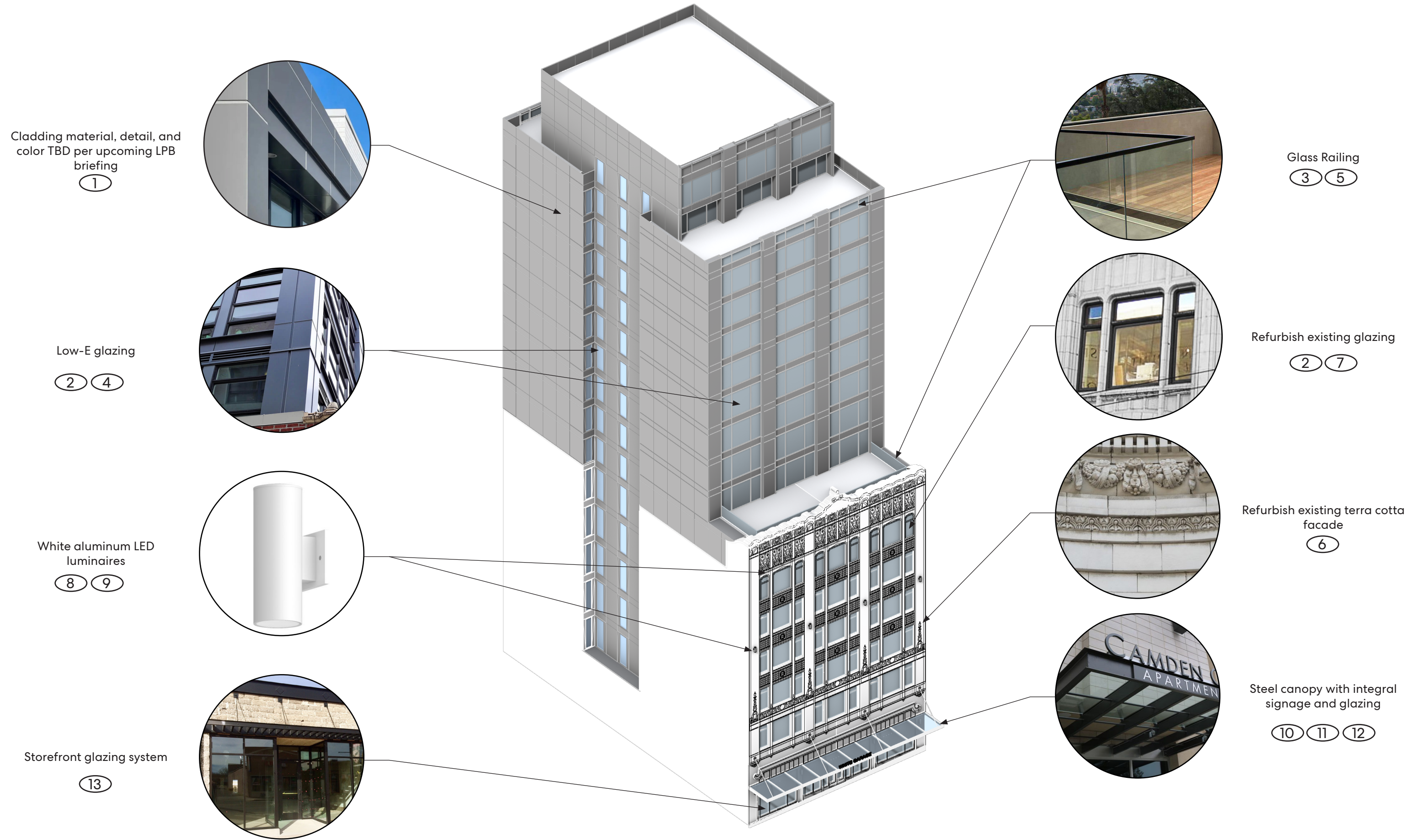


Transverse Section

6 STORY EXISTING ADJACENT BUILDING

2 STORY EXISTING ADJACENT BUILDING





Cladding material, detail, and color TBD per upcoming LPB briefing  
 ①



Low-E glazing  
 ② ④



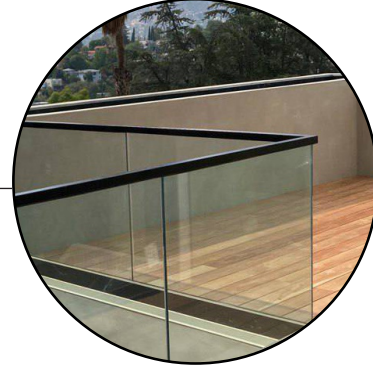
White aluminum LED luminaires  
 ⑧ ⑨



Storefront glazing system  
 ⑬



Glass Railing  
 ③ ⑤



Refurbish existing glazing  
 ② ⑦



Refurbish existing terra cotta facade  
 ⑥



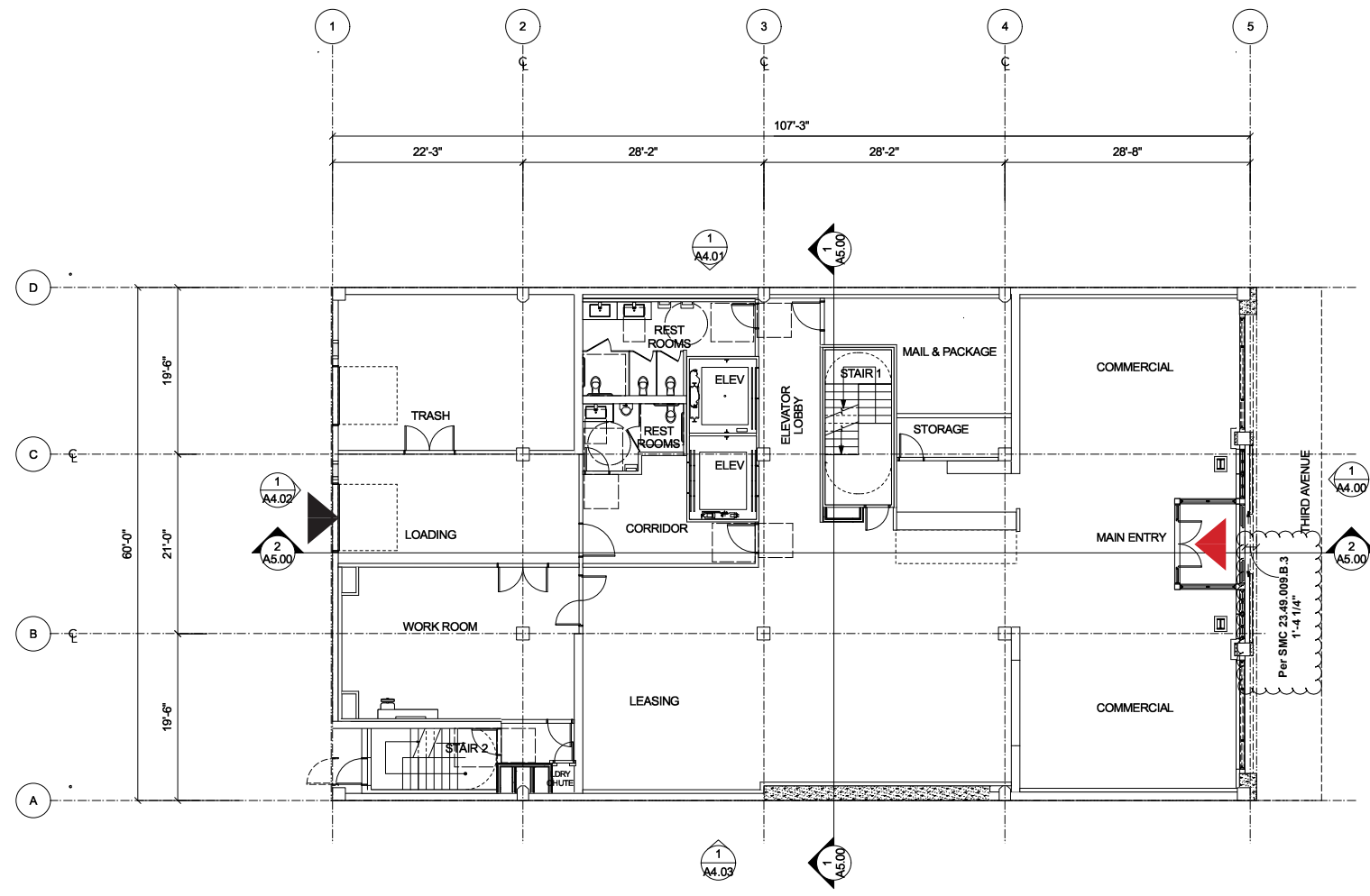
Steel canopy with integral signage and glazing  
 ⑩ ⑪ ⑫





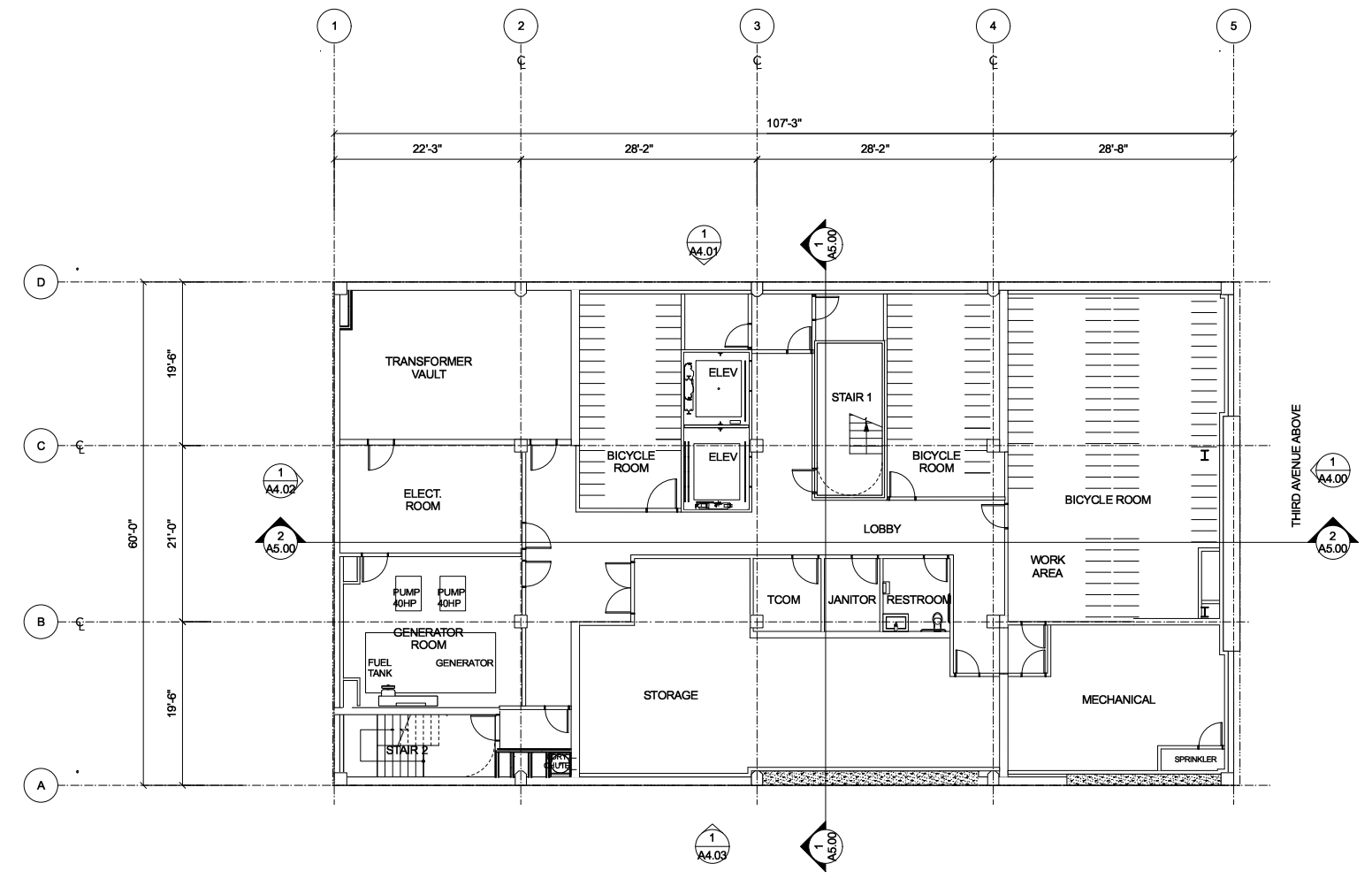






Plan: Level 1 Street

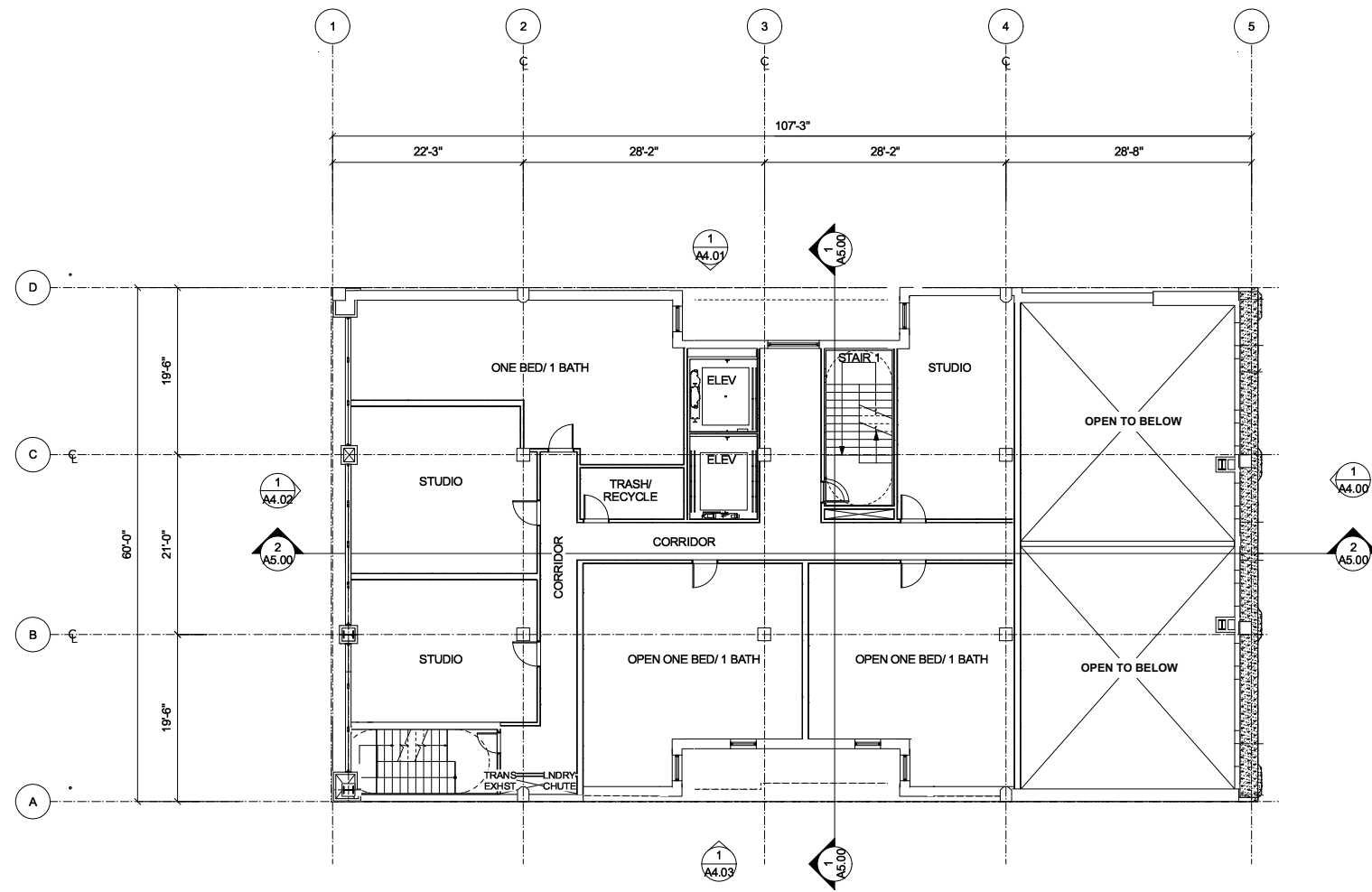
▶ Pedestrian Entrance    
 ▶ Loading Entrance



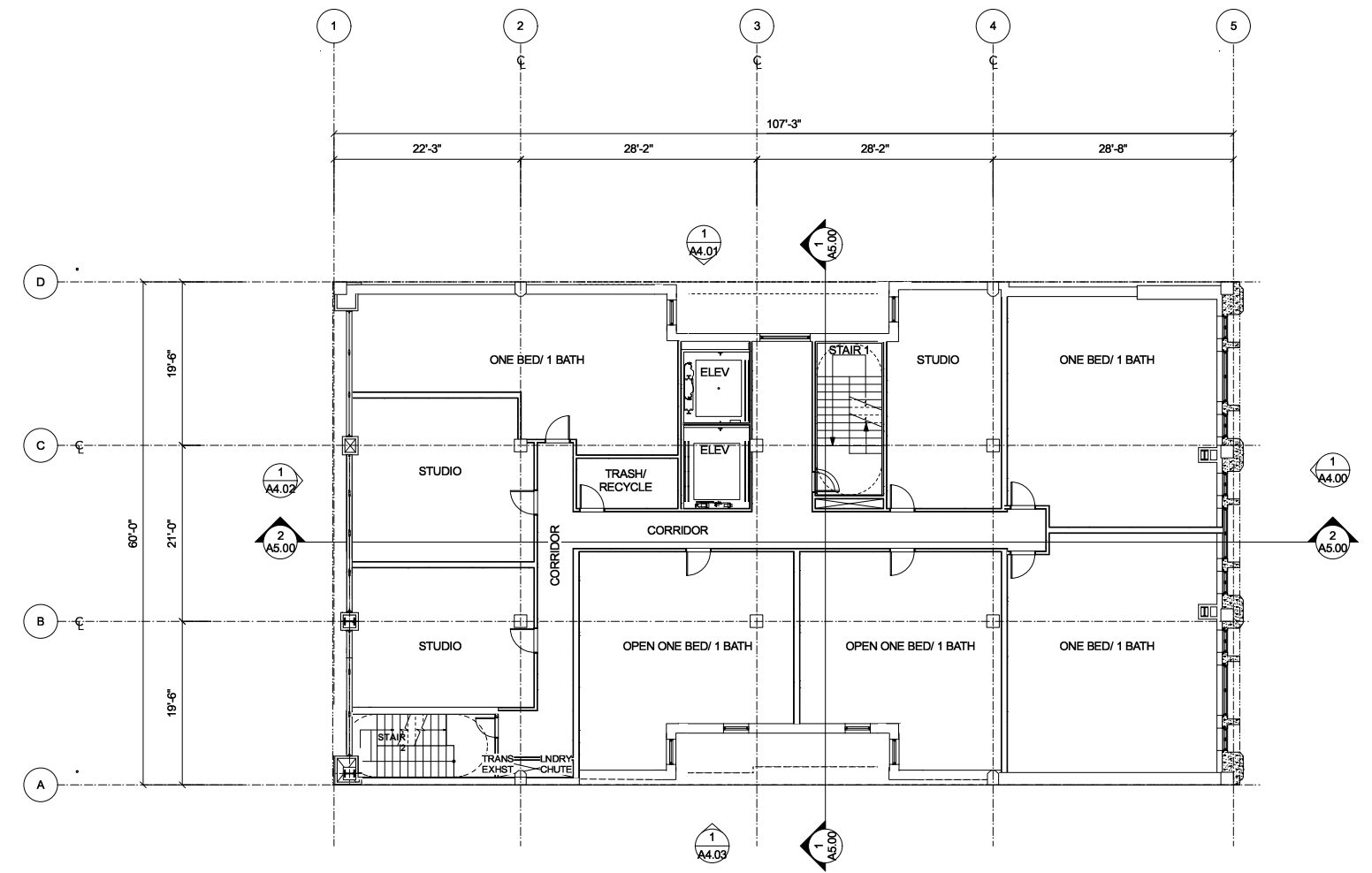
Plan: Basement Level







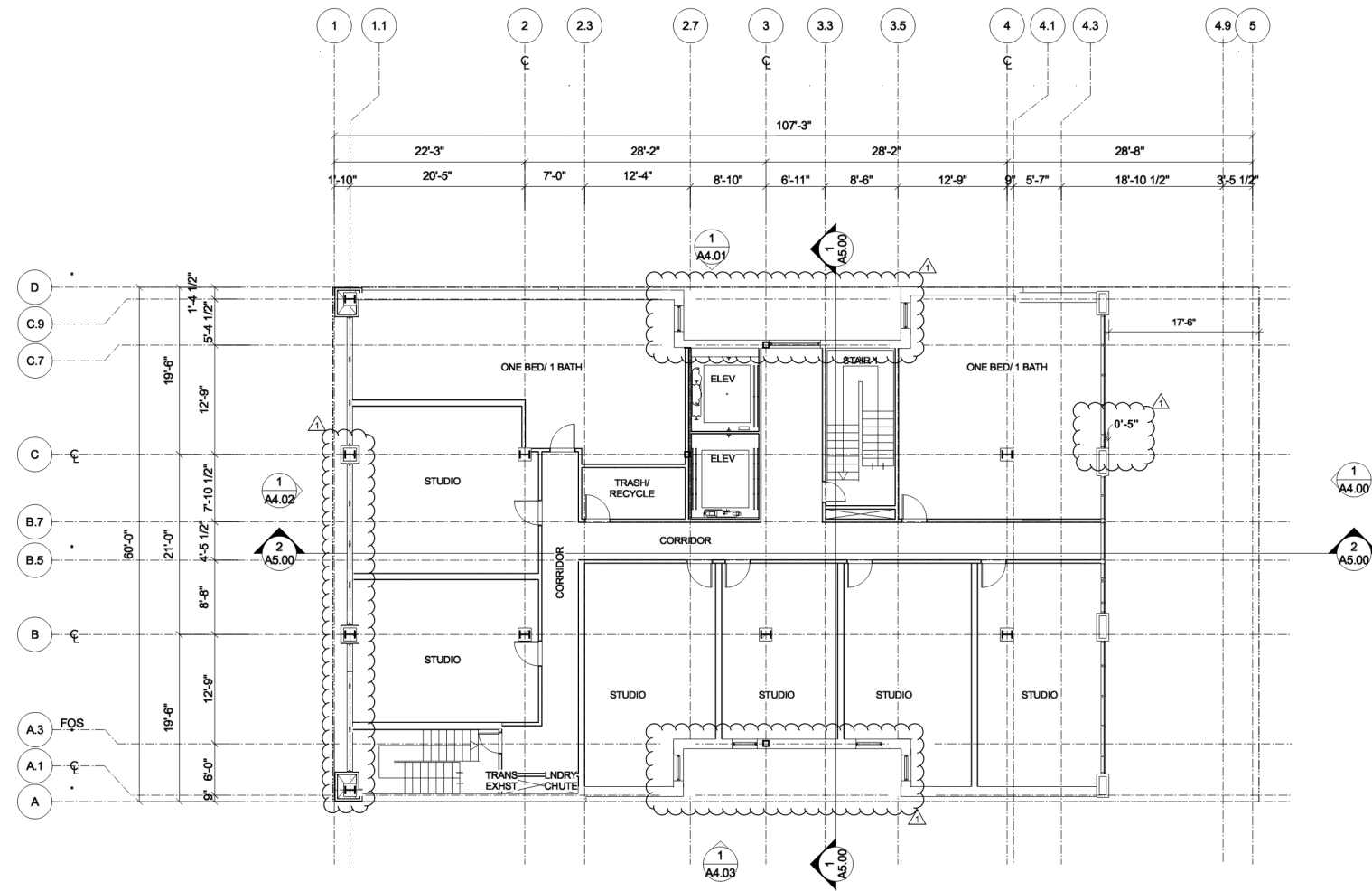
Plan: Level 7 Residential Units



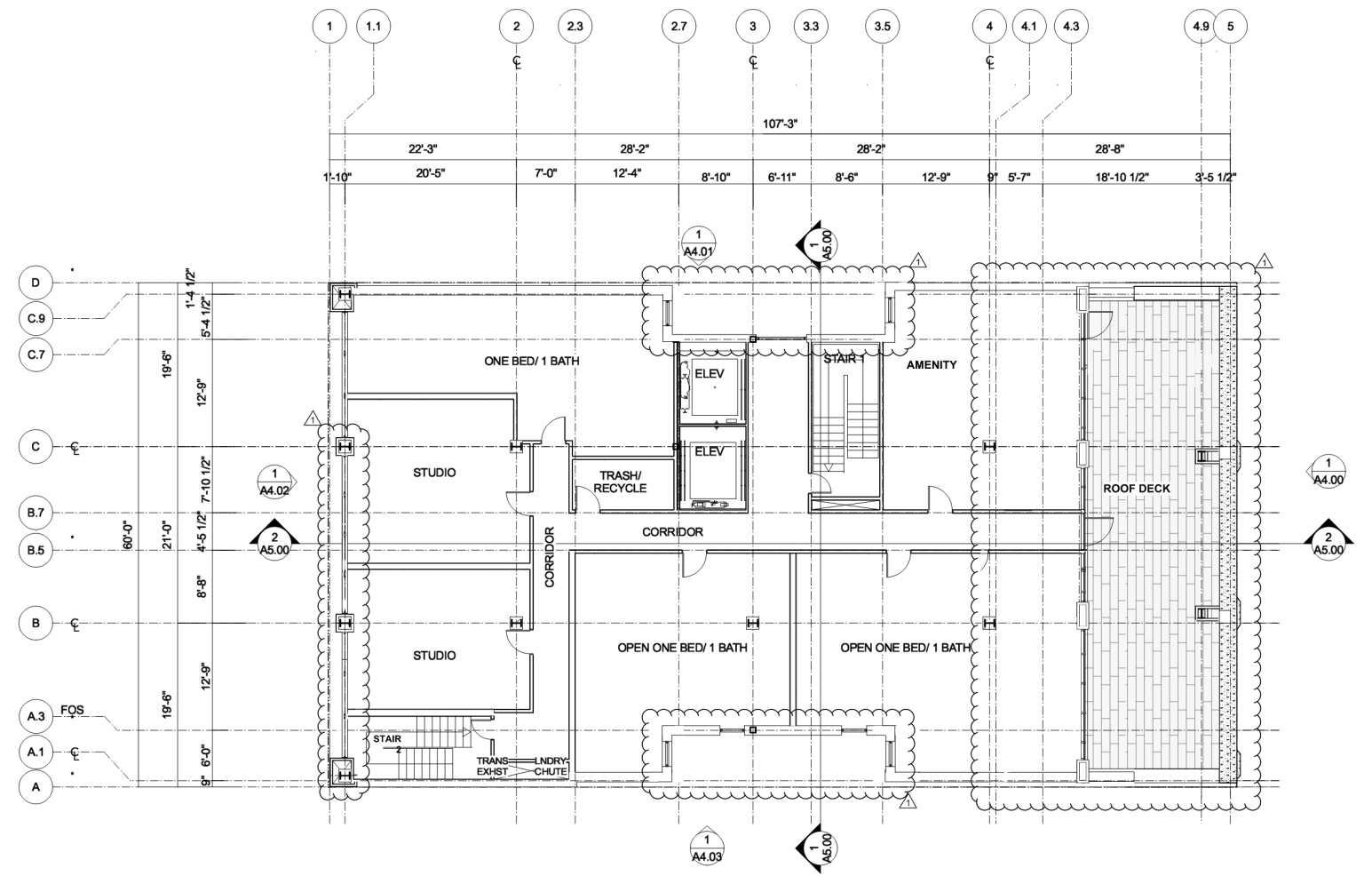
Plan: Levels 2-6 Residential Units







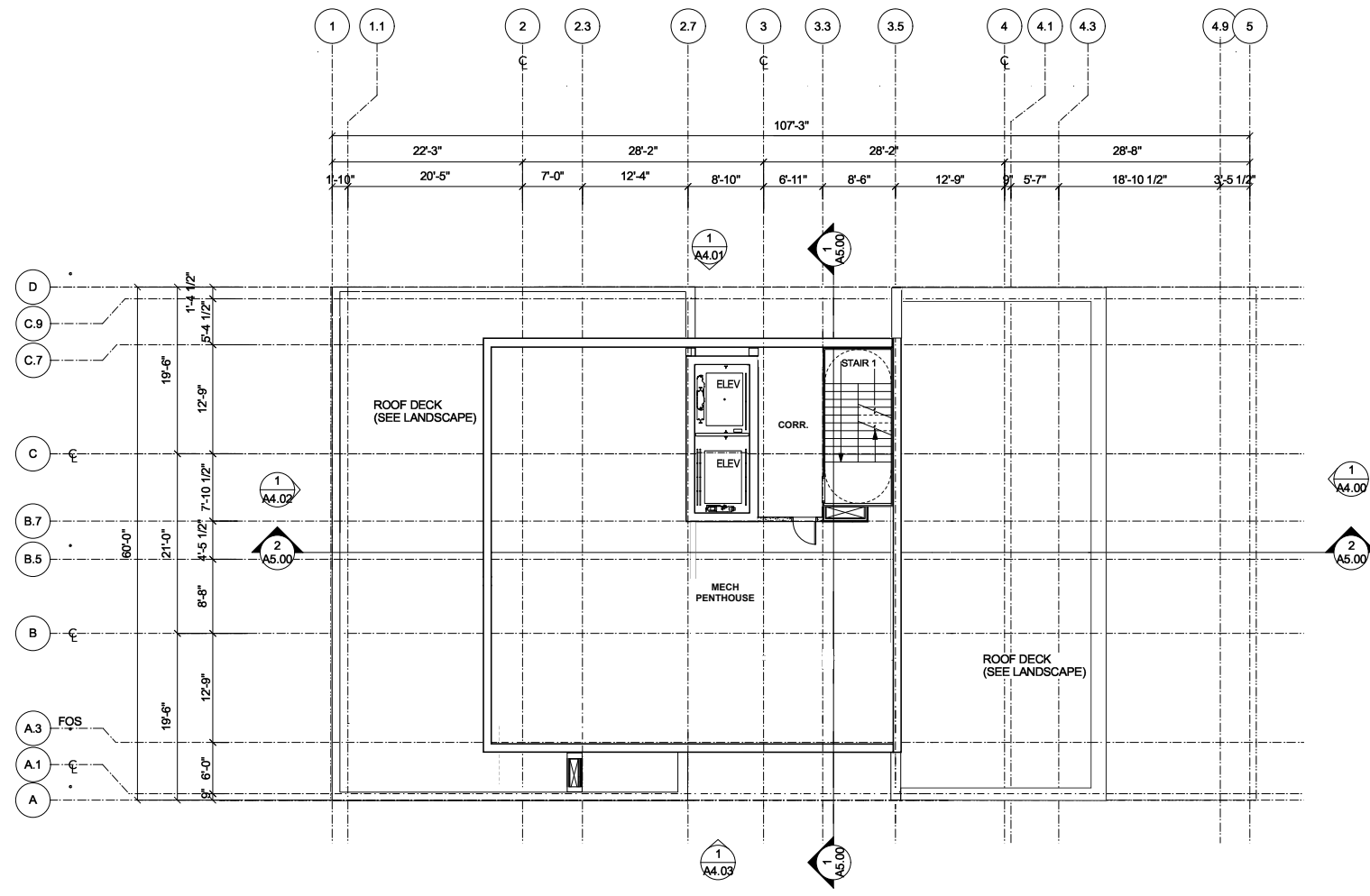
Plan: Level 9-15 Residential Units



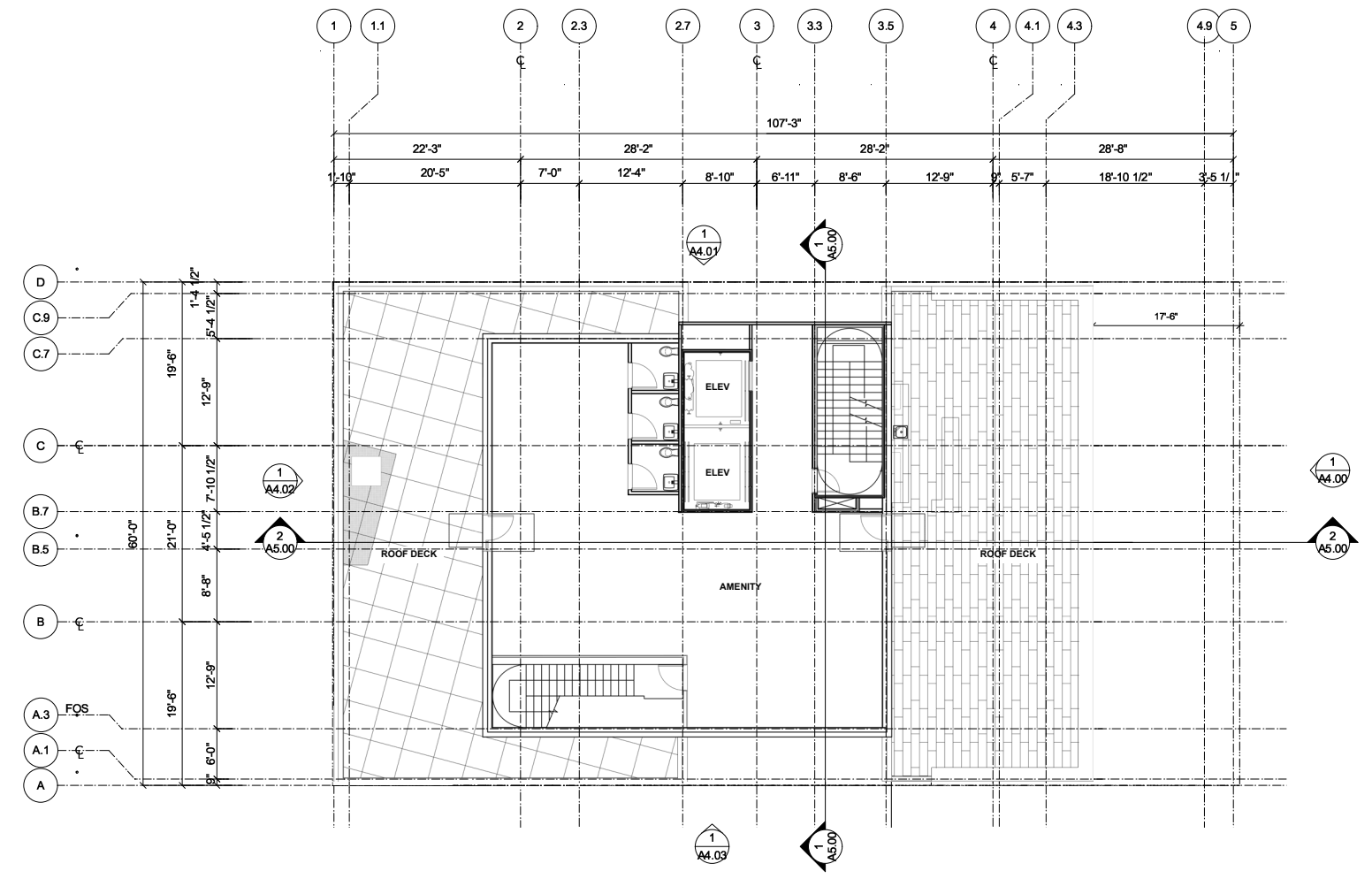
Plan: Level 8 Residential Units







Plan: Upper Roof Level



Plan: Level 16 Amenity Space





# ADDENDUM

Excerpts from EDG  
#3037644-EG



# urban design analysis



## broad neighborhood context

This site is located in the Belltown Urban Village Center, a part of the Belltown core of Downtown Seattle. This area of Belltown is filled with rapid development, containing contemporary high rises and towers, as well as a fabric of historic buildings. New developments are reaching for maximum zoning heights while many mid and low rise buildings remain. Belltown's residential capacity is increasing with each of these new developments.
















This site is centered in an area rich with entertainment, night life, shopping, and transport to other parts of the city. It is a short walk to Belltown's main restaurant and bar avenues, as well as entertainment venues, such as the Moore Theater. It is close to Pike Place Market, Westlake Center, and Pacific Place. It sits along Seattle's Third Avenue Downtown Bus Corridor and is a five minute walk to the light rail station and the monorail. It also sits between the current Second Avenue and future Fourth Avenue bicycle lanes.

## immediate neighborhood context

The site is located at 1915 3rd Avenue, mid-block on 3rd avenue in the Belltown core of Downtown Seattle. It is zoned DMC 240/290-440 and is within walking distance to all of the major downtown amenities including the Pike's Place Market, West Lake Center and the Monorail. The surrounding buildings include a variety of uses and heights ranging from two story retail spaces, multi story parking structures, and several other mixed use projects providing both housing and retail spaces for a very active streetscape experience. When completed, the project will be approximately the same height as the building across the alley with views of Mt. Rainier and the Puget Sound.



**KEY:**

	DOC1 U/450-U		PMM 85		DMR/R 85
	DMC 340/290-440		DMC 145		DMC 125
	PSM 100/100-120		DRC 85-170		DMC 340/290-440
	DMC 170		DOC2 500/300-550		
	DMC 170		DMC 340/290-440		
	DMC 240/290-440		DMR/R 145/65		

## adjacent and nearby streets

The White Garage is located along Third Avenue between Virginia Street and Stewart Street. Third Avenue is a Class I pedestrian street with street frontage use, overhead weather protection, streetscape, and transparency requirements.

## views and amenities

At the building's proposed height, there will be views of the Puget Sound, Mt. Rainier, and Downtown Seattle. Plans include a rooftop amenity bar with outdoor pool, as well as a rooftop residential amenity space. Multiple hubs of entertainment, restaurants, and night life surround the site.

## future projects

The surrounding 9 Block Vicinity includes multiple active high rise developments, most reaching the maximum allowed zoning height of 440'. This includes projects such as 3rd and Virginia, 3rd and Lenora, and 2nd and Stewart, all reaching for maximum zoning.

## community outreach

The project participated in City of Seattle's Early Outreach for Design Review process, completed in August of 2021. Flyers were mailed to 824 residents and businesses within 500-feet of 1915 3rd Ave, including project address, background, website and email contact information. Flyers were also shared by e-mail with neighborhood groups, including Bellevue Community Council, Belltown United and Downtown Seattle Association regarding the project.

An invitation to participate in an online project survey was provided and completed by 10 people. Over the duration of the community outreach, the project team developed and monitored a project website which received 38 total visits during the public comment period. The project team fielded feedback and expressions of support for the project through messages received at a project email account.

Comments overall expressed support for this project, responding to the building design elements (i.e. materials, character, eco-friendliness) and non-design elements (i.e. retail, security, units). Specific design related comments prioritized a unique and interesting building utilizing attractive and environmentally friendly materials. Comments also emphasized the building's relationship to the existing neighborhood character. The current design proposal responds to this input by highlighting the existing, historic façade and its distinctive terracotta ornamentation. The addition to this original structure has been organized to enhance these features through symmetry, repetition, and balanced massing.

Community and site related comments expressed a strong desire for street front activation including commercial uses that are neighborhood focused. The current design proposal includes a commercial and residential lobby at the ground floor, enhancing the existing façade at street level to create a timeless, dramatic, and attractive entrance serving guests and residents. Storefront improvements will engage the public through human scale amenities including unique lighting, weather protection, ornamental plantings, and street trees.



# urban design analysis

9-block vicinity

## HIGH RISE



4

3RD AND LENORA



5

1930 3RD AVE AT 3RD AND VIRGINIA



6

3RD AND VIRGINIA

## ENTERTAINMENT



7

MOORE THEATER + HOTEL

## TRANSPORTATION



10

WESTLAKE LIGHT RAIL STATION



11

3RD AVE BUS CORRIDOR



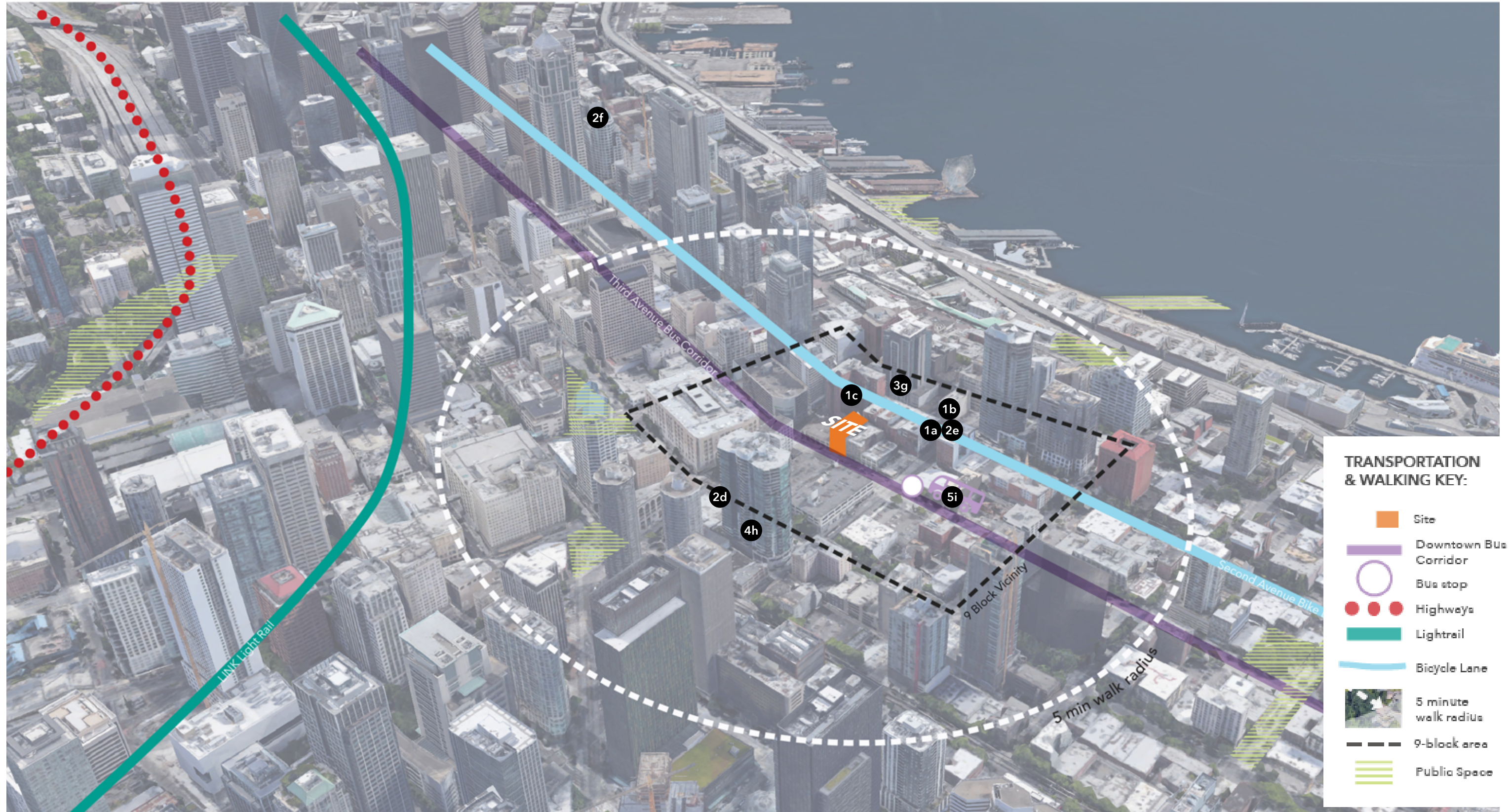
12

2ND AVE BIKE LANE



# urban design analysis

neighborhood vicinity: transit + community connections

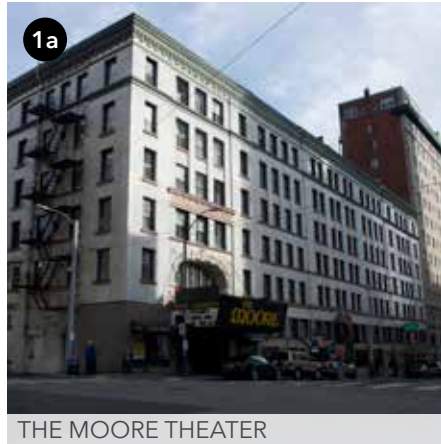




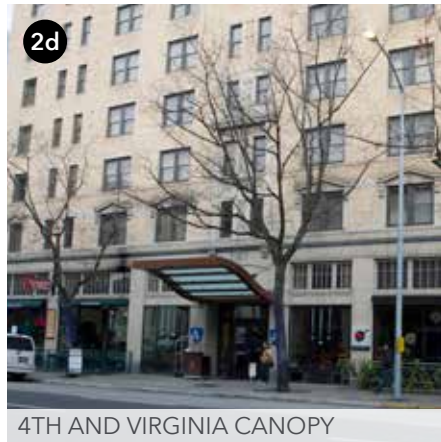
# urban design analysis

## neighborhood vicinity: existing patterns + elements

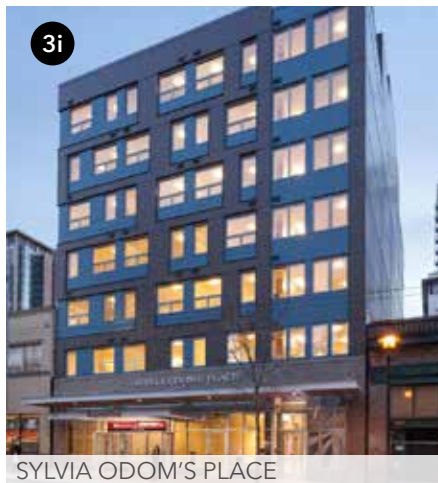
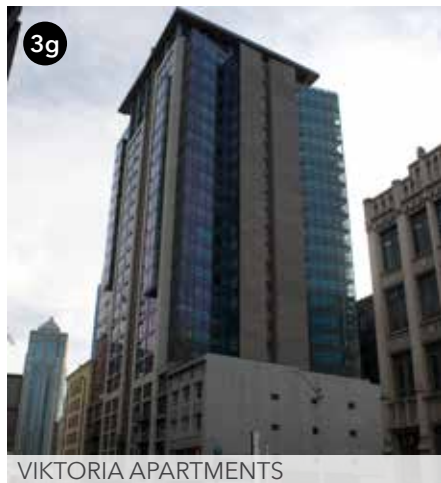
### HISTORIC CONTEXT



### HUMAN SCALE



### RELATION TO NEW DEVELOPMENT



### neighborhood precedents:

- [a] **THE MOORE THEATER**  
1932 2nd Ave
- [b] **THE PALLADIAN**  
2000 2nd Ave
- [c] **THE JOSEPHINUM**  
1902 2nd Ave
- [d] **HOTEL ANDRA**  
4th and Virginia
- [e] **PALLADIAN ENTRY**  
2000 2nd Ave
- [f] **WATERMARK TOWER**  
1107 1st Ave
- [g] **VIKTORIA APARTMENTS**  
1915 2nd Ave
- [h] **ESCALA**  
1920 4th Ave
- [i] **SYLVIA ODOM'S PLACE**  
2017 3rd Ave

- 1 **EXISTING HIGH RISE BUILDINGS: STRONG BASE**  
Belltown is an area with many existing historic high rise buildings to draw precedent from. These historic buildings feature strong pedestal bases and entries along the streetscape. The proposed scheme will reinforce this by restoring the existing decorative elements and reviving the street front and entry. The proposed addition will reinforce the language of the historic facade while not impeding its visual presence.
- 2 **CANOPY & SIGNAGE EXPRESSION | REINFORCE PEDESTRIAN SCALE**  
Many existing historic buildings in the area have included canopy and signage additions to tie in the existing decorative elements with its contemporary use. The proposed scheme will feature a glass canopy with thin elegant lettering to provide an uninterrupted view of the existing terracotta decorative elements.
- 3 **HIGH RISE CONSTRUCTION NEW**  
Because of the variety of scale and building age in the surrounding context, this proposed scheme is uniquely poised to act as a connection between the two scales and aesthetics of this neighborhood. The proposed scheme will revitalize the strong existing structure while providing a contemporary addition that relates back to the original features. The new volume will allow the building remain a prominent feature of this neighborhood, adapting to the changing height and scale of the area.



# urban design analysis

## site context



The Josephinum, 1902 2nd Ave



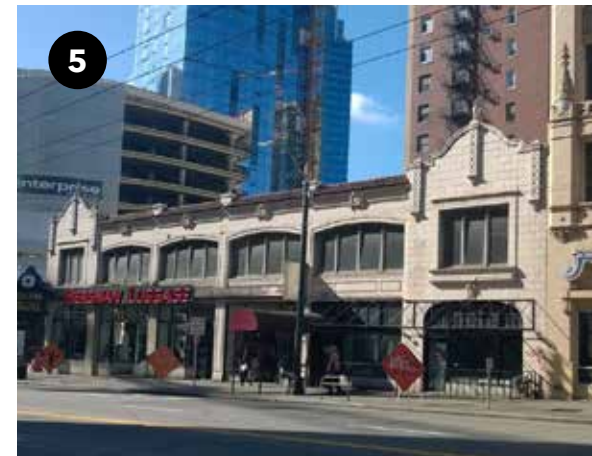
The Palladian



Securities, 1904 3rd Ave



Bon Marche Building, 300 Pine St.



Donahoe Building Bergman Luggage, 1901 3rd Avenue



Dahlia Lounge, 2001 4th Ave



STREET VIEW

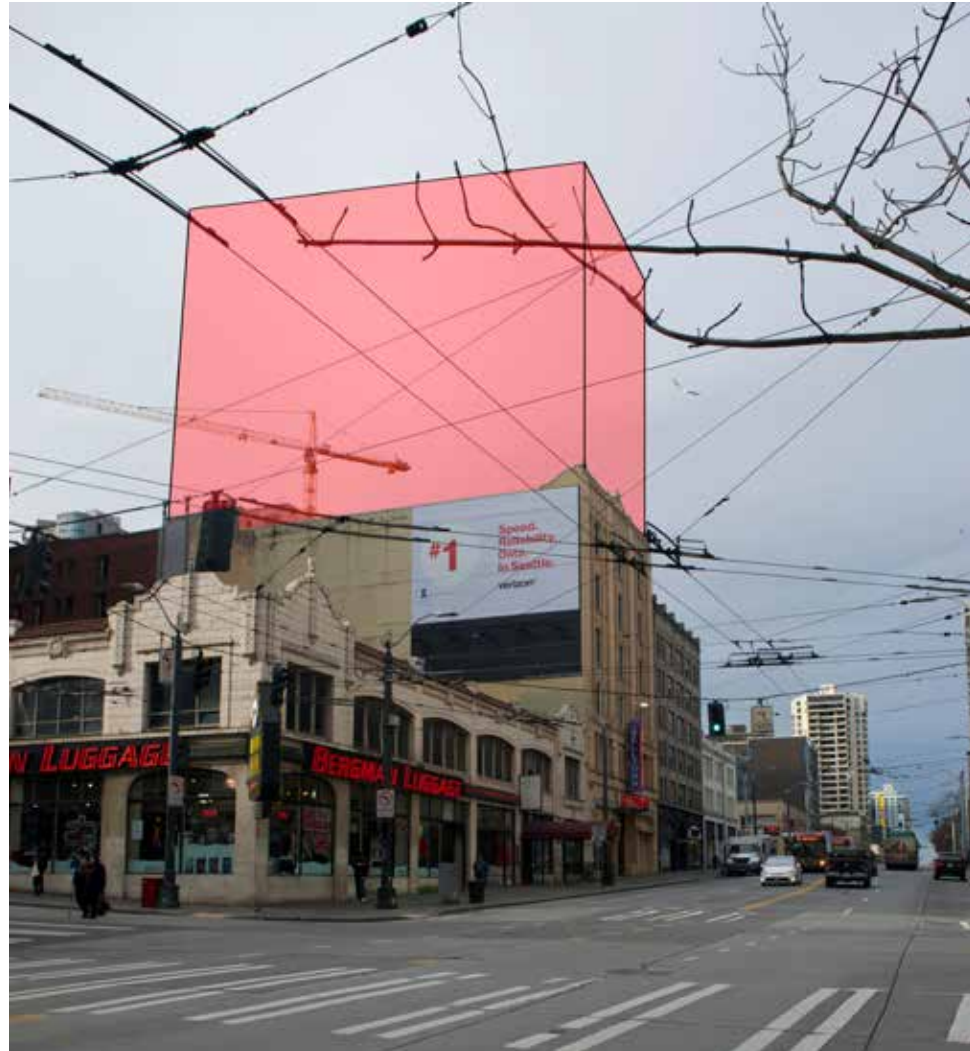


ALLEY VIEW

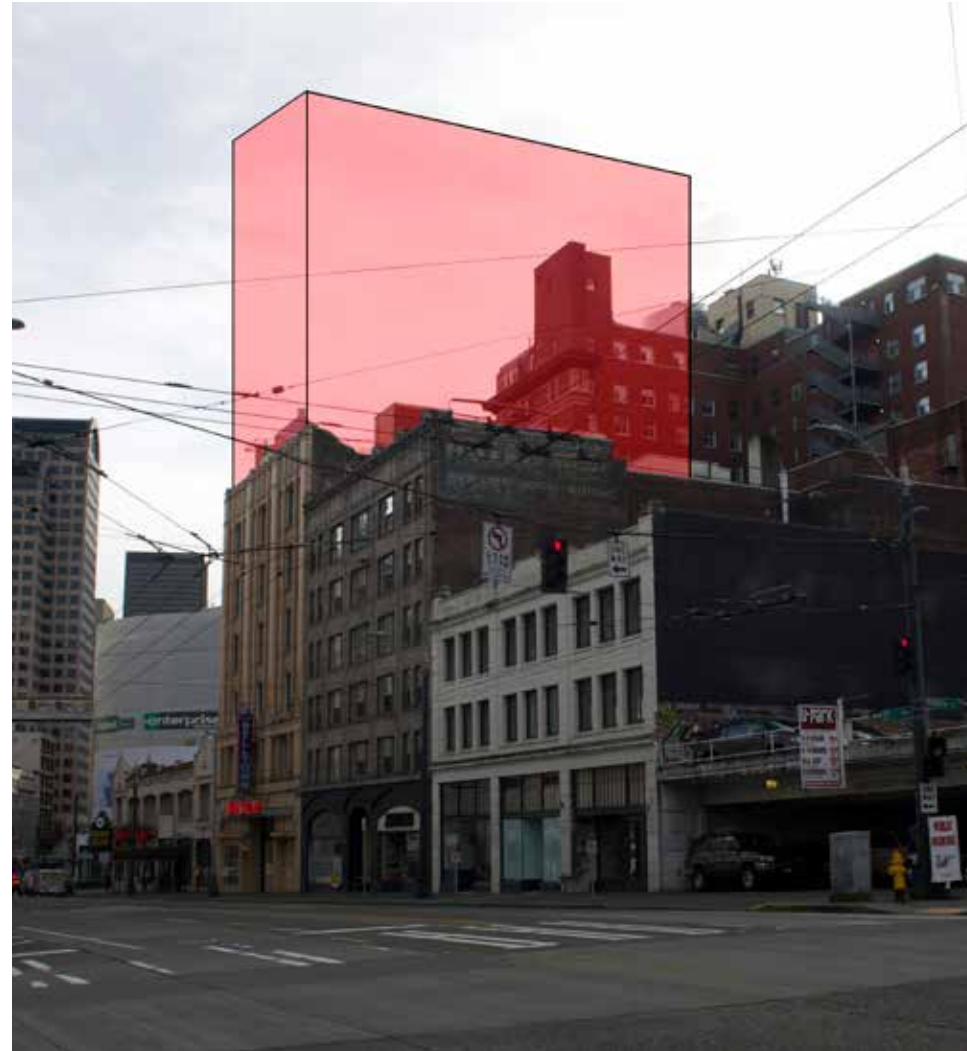


# urban design analysis

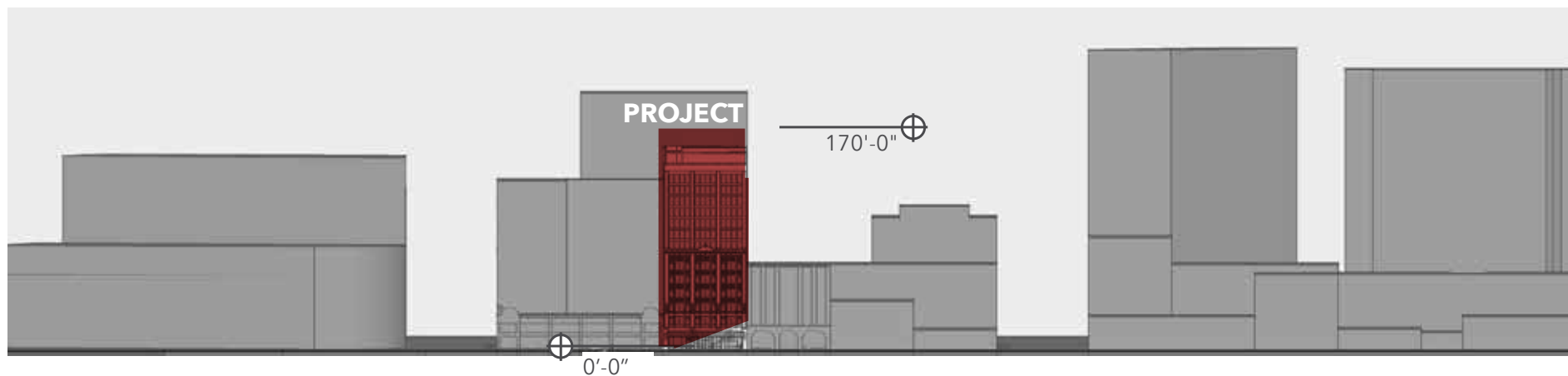
## site context



LOOKING NORTH ALONG 3RD AVENUE WITH ALLOWED ZONE MASSING



LOOKING SOUTH ALONG 3RD AVENUE WITH ALLOWED ZONE MASSING



### HEIGHT + SCALE

The existing structure is a six story building on a block that has multiple future developments in progress. The proposed scheme would add nine stories with a rooftop amenity level. At the height of 185', this proposed scheme would bridge between the historic and pedestrian scale and the surrounding planned towers.

### FORM CONSIDERATIONS

- Addition to existing structure to have small set back in order to keep the historic facade prominent
- Additional stories provides opportunity to best utilize site square footage and development potential.
- Windows on the addition to follow queues and proportions from existing historic facade.
- Proposed scheme utilizes not only the existing facade but also the existing structure.

### EXISTING CONDITIONS




The site is located mid-block between Stewart Street and Virginia Street on 3rd Avenue and is among many other buildings identified in the Belltown neighborhood as having iconic character. The existing exterior is in good condition and intended to be kept as an adaptive reuse with the intention to keep the character of the existing block intact, with other historic building such as the Haddon, Securities and Josephinum Buildings in the vicinity of the project. While the design aesthetic in the Belltown area is rather mixed, this block on 3rd Avenue has a historic character that provides inspiration for the addition proposed for this project.

The existing sidewalk is 12' wide with a curb cut for the existing parking in the structure and street trees are present along the block. Located on a major pedestrian thorough fare and several bus lines, the street receives a high amount of pedestrian traffic. The existing alley is in average condition with both concrete and brick paving with access from both Virginia and Stewart Streets, and the existing façade at the alley will be revitalized with new windows, paint and alley access points for the proposed commercial uses.



# urban design analysis

## streetscape montage

KEY:  Project Site  Area Directly Across from Project Site  New Development

STREETSCAPE - 3RD AVENUE, looking East



Rapid Ride & Bus Stop  
Rapid Ride Lines D & E &  
Bus Lines 1, 2, 3, 4, 5, 13,  
15, 16, 17, 18, 24, 26, 28,  
29, 33, 40, 82

New Development

U-Park System  
approx 75 spaces

Virginia Street

Bed, Bath & Beyond

STREETSCAPE - 3RD AVENUE, looking West



Republic Parking  
Northwest

Enterprise Rent-A-Car  
Alamo Rent A Car

Stewart Street

Bergman Luggage

Site

Haddon Hall Apartments  
Zanadu Comics



# urban design analysis

## streetscape montage



AREA DIRECTLY ACROSS FROM PROJECT SITE

STREETSCAPE - 3RD AVENUE,  
looking East

Securities Building Garage  
3rd Avenue Entrance  
approx. 178 spaces

Your Eyes

Edible Arrangements

Third Avenue  
Cafe

Securities Building

Stewart Street

Olive Way

Macy's



STREETSCAPE - 3RD AVENUE,  
looking West

U-Park System  
approx. 50 spaces

Virginia Street

Swiftly Printing

Rapid Ride & Bus Stop  
Rapid Ride Line C & Bus  
Lines 2, 3, 4, 14, 21, 27, 55,  
56, 57, 82, 116, 118, 119,

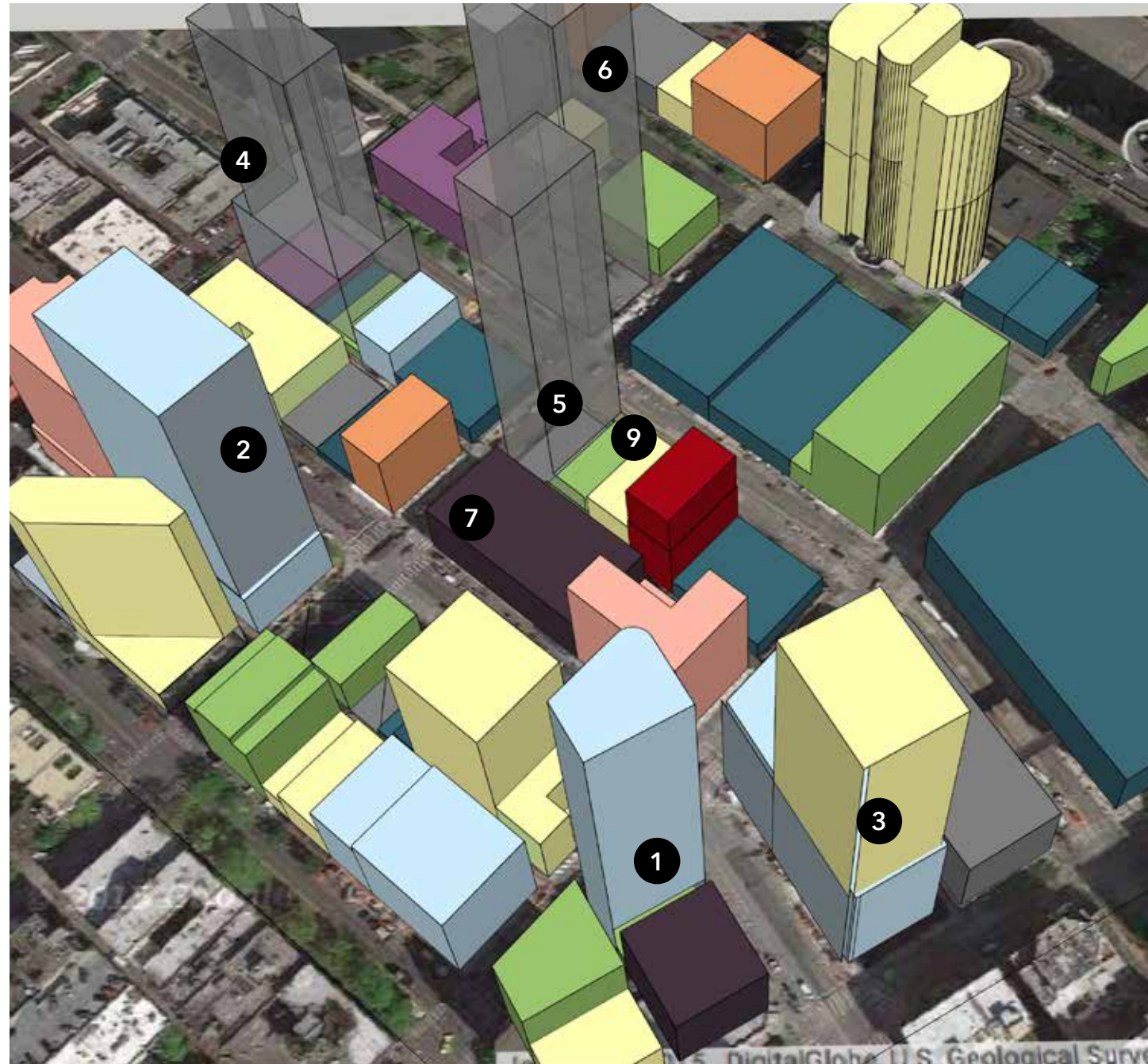
Under  
Construction

Abacadabra  
Printing



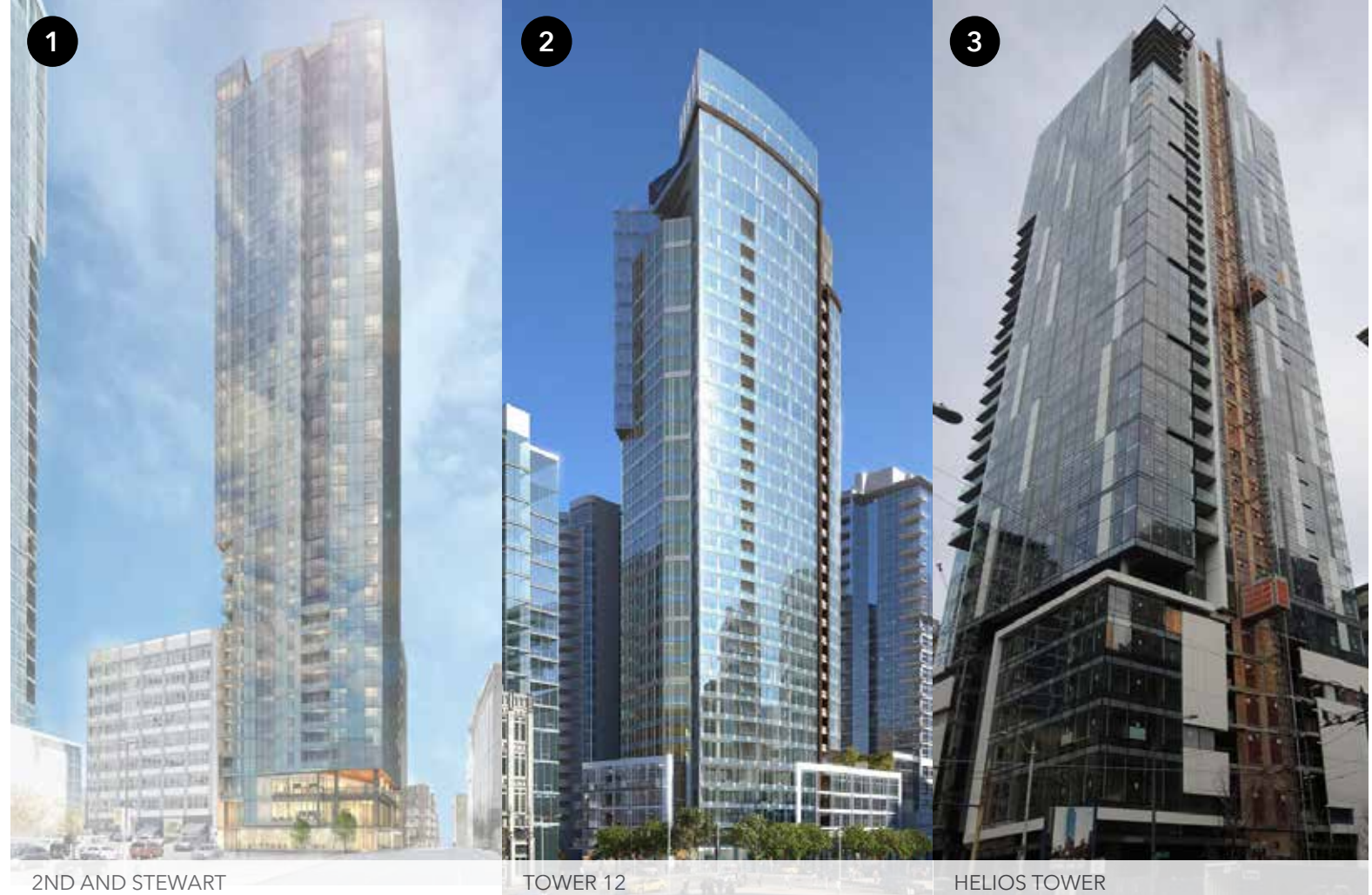
# urban design analysis

9-block vicinity



- LEGEND:
- Site
  - Offices
  - Mixed-Use
  - Parking
  - Retail
  - Hotel
  - Services
  - Residential
  - Entertainment
  - Under Construction

## RECENT DEVELOPMENTS



2ND AND STEWART

TOWER 12

HELIOS TOWER

## STREETSCAPES



PALLADIAN

HADDON HALL



### 23.49.008- STRUCTURE HEIGHT

A.3 - 290' limit max for residential uses, 440' limit max for residential uses with incentives.

**Proposed: Preferred Scheme has a Building Height of approximately 185'.**

D.1 - Rooftop features that are permitted with unlimited rooftop coverage.

Open railings, Insulation material, rooftop decks and other similar features

Solar collectors up to 7feet above the height limit.

D.2 - Rooftop features that are permitted as long as the combined coverage of all rooftop features does not exceed 55% of the roof area.

Stair Penthouses

Covered or enclosed common recreation area

Mechanical Equipment

**Proposed: Preferred Scheme remains below allowable roof coverage.**

**Allowed: 3,104 SF**

**Proposed: 1,112 SF**

### 23.49.009 - STREET-LEVEL USES

A - One or more street level uses required on street level on all lots abutting street designated on Map 1G

3rd Ave: Street Level Uses Required

B.1 - 75% of the street frontage required to be occupied by permitted uses such as services, retail, entertainment uses, etc.

**Proposed: Entire proposed frontage complies with permitted uses (coffee shop, restaurant, and bar)**

### 23.49.010 - REQUIREMENTS FOR RESIDENTIAL USES

B - Common recreation area: 5% of total gross floor area in residential use.

Max of 50% of common area may be enclosed

Min. horizontal dimension for required common area shall be 15 feet, except at open spaces. No space shall be less than 225 square feet.

**Proposed: All residential amenity space to be provided on the roof deck.**

### 23.49.011 - FLOOR AREA RATIO

A.1 - Base FAR: 5, Max FAR: 7

B.1 - Exemptions from FAR calculations are as follows:

Street Level Uses (retail, sale & services)

Residential uses

Floor area below grade

3.5% allowance for mechanical space

**Proposed: Allowable FAR of 45,360 SF. Preferred Scheme has 45,114 SF.**

### 23.49.018 - OVERHEAD WEATHER PROTECTION

A - continuous weather protection required along entire street frontage of structure

B - Minimum dimension of 8' wide or extend to 2' from curb line, whichever is less

D - Must be between 10' to 15' above the sidewalk

**Proposed: Canopy will be continuous along the length of the existing street frontage and will be located approx. 12' above grade.**

### 23.49.019 - PARKING REQUIREMENTS

A.1 - No Parking is required

E.1 - Min. number of off-street bike parking spaces required is as follows:

Residential short term: 1 space for every 2 dwelling units

Residential long term: 1 space for every dwelling unit, 3/4 ratio after 50

**Proposed: No automobile parking is provided. Bike Parking: 112x0.05=56 short term stalls required for residential. 97 long term stalls req'd. Total = 153 stalls provided at Basement Level.**

### 23.49.022 - MINIMUM SIDEWALK AND ALLEY WIDTH

Minimum width of the street as identified by Map 1C is to be 18'

A.1 - If a new structure is proposed on lots abutting the streets, sidewalks shall be widened, if necessary, to meet the minimum standard.

**Proposal: The existing structure is to remain so it is infeasible to meet the minimum required street width of 18'. The existing sidewalk width is 12' and is consistent with the existing lots to the North and South.**

### 23.49.056 - MINIMUM FAÇADE HEIGHT

A.1 - Class I Pedestrian Street (DMC): 25'

C.1 - Façade Transparency Requirements

Requirements apply to the area between 2' and 8' above the sidewalk

**Proposed: The Preferred Scheme has a fully glazed ground level.**

C.2 - Façade Transparency requirements do not apply to residential use areas.

C.4 - Class I pedestrian Streets: Min. of 60% of street level, street-facing façade to be transparent.

D.2 - Blank Façade Limits for Class I Pedestrian Street

Blank façade areas shall be no more than 15' wide.

Any blank segments shall be separated by at least 2'

**Proposed: No blank façade provided.**

E - Street trees are required on all pedestrian classified streets

**Proposed: 2 street trees provided.**

### 23.49.058 - UPPER-LEVEL DEVELOPMENT STANDARDS

A - A 'tower' is a portion of the structure over 85' that has non-residential use above 65' high or above 160' high.

C.1 - Façade Modulation

Modulation is required above a height of 85' for any portion of structure that is within 15' from a street lot line.

E.1 - Max limit on residential gross floor area per story

Avg residential area limit of a tower if height exceeds the base height limit for residential use: 10,700 SF

**Proposed: Average residential floor area = 5,820 SF.**

E.2.A - Maximum Tower Width

Max width of building above 85' along north/south axis (parallel to the Avenues) shall be 120' or 80% of the width of the lot, whichever is less

E.2.A.1 - Exception: On a lot where the limiting factor is the 80% width limit, the max. façade width is 120', if all elevations above a height of 85', no more than 50% of the area of the lot located within 15' of the street lot line.

F.3 - If any part of a tower exceeds 160' in height, then all portions of the tower that are above 125' in height must be separated from any other existing tower that is above 160' in height, and the min. separation required between towers from all points above the height of 125' in each tower is 80'.

**Proposed:**

**Adjacent high rise buildings:**

**Moore Theater (mixed-use): 7 stories x 12' per floor = 84'**

**=> Does not meet definition of a tower**

**Josephinum (mixed-use): 12 stories Residential x 11' per floor = 132'**

**1 story Non-Residential x 18' = 18'**

**Total height = 150'**

**=> Does not meet definition of a tower**

### 23.53.030 - ACCESS EASEMENT STANDARDS

B.1 - Width of new alley right-off-ways in the Downtown zones is to be 20 feet wide.

F.1 - Existing Alleys Which Do Not Meet the Minimum Width - When existing structures are located in the portion of the lot to be dedicated, that portion of the lot shall be exempt from dedication requirements.

**Proposed: Because the existing 6 story structure is to remain in all proposed schemes, the project would meet the exemption requirements identified in SMC 23.53.030.F.1.**



# zoning summary

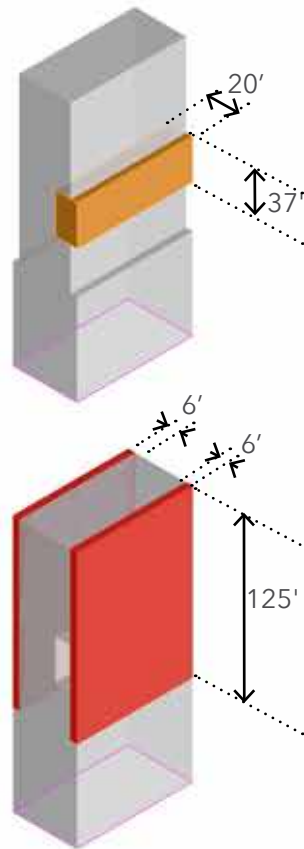
## building envelope

### TOWER SEPARATION

#### 23.49.058.D.3

If any part of a tower exceeds 160' in height, then all portions of the tower that are above 125' in height must be separated from any other existing tower that is above 160' in height, and the min. separation required between towers from all points above the height of 125' in each tower is 80.'

(See page 44-45 for director special exception)

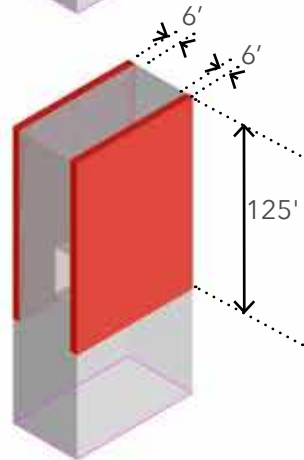


### MAXIMUM FACADE WIDTH

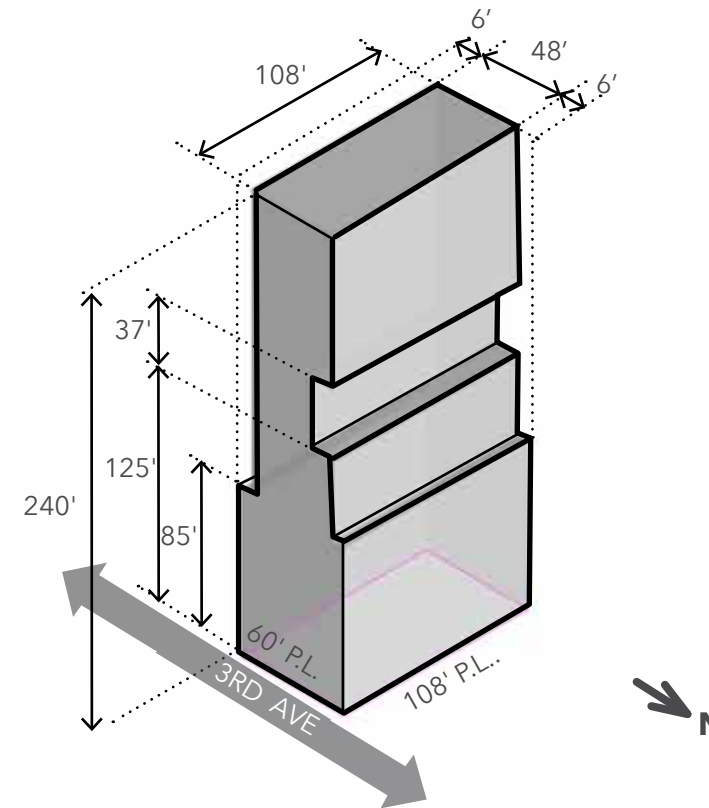
#### 23.49.058.C.2

Max façade width for portions of a building above 85' along the north/south axis of the site shall be 120 feet or 80 percent of the width, whichever is less.

(See page 42-43 for departure request)



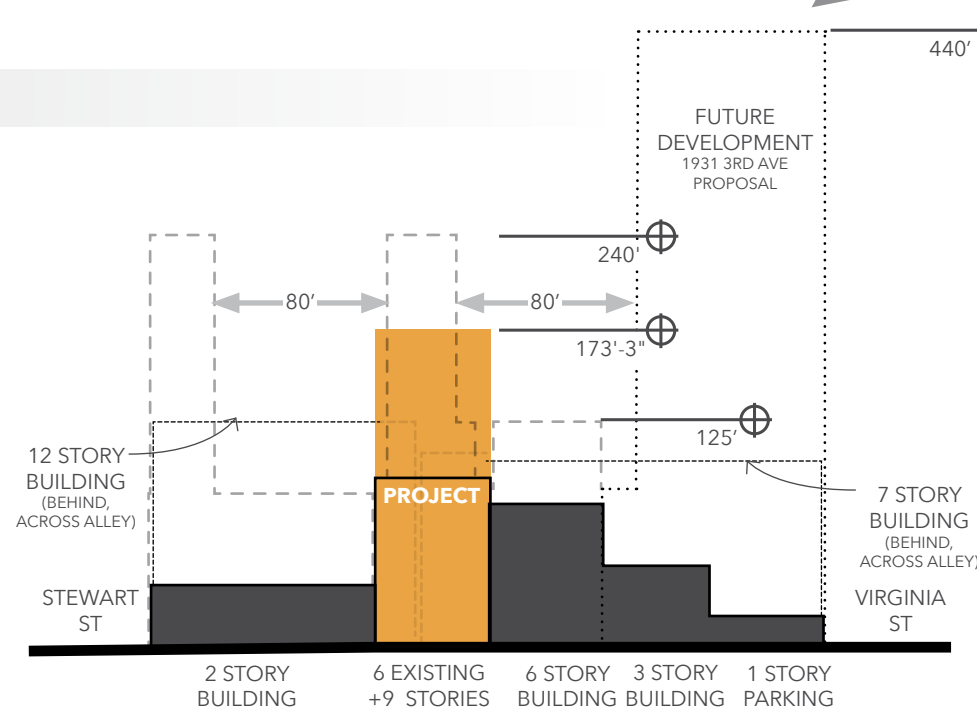
### ZONING ENVELOPE (ALLOWED, INCORPORATING ALL CONSTRAINTS)



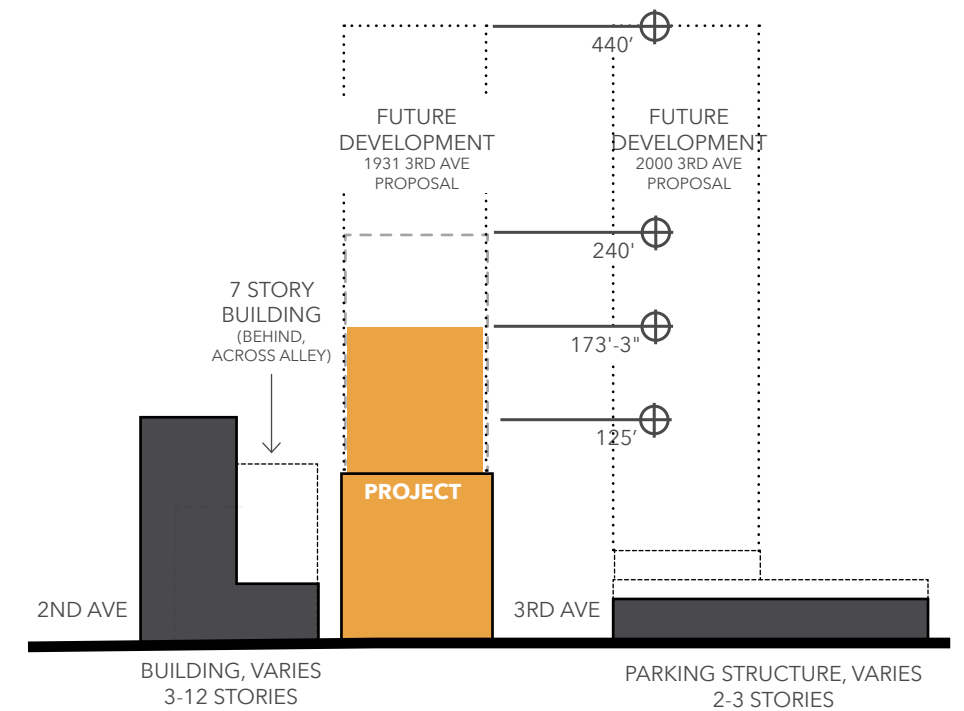
### ALLOWABLE HEIGHT AND SURROUNDING CONTEXT



- KEY:
- ADJACENT EXISTING BLDG.
  - PROJECT
  - BEHIND
  - ALLOWABLE HEIGHT
  - PROPOSED FUTURE DEVELOPMENT
  - TOWER SEPARATION



SITE SECTION S-N



SECTION W-E



# architectural concepts

## option 3 (preferred): proposal & site plan



### STATISTICS

# Units: 112 Residential Units  
 Residential Amenity SF: 3,657 SF  
 Commercial Retail SF: 3,680 SF  
 Existing SF: 44,729 / Overall SF: 93,972  
 Bike Stalls: 56

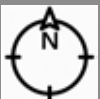
Code Compliant: Requires departures

### PROS

- Responds to existing building articulation
- Mimics existing rhythm
- North/South modulation for added interest
- Larger room/unit square footage per plan
- Consistent with neighborhood character (material and style)

### CONS

- Requires departure





# priority design guidelines

## downtown and belltown guidelines

### SITE PLANNING & MASSING:

#### Response to the Larger Context

##### A-2 ENHANCE THE SKYLINE

Downtown Guidelines: Design the upper portion of the building to promote visual interest and variety in the downtown skyline.

**Response:** The proposed addition and a portion of the existing building is set back along the north and south sides to pull away from the existing structures, while providing a wider street front elevation to produce a more balanced proportion to the existing facade along 3rd Avenue. Also, the project is proposed to be approximately 185' in height which fits well with the immediately surrounding, existing projects such as the Securities and Josephinum buildings.

### ARCHITECTURAL EXPRESSION:

#### Relating to the neighborhood Context

##### B1 - RESPOND TO THE NEIGHBORHOOD CONTEXT

Belltown Guidelines: Develop an architectural concept and compose the major building elements to reinforce desirable urban features existing in the surrounding neighborhood.

**Response:** The proposed addition not only keeps the existing structure to provide continuity with the surrounding context but also strives to follow the scale and proportion already established by the existing building. While the street frontage of the additional stories match the existing massing, the north and south sides then step back approx. 15' from the main facade to allow articulation of the upper mass as well as allow more light to penetrate down to the street level.

##### B2 - CREATE A TRANSITION IN BULK AND SCALE

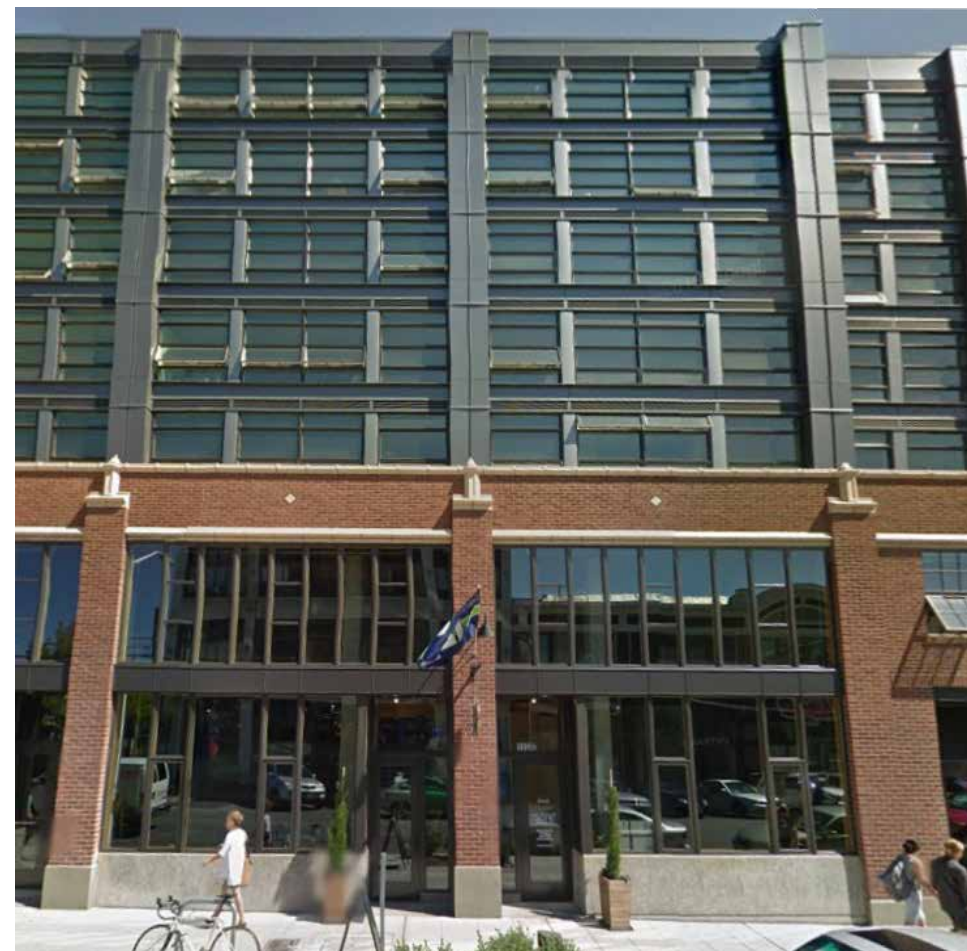
Belltown Guidelines: Compose the massing of the building to create a transition to the height, bulk, and scale of development in nearby less- intensive zones.

**Response:** The proposed project is not building to the maximum height limit and will be approximately 185' in height in a 400' zone and when completed will be a similar height or shorter than the majority of the new structures in the surrounding 9 block area. Also, with the addition of the upper stories, the new structure will be slightly set back at the street frontage to allow for a clean delineation between old and new adding emphasis to the existing ornate parapet along 3rd Avenue.

### B3 - REINFORCE THE POSITIVE URBAN FORM & ARCHITECTURAL ATTRIBUTES OF THE IMMEDIATE AREA.

Belltown Guidelines: Consider the predominant attributes of the immediate neighborhood and reinforce desirable siting patterns, massing arrangements, and streetscape characteristics of nearby development.

**Response:** The proposed design not only keeps the existing structure to provide continuity with the surrounding context but also follows the similar scale and proportions for the additional stories being added above. This allows for a renewal of the current architectural patterns by emphasizing the more dominant qualities found in the existing character.



SCALE AND PROPORTION TRANSLATED OVER VARYING MATERIALS

### THE STREETScape:

#### Creating the Pedestrian Environment

##### C1 - PROMOTE PEDESTRIAN INTERACTION

Belltown Guideline: Spaces for street level uses should be designed to engage pedestrians with the activities occurring within them. Sidewalk- related spaces should appear safe, welcoming, and open to the general public.

**Response:** By keeping the existing structure, the project provides a street frontage consistent with the surrounding building context with Haddon Hall to the north and the notable 2-story Bergman luggage retail building to the south. The three buildings, in alignment, form a street frontage that fits well with the existing historic character and pedestrian experience of 3rd Avenue. The proposed project also intends to install two street trees and a unique entry canopy to enhance the character of the existing building.

##### C4 - REINFORCE BUILDING ENTRIES

Downtown Guidelines: To promote pedestrian comfort, safety, and orientation, reinforce building entries.

**Response:** The proposed project design will allow for a large double height lobby space along the 3rd Avenue street frontage to be fully glazed as well as a canopy along the length of the street frontage, providing continuous overhead weather protection and allowing for identifiable entry signage for residents and guests. Due to the limited width of the site as well as the reuse of the existing structure, the integrated canopy design will allow for a very prominent marker for the building entrance.



# suggested priority design guidelines

## C-5 - ENCOURAGE OVERHEAD WEATHER PROTECTION

Belltown Guidelines: Project applicants are encouraged to provide continuous, well-lit, overhead weather protection to improve pedestrian comfort and safety along major pedestrian routes.

**Response:** The project proposes to provide continuous canopy coverage along the street frontage of the existing structure. This would provide adequate weather coverage and will be designed to coordinate with the originally designed street frontage aligning with the column/exterior fenestrations. The canopy will be designed to keep draining rain water off the pedestrians and will be well-lit with both under canopy lighting and wall sconces to provide an inviting entrance to restaurant guests and residents alike.

## C-6 - DEVELOP THE ALLEY FAÇADE

Belltown Guidelines: To increase pedestrian safety, comfort, and interest, develop portions of the alley facade in response to the unique conditions of the site or project.

**Response:** With the use of the existing structure, the project is unable to set back any portion for the use of loading but has taken into account the need to service deliveries as well as trash collection. The interior back of house spaces will be designed accordingly to allow for service access from the alley to these areas. Appropriate lighting will be added for safety purposes. There is no parking provided in the project so will add minimal additional car traffic to the alley.

## PUBLIC AMENITIES:

### Enhancing the Streetscape & Open Space

## D-3 - PROVIDE ELEMENTS THAT DEFINE THE PLACE

Belltown Guidelines: Provide special elements on the facades, within public open spaces, or on the sidewalk to create a distinct, attractive, and memorable "sense of place" associated with the building.

**Response:** With the site location on 3rd Avenue, this street has been identified as both a Transit and Promenade Street. The project proposes to install two new street trees along the street frontage of the project and will consider what 'civic' installations might be the most appropriate when placed in front of the existing structure. Due the use of the existing structure, this does not allow for the widening of the sidewalk but allows for much greater character interaction as the project intends to utilize street level glazing, a new entry canopy and appropriate lighting to emphasize the character of the existing ornate terra cotta facade.

## D-4 - PROVIDE APPROPRIATE SIGNAGE

Belltown Guidelines: Design signage appropriate for the scale and character of the project and immediate neighborhood. All signs should be oriented to pedestrians and/or persons in vehicles on streets within the immediate neighborhood.

**Response:** With the use of the existing structure, it is important for the project to have signage fitting the style and scale of the proposed additional structure and use. The current neon mini storage sign will be removed and a smaller, more appropriately scaled sign will be attached to the canopy to help emphasize the street level entrance for guests and residents.



CLEAN LINES AND SIMPLE LETTERING CREATE A TIMELESS EFFECT

## D-5 - PROVIDE ADEQUATE LIGHTING

Belltown Guidelines: To promote a sense of security for people downtown during nighttime hours, provide appropriate levels of lighting on the building facade, on the underside of overhead weather protection, on and around street furniture, in merchandising display windows, in landscaped areas, and on signage.

**Response:** The proposed scheme would provide a canopy that would be continuous along the width of the building. Lighting provided under the canopy as well as wall sconces and light emitting from the fully glazed street level elevation will provide adequate lighting for a safe and inviting street frontage. There will also be up lighting provided along the column faces to add drama and emphasis to the connection between the old and new structures.

## VEHICULAR ACCESS & PARKING:

### Minimizing the Adverse Impacts

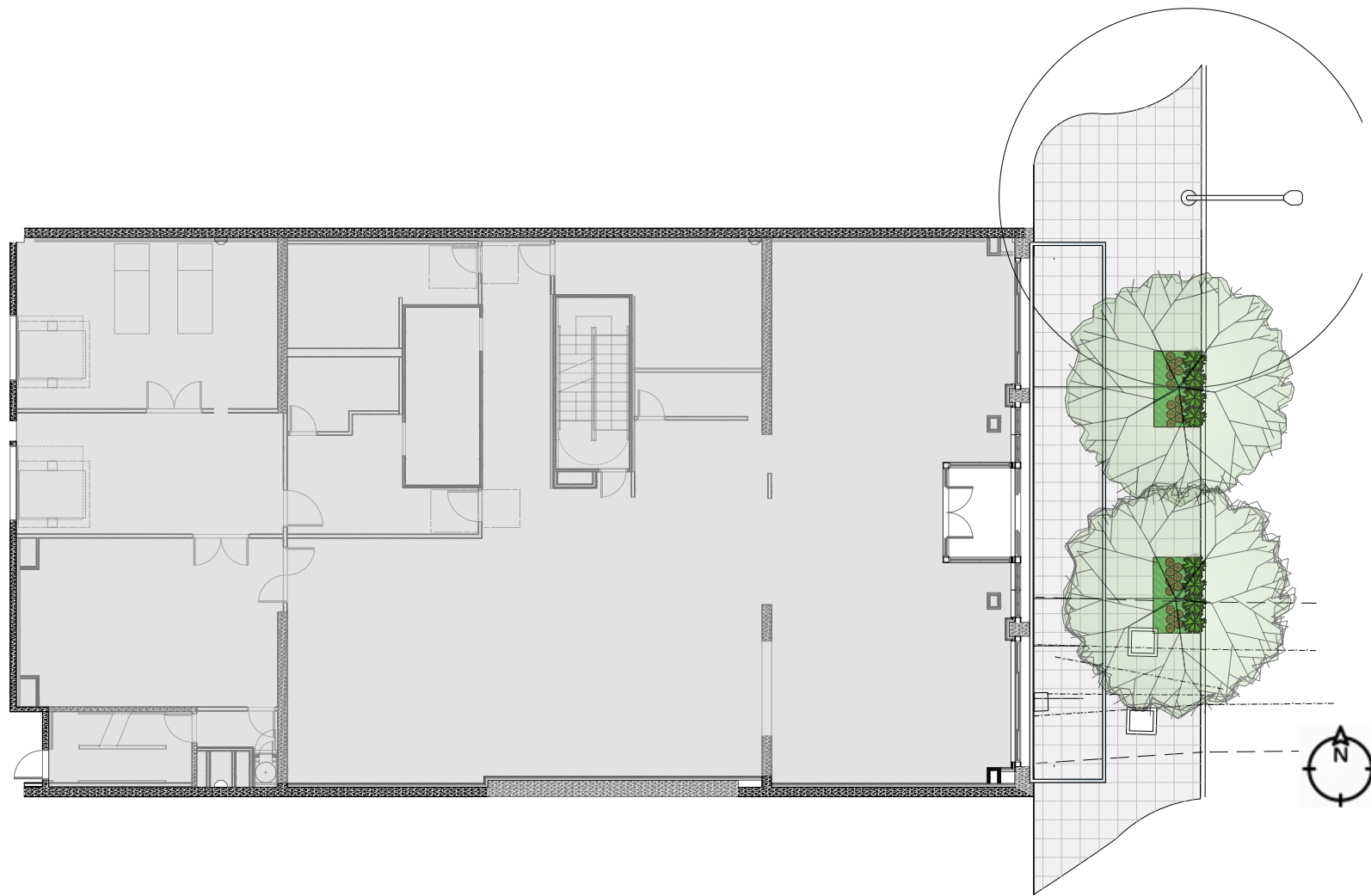
## E-1 - MINIMIZE CURB CUT IMPACTS

Downtown Guidelines: Minimize adverse impacts of curb cuts on the safety and comfort of pedestrians.

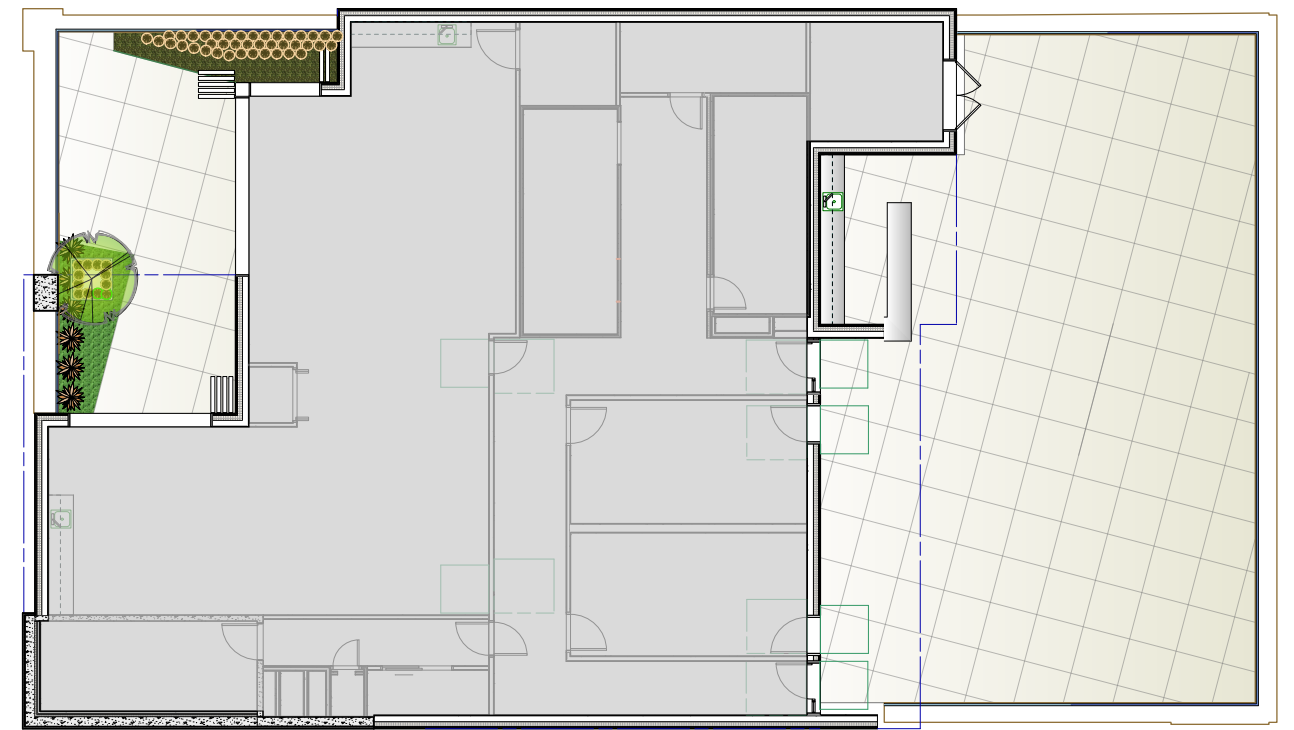
**Response:** There is one existing curb cut used for the existing mini-storage parking. The proposed project will have no parking provided and the curb cut will be removed for a continuous and clean sidewalk along the length of the structure with all service access to be from the alley.



# landscape plans



GROUND LEVEL LANDSCAPE



ROOF LEVEL LANDSCAPE

Maximum required Residential Amenity Space: 1,478 SF\*

Residential Amenity Space provided: 2,500 SF

\*Per Option 3



# landscape planting concepts

option 3 (preferred)



BETULA NIGRA  
RIVER BIRCH



PHORMIUM TENAX  
NEW ZEALAND  
FLAX



DESCHAMPISIA  
MAIDEN HAIR GRASS



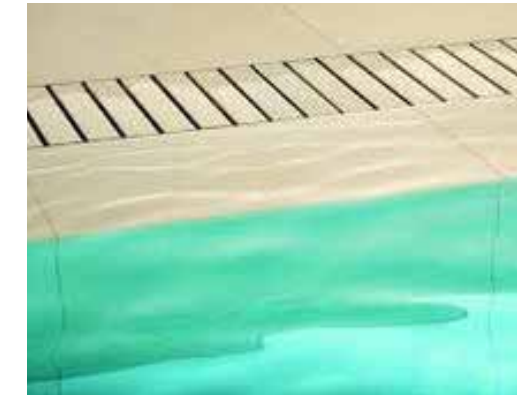
PACHYSANDRA TERMINALIS  
VARIGATED JAPANESE



OPHIPOGON NIGRESCENS  
BLACK MONDO GRASS



ILEX CRENATA  
JAPANESE HOLLY



POOL EDGE



DECK LIGHTS



ULMUS  
FRONTIER ELM



CORTEN STEEL PLANTER



POOL BLUE TILE

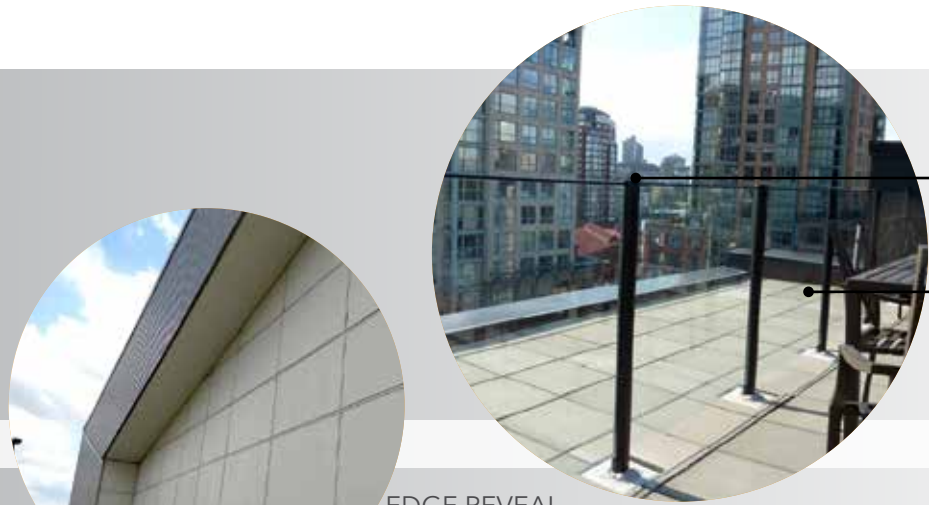


SANDBLASTED AND SAW CUT  
CONCRETE PAVING



# concept development

proposed materials



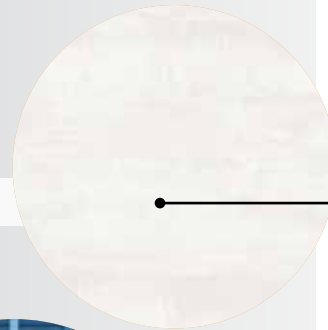
GLASS RAILING TO PRESERVE VIEWS



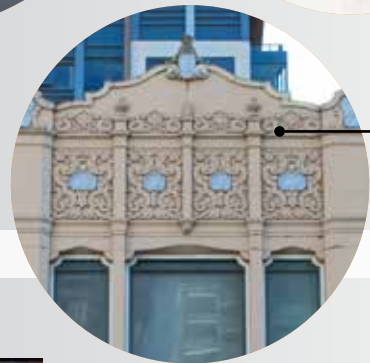
EDGE REVEAL



METAL CLADDING



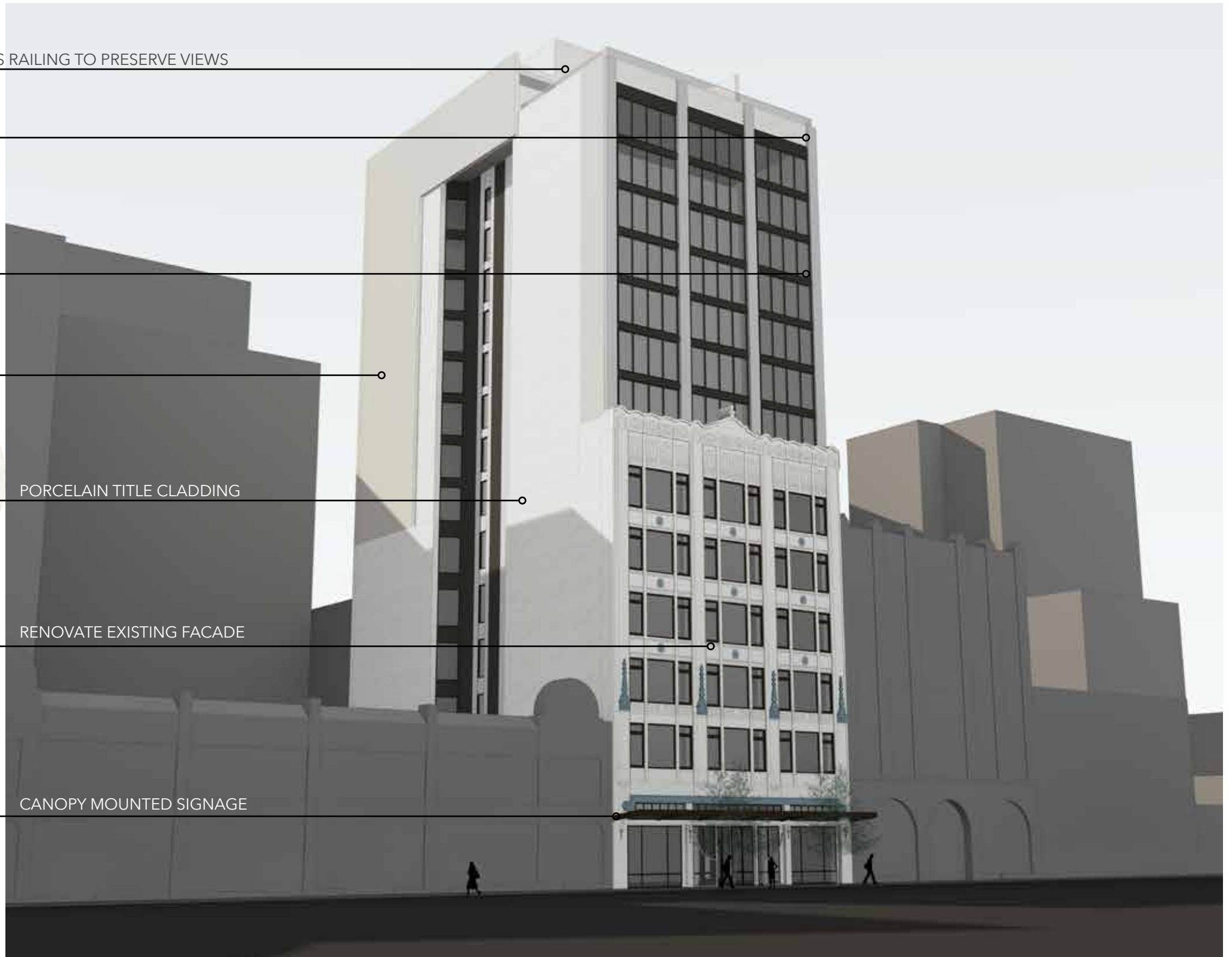
PORCELAIN TILE CLADDING



RENOVATE EXISTING FACADE



CANOPY MOUNTED SIGNAGE



pedestrian view west from 8th avenue



# departure request

## maximum facade width

### DEPARTURE REQUEST 1

#### 23.49.058.C.2

Max façade width for portions of a building above 85' along the north/south axis of the site shall be 120 feet or 80 percent of the width, whichever is less.

On a lot where the limiting factor is the 80 percent width limit, the max façade width is no more than 50 percent of the area of the lot located within 15 feet of the street lot line is occupied by the structure.

**Departure Request - Option 3 (preferred):** The maximum permitted facade width above the 85 foot height on this lot is 48 feet or 80% of the lot width as measured along 3rd Avenue. The proposed facade width above 85 feet is 60 feet or 100% of the lot width, which is a departure of 12 feet.

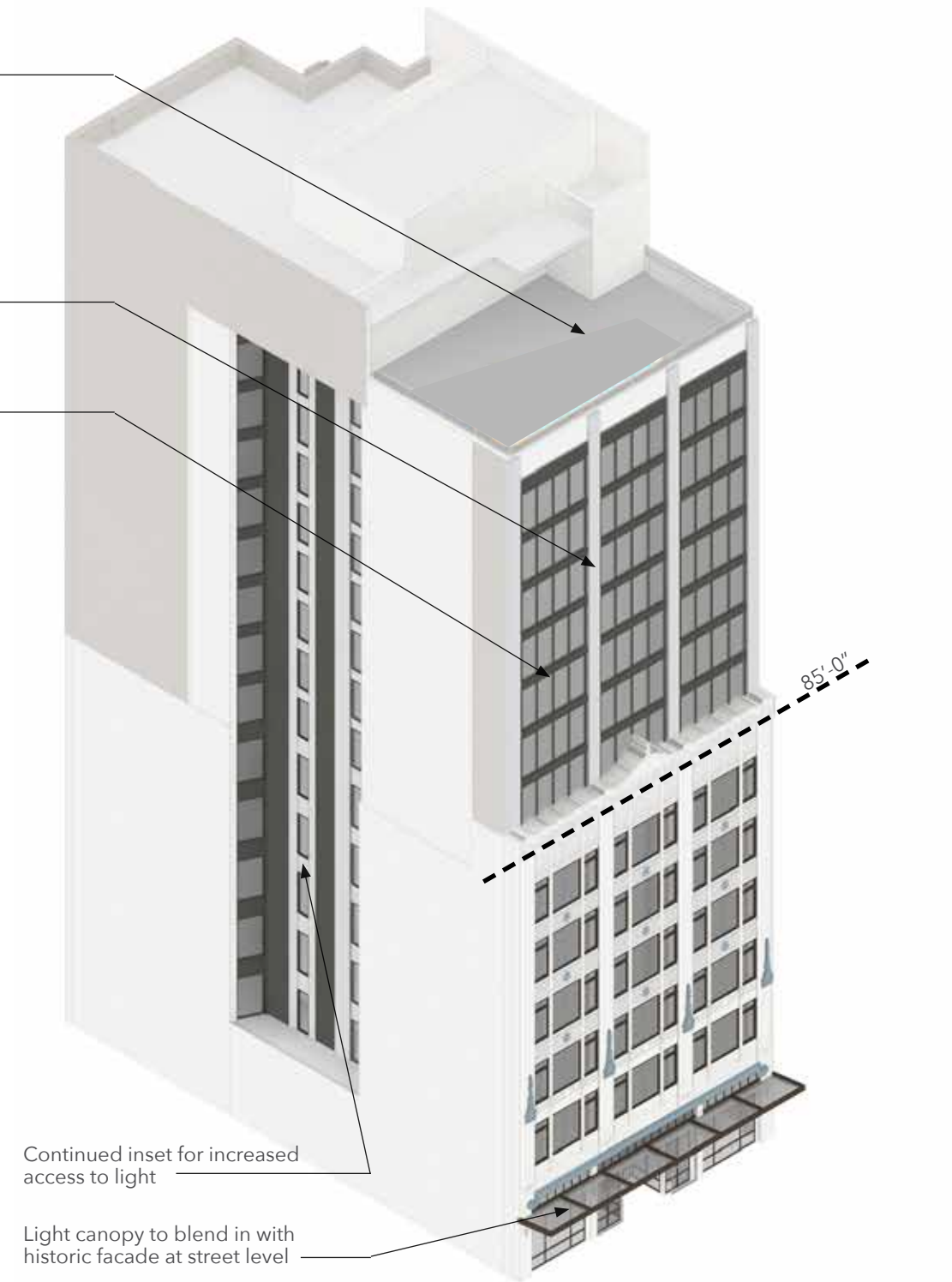
**Departure Request - Option 2:** The proposed facade width above 85 for option 2 is similarly 60 feet or 100% of the lot width, which is a departure of 12 feet. Option 2 differs in that this departure is necessary for only 24' along E-W lot line (approx. 25%).

**Design Rationale:** In an effort to keep the architectural integrity of the building, the departure allows the preferred design to create a better link between the existing structure's historic proportions and character by allowing the width to carry up the height of the building. The new addition will carry similar proportions both in height and width along with the extension of the existing columns that interact with the street level experience. In addition, the departure will increase the feasibility of developing the site in line with the Landmarks Preservation Board's recommendations (page 13), allowing the overall height of the building to be lower with more units per floor.

Top floor setback allows for smaller street presence and roof amenity space.

Vertical facade elements at addition to emphasize and continue historic facade

Addition reflects articulation and scale of historic facade

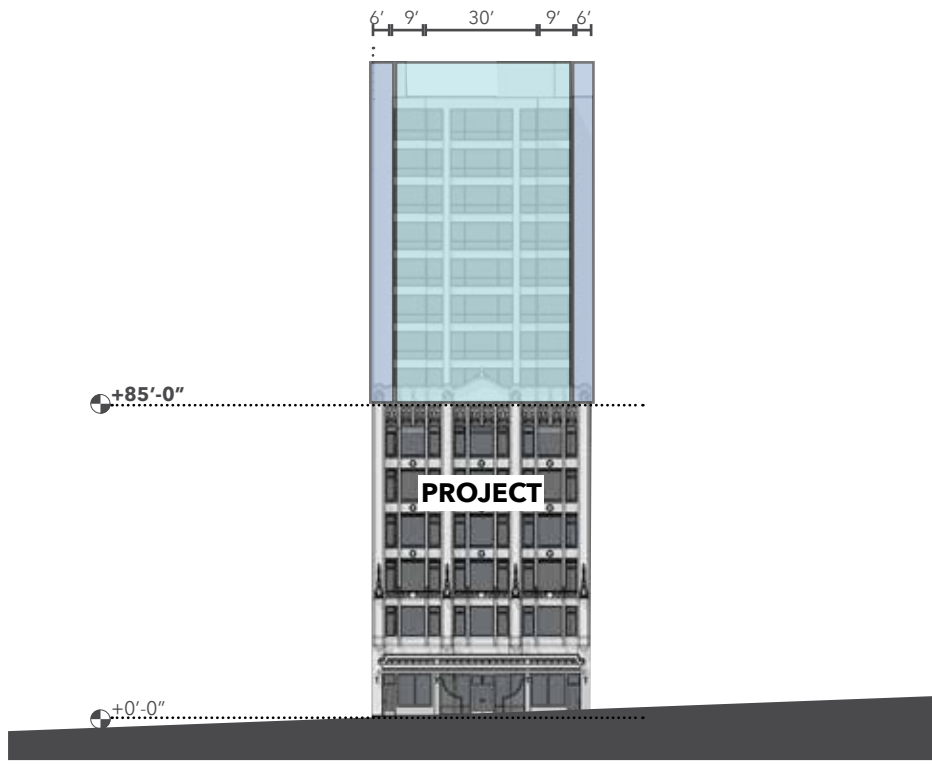


3 DIMENSIONAL VIEW

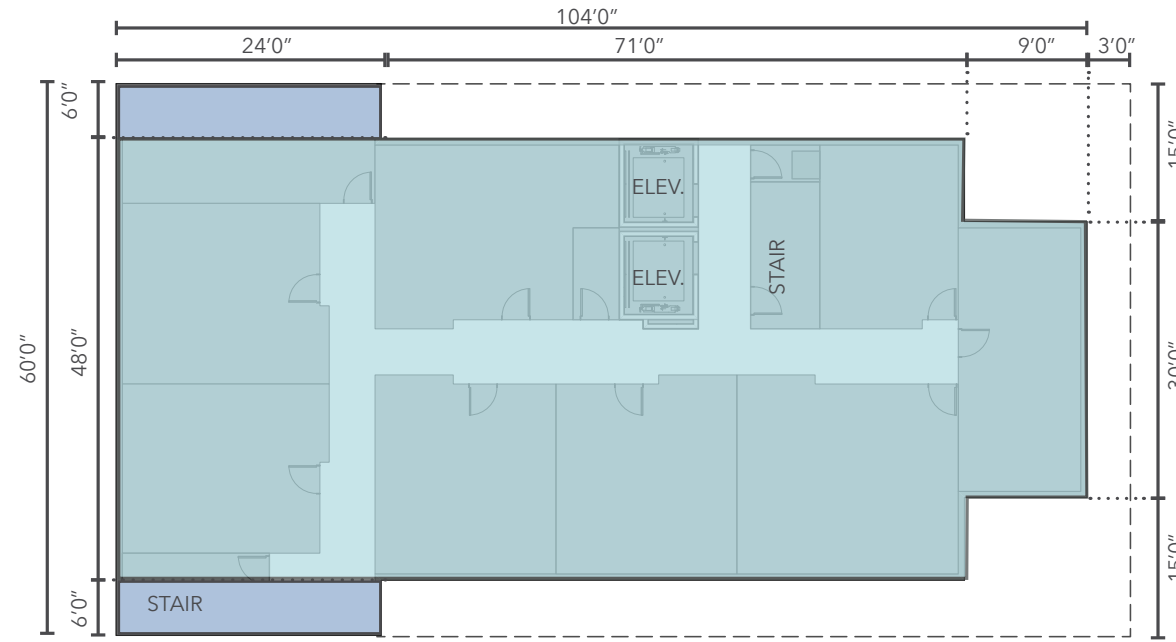


# departure request

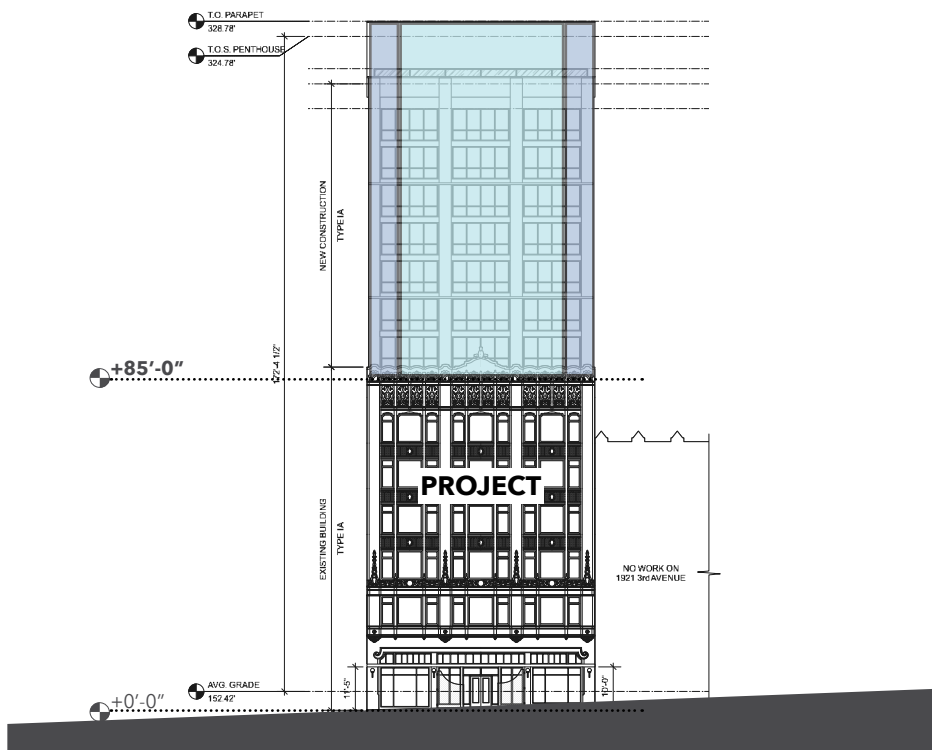
maximum facade width



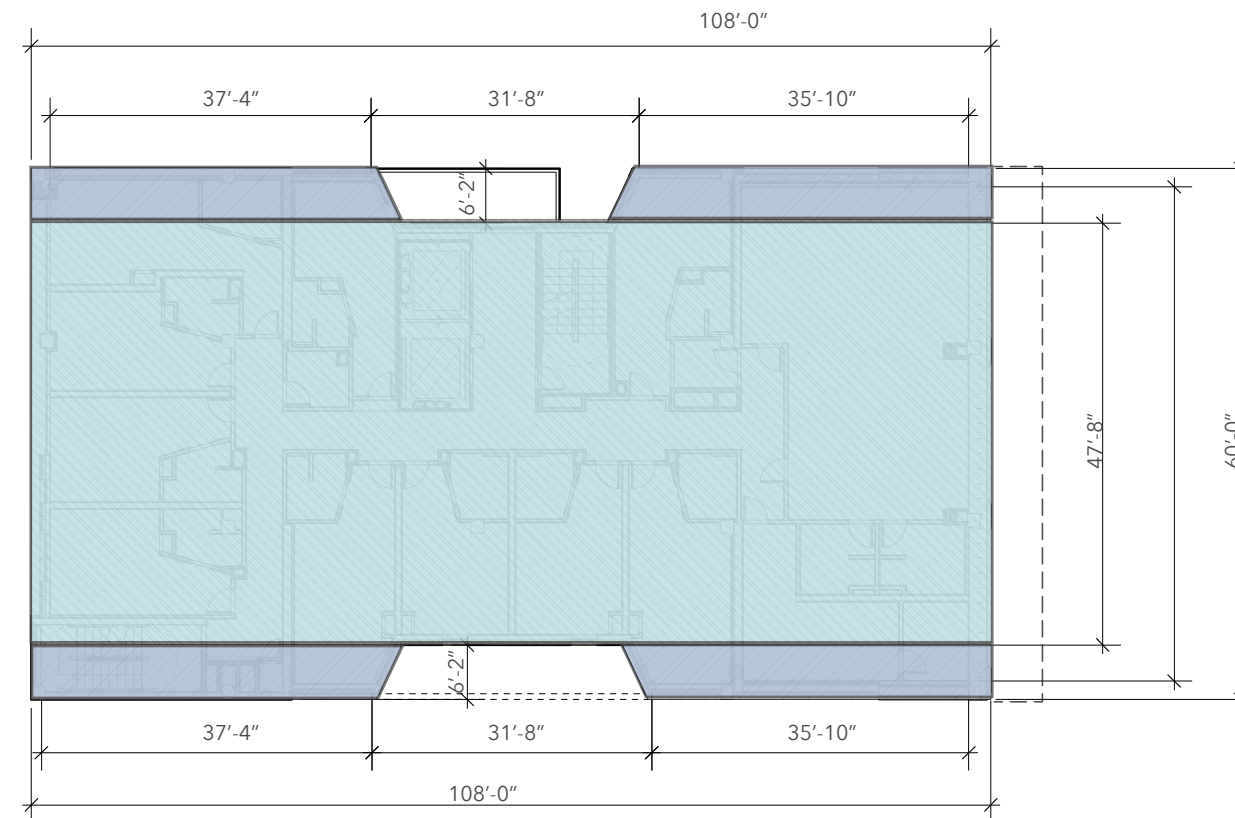
OPT. 2 EAST ELEVATION



OPT. 2 - TYPICAL LEVEL ABOVE 85'



OPT. 3 (PREFERRED) EAST ELEVATION



OPT. 3 (PREFERRED) TYPICAL LEVEL ABOVE 85'



NTS

- REQUESTED DEPARTURE
- ALLOWABLE BUILDING WIDTH

\*drawings not to scale



# director special exception | tower separation

## tower separation

### DIRECTOR SPECIAL EXCEPTION 23.49.058.D.3

If any part of a tower exceeds 160' in height, then all portions of the tower that are above 125' in height must be separated from any other existing tower that is above 160' in height, and the min. separation required between towers from all points above the height of 125' in each tower is 80.'

### DIRECTOR SPECIAL EXCEPTION - 23.49.058.D.6

If the presence of an existing tower would preclude the addition of another tower proposed on the same block, as a special exception, the Director may waive or modify the tower spacing requirements of this Section 23.49.058

FOR DESIGN OPTION 3 (PREFERRED) & DESIGN OPTION 2:

#### a. IMPACT ON ADJACENT RESIDENTIAL STRUCTURES

Minimal to no impact on adjacent residential structures. The residential building to the north only has windows on the street and alley facades. The proposed 1931 3rd Ave tower to the north has a black façade up to 154 ft along their southwest property line before it steps back 14'.

#### c. IMPACT OFFSET

The proposed development will preserve the existing Landmarked structure on the site.

#### c. IMPACT ON PUBLIC ENVIRONMENT

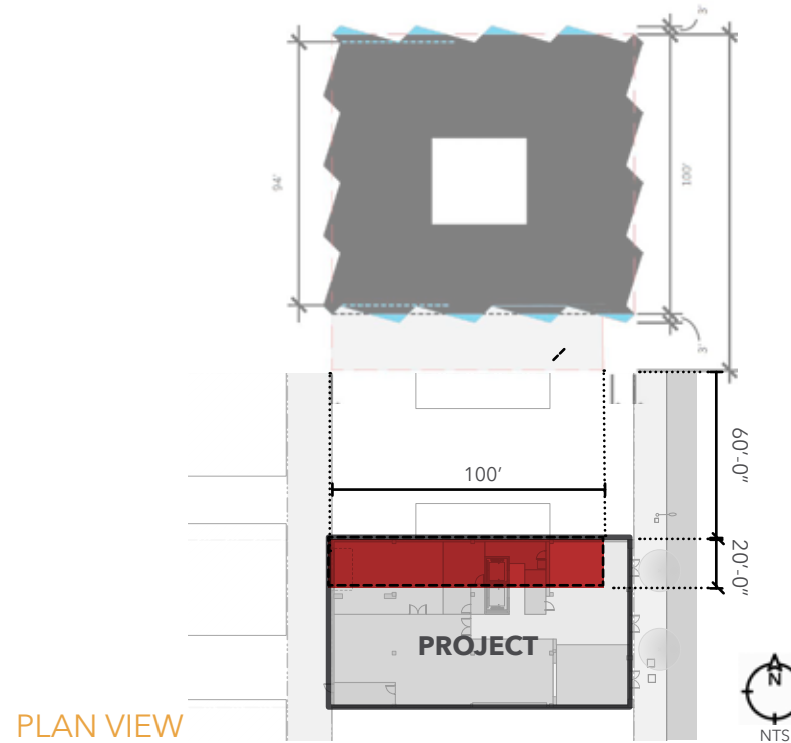
There is no impact to views from the street or public open spaces. It will have minimal impact to the shadows on the street because the project's site is oriented along a northeast-southwest axis.

#### d. DESIGN CHARACTERISTICS

(See notes to the right for option #3, and notes on page 45 for option #2)

#### f. ENCOURAGING RESIDENTIAL DESIGN IN DOWNTOWN

The project is a residential development.



PLAN VIEW

### DESIGN OPTION 3 (PREFERRED)

#### DIRECTOR SPECIAL EXCEPTION - 23.49.058.D.6

#### Request for Exception:

To provide additional stories consistent with the historic facade of the existing structure, the project requests a departure for 20' of width and 37' of height along approx. 100' of required separation.

#### 23.49.058.D.6 d) DESIGN CHARACTERISTICS

An exception for the preferred scheme, Option 3, would allow the massing to take design cues from the landmark building and reuse the existing building's structure. It responds to the landmark building's articulation and mimics the existing rhythm of glazed versus opaque facade. The overall form of design Option 3 is compatible with the landmark building in that it does not detract attention from the historic articulated facade. Compatibility with the landmark portion of the project was prioritized in past meetings with the Landmark Preservation Board –see page 13. An addition matching the width of the existing facade was shown at this time and preferred by the board.

\*see page 28-31 for additional annotations & views



ELEVATION VIEW



# director special exception | tower separation

tower separation

## DESIGN OPTION 2

### DIRECTOR SPECIAL EXCEPTION - 23.49.058.D.6

#### Request for Exception:

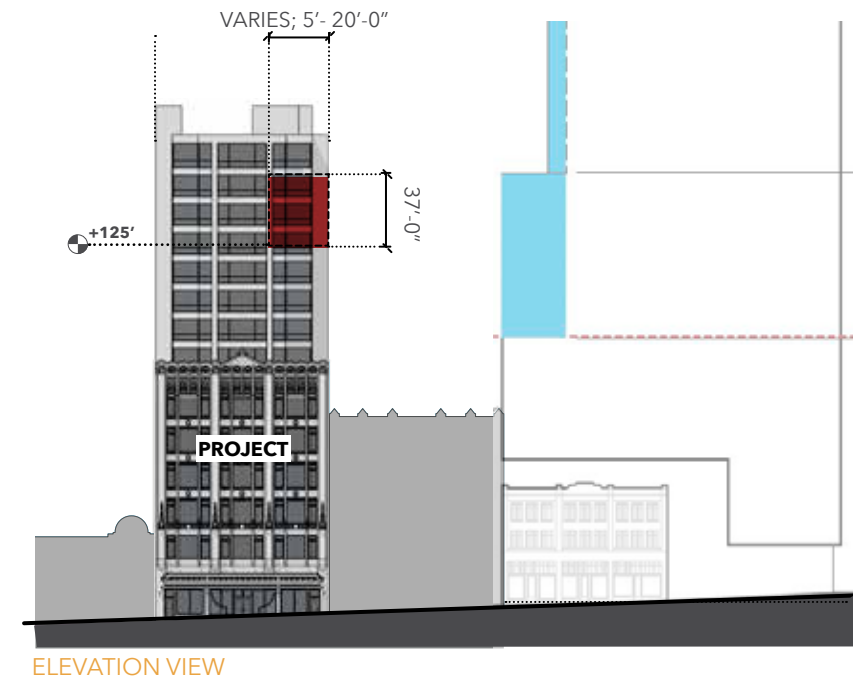
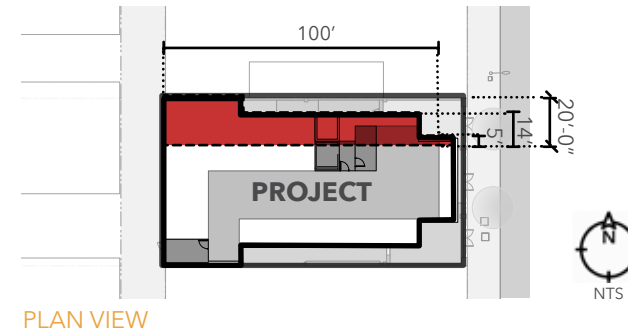
To provide additional stories consistent with the historic facade of the existing structure, the project requests a departure varying from 5- 20' of width and 37' of height along approx. 100' of required separation.

#### 23.49.058.D.6 d) DESIGN CHARACTERISTICS

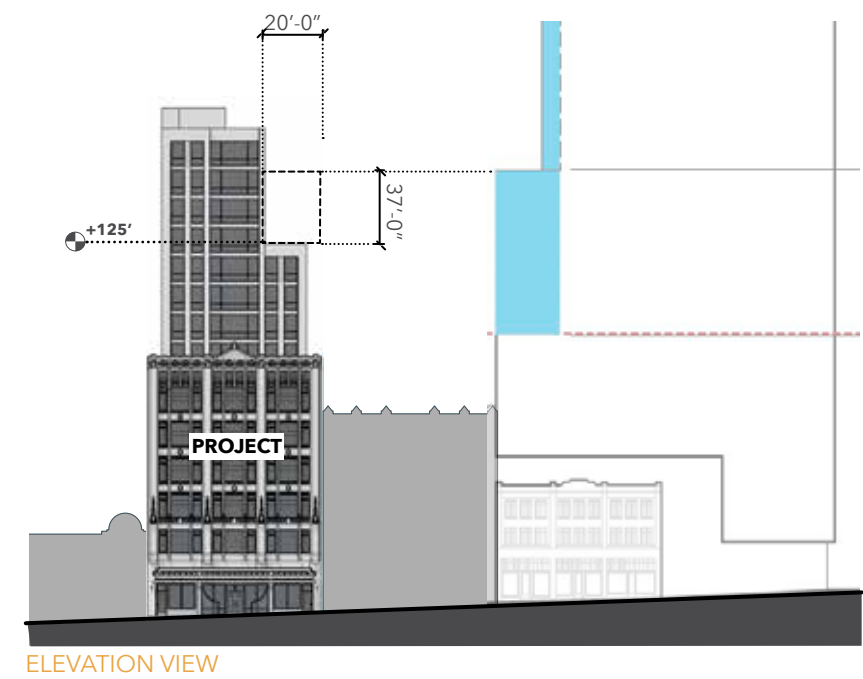
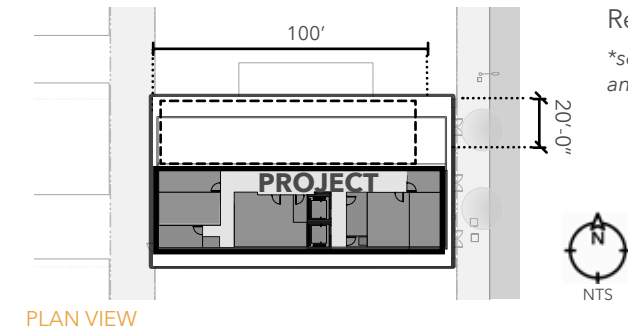
An exception for design Option 2 would allow the new addition to use the rhythm of the landmark building's facade to dictate where the addition sets back. While the area within the required tower set back for Option 2 is less than Option 3, the removal of the street facade corners creates a stark contrast between the landmark building and the new addition.

\*see page 24-27 for additional annotations & views

### DESIGN OPTION 2 DRAWINGS:

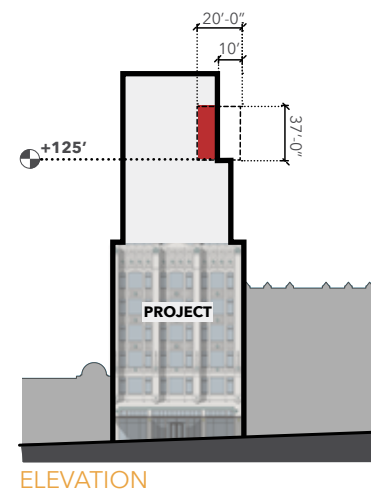


### DESIGN OPTION 1 (COMPLIANT):



Request for exception: None  
\*see page 20-23 for additional annotations & views

### ALTERNATIVE MASSINGS:

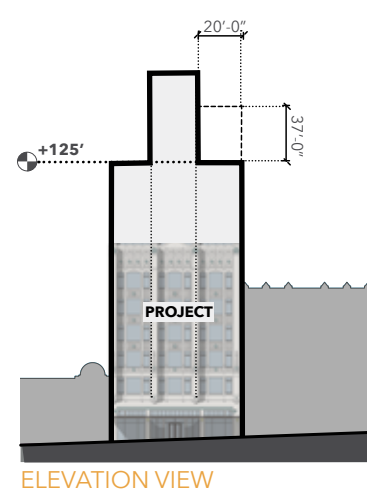


#### Alternative massing A

EXCEPTION REQUIRED: 10' width and 37' height along appx. 100' of req'd separation

PROS: - min. departure req'd

CONS: - asymmetrical  
- distracts from landmark structure

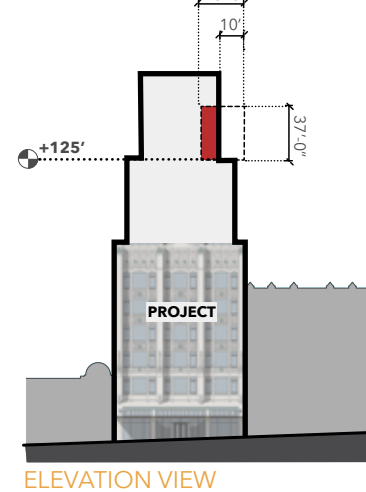


#### Alternative setback massing B

EXCEPTION REQUIRED: None

PROS: - symmetrical  
- vertical elements continued

CONS: - unbalanced form  
- infeasible floor plan



#### Alternative setback massing C

EXCEPTION REQUIRED: 10' width and 37' height along appx. 100' of req'd separation

PROS: -symmetrical  
-balanced form

CONS: -distracts from landmark structure